

Milwaukee West Line Fox River Bridge Improvement Project (Metra Bridge Z-100)

Environmental Assessment

February 6, 2017

Federal Transit Administration

Metra



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Environmental Assessment

for the

Milwaukee West Line Fox River Bridge Improvement Project (Metra Bridge Z-100) Elgin, Illinois

prepared by the

**U.S. Department of Transportation
Federal Transit Administration**

and

Metra

pursuant to:

National Environmental Policy Act of 1969 (42 USC § 4321, et seq.), Efficient Environmental Reviews for Project Decisionmaking (23 USC § 139), Council on Environmental Quality Regulations for Implementing the Procedures of the National Environmental Policy Act (40 CFR § 1500-1508), and FHWA/FTA Environmental Impact and Related Procedures (23 CFR § 771)

2/2/2017

Date of Approval

Marisol Simón

Marisol Simón

Regional Administrator

U.S. Department of Transportation

Federal Transit Administration

Bruce Marcheschi

Bruce Marcheschi

Chief Engineering Officer

Metra

2/8/17

Date of Approval

The following persons may be contacted for additional information concerning this document:

Mark Assam, AICP
Environmental Protection Specialist
Federal Transit Administration
200 W. Adams Street, Suite 320
Chicago, IL 60606
Telephone: (312) 353-4070

Andrew Roth
Director, Design, Stations & Parking
Metra
547 W. Jackson Boulevard
Chicago, IL 60661
Telephone: (312) 322-1534

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List of Acronyms and Abbreviations

ADA	Americans with Disabilities Act
ADID	Advanced Identification Program
AL	Aquatic Life
APE	Area of Potential Effects
AQ	Aesthetic Quality
AREMA	American Railway Engineering and Maintenance-of-Way Association
dB	Decibels
dba	A-weighted decibels
BMP	Best Management Practice
CCDD	Clean Construction Demolition Debris
CERCLA	Comprehensive Environmental Response, Compensation, and Liability Act
CF	Community Facility
CFR	Code of Federal Regulations
CMAA	Chicago Metropolitan Agency for Planning
CMP	Congestion Management Process
CP	Canadian Pacific
CWA	Clean Water Act
DHHS	Department of Health and Human Services
EA	Environmental Assessment
EcoCAT	Ecological Compliance Assessment Tool
EDR	Environmental Data Resources, Inc.
EJ	Environmental Justice
ESA	Endangered Species Act
FC	Fish Consumption
FEMA	Federal Emergency Management Agency
FHWA	Federal Highway Administration
FIRM	Flood Insurance Rate Map
FONSI	Finding of No Significant Impact

FRA	Federal Railroad Administration
FTA	Federal Transit Administration
FTE	Full Time Equivalent
GHG	Greenhouse Gases
HARGIS	Historic and Architectural Resources Geographic Information System
IAIS	Iowa Interstate railroad
IDNR	Illinois Department of Natural Resources
IDOT	Illinois Department of Transportation
IEPA	Illinois Environmental Protection Agency
IHPA	Illinois Historic Preservation Agency
INAI	Illinois Natural Areas Inventory
ITA	Incidental Take Authorization
Leq	equivalent continuous sound level
Ldn	day-night average sound level
LUST	Leaking Underground Storage Tank
Lv	vibration velocity level
MOA	Memorandum of Agreement
MPH	miles per hour
MWD	Milwaukee District West Line
NEPA	National Environmental Policy Act of 1969
NFI	No Further Remediation
NHPA	National Historic Preservation Act
NPS	National Park Service
NRHP	National Register of Historic Places
NRI	Nationwide Rivers Inventory
NWI	National Wetland Inventory
OWR	Office of Water Resources
PC	Primary Contact
PCBs	Polychlorinated Biphenyls
PTC	Positive Train Control
RCRA	Resource Conservation and Recovery Act

RPP	Regional Permit Program
RTA	Regional Transportation Authority
SARA	Superfund Amendments and Reauthorization Act
SC	Secondary Contact
SESC	Sediment Erosion and Sediment Control
SFHA	Special Flood Hazard Areas
SHPO	State Historic Preservation Officer
SIP	State Implementation Plan
SRP	Site Remediation Program
TDM	Transportation Demand Management
TIP	Transportation Improvement Program
TOD	Transit-Oriented Development
TMDL	Total Maximum Daily Loads
TSS	Total Suspended Solids
Uniform Act	Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended
UPRR	Union Pacific Railroad
USACE	United States Army Corps of Engineers
USC	United States Code
USDOT	U.S. Department of Transportation
USFWS	US Fish & Wildlife Service
UST	Underground Storage Tank
USEPA	U.S. Environmental Protection Agency
VdB	root mean square vibration velocity in decibels relative to 1 microinch per second
WOUS	Waters of the United States

Executive Summary

The project undertaking described within this document is for the improvement of the Milwaukee West Line bridge over the Fox River in the City of Elgin, Kane County, Illinois. The project is known as the Milwaukee West Line Fox River Bridge Improvement Project (Metra Bridge Z-100) (the Project). The existing bridge provides a river crossing for the Metra Milwaukee West Line commuter rail service and the Canadian Pacific (CP) Railroad. The existing bridge carries one mainline track over the river and connects to two tracks beyond both ends of the bridge.

The Project is sponsored by Metra and the Federal Transit Administration (FTA). Metra proposes to improve the existing bridge to address its poor and deteriorated condition and to provide a second mainline track across the Fox River. Funding for the Project would come from a combination of local and federal sources, including Metra, the CP Railroad, and a U.S DOT TIGER VII grant. The federal funds from the TIGER Grant are administered by FTA.

The National Environmental Policy Act of 1969 (NEPA) mandates the consideration of environmental impacts before approval of any federally funded project that may have significant impacts on the environment or where impacts have not yet been determined (42 U.S.C. § 4321 et seq.). FTA and Metra prepared this Milwaukee West Line Fox River Bridge Improvement Project (Metra Bridge Z-100) **Environmental Assessment** (EA) in accordance with NEPA and other applicable regulations, including Section 106 of the National Historic Preservation Act (NHPA) and other agency regulations and guidelines.

This EA considers the impacts of implementing the proposed Project on the physical, human, and natural environments in the project area. FTA will issue a finding on the proposed Project based on the significance of impacts identified during the NEPA process. FTA's finding will guide future design and implementation of the Project.

The Project, located about 35 miles northwest of downtown Chicago, is a railroad bridge that carries 54 Metra commuter trains and up to 8 Canadian Pacific Railroad (CP) freight trains daily. The bridge (also known by its bridge number, Metra Bridge Z-100) was originally constructed in 1881. Although the structure has been regularly maintained, many components are substantially deteriorated and can no longer be economically repaired. The key components of the signal system controlling train movements across the bridge date from the 1950s. This signal equipment is not compliant with standards for Positive Train Control (PTC)¹. The 500-foot bridge is the only single-track segment on the Milwaukee West Line between Elgin and downtown Chicago, creating a bottleneck at both ends of the bridge. Trains must reduce speed to move through switches², and train schedules must be carefully coordinated to avoid trains arriving on the bridge at the same time. Any blockages on this single-track segment delays passenger and freight trains throughout

¹ Positive Train Control (PTC) is an advanced system designed to automatically stop a train before certain accidents occur. In particular, PTC is designed to prevent: Train-to-train collisions, derailments caused by excessive train speed, train movements through misaligned track switches, and incursions into established work zones.

² A railroad **switch** or turnout is a mechanical installation enabling trains to be guided from one track to another.

the corridor. Impacts on freight traffic may extend outside the Chicago region. To address these issues, Metra proposes to replace the bridge with a completely new structure, expanded to accommodate two tracks and controlled by a modern, PTC-compatible signal system. This Project would cost approximately \$34 million, of which the TIGER VII grant will provide \$14 million.

Taking the opportunity to double-track the new bridge would remove the delay-causing bottleneck. Train operations would be less vulnerable to blockages on the bridge. The second track would make it easier to schedule trains arriving at the bridge at the same time, adding flexibility and improving train on time performance. The availability of a second track would allow one track to be removed from service when maintenance is required, allowing work to be completed faster and more efficiently.

Supporting information on the Purpose and Need for this Project is provided in **Chapter 1**.

Alternatives Considered

The alternatives considered in this EA included the No Build Alternative and five Build Alternatives. They are described in further detail in **Chapter 2**. Reasonable build alternatives were assessed based on their ability to satisfy the Project's Purpose and Need. They were also assessed on their ability to avoid and minimize impacts to identified resources (human and natural). Performance of the viable alternatives was judged against each other, leading to selection of the Preferred Build Alternative.

The proposed Project evaluated in this EA was developed and evolved through a multiyear planning process that began in 2010, as further described in **Section 2.1**. This EA compares the No Build Alternative and Preferred Build Alternative for the Milwaukee West Line Fox River Bridge Improvement Project (Metra Bridge Z-100). The No Build Alternative is a required alternative as part of the NEPA environmental analysis and is used for comparison purposes to assess the relative benefits and impacts of implementing the Project (40 CFR 1502.14).

No Build Alternative

The No Build Alternative would maintain the existing single track bridge. Repair and maintenance on the existing bridge would continue. However, the nature and extent of the repairs would become greater, more frequent, and more costly. Detailed repairs (as specified by Metra Engineering) would include rehabilitation of the existing masonry piers, including repair of spalled/damaged stone, tuck pointing masonry joints, and pressure grouting to assure internal masonry joints are solid. The underwater concrete encasement (or covering) is exhibiting minor hairline cracks which would require future underwater inspections. The three western spans located under US Route 20 would be replaced in the near future due to accelerated corrosion caused by salt spray and drainage from the highway facility above. Structural steel would require rehabilitation where section loss (i.e. corrosion of the steel such that the beams/girders are weakened) is extensive and cross braced connections have failed or are near failure. Lastly, a crack

in the top flange has been identified which would require strengthening with additional installation of steel plates bolted to the top and bottom of the top flange. It is important to note that as a result of these required repairs, some of which are extensive, the No Build Alternative does not mean no construction would occur on the bridge.

Preferred Build Alternative

Major project elements of the Preferred Build Alternative are further detailed in Section 2.2 and include the following:

Structure

The Milwaukee West Line Fox River Bridge Improvement Project (Metra Bridge Z-100) would demolish the existing single-track bridge, including the bridge piers, and construct a double-track bridge built to modern design standards. One track would be rebuilt on the same alignment as the existing track, and a second track would be built immediately west, separated by a distance of 18 feet between the centers of the two tracks. The new bridge would have a ballasted deck, providing a superior ride, less expensive maintenance, and better protection from moisture and salt damage than the existing open-deck design. The existing stone masonry piers would be demolished and rebuilt using concrete, an economical alternative that provides similar strength and greater resistance to longitudinal forces. With two fewer piers than the existing bridge, the new three-pier bridge would reduce obstruction to water flow in the river below.

Signals

This Project would replace signal components near the bridge, including the wayside signal devices, switch machines, snow-melters and backup generator. A new interlocking would be installed, sheltered in two new relay houses on either side of the bridge. New underground cable for the signal system would be installed and would be compliant with PTC standards.

The preliminary construction cost estimate for the Preferred Build Alternative is based on conceptual engineering and would be refined through ongoing preliminary engineering. The anticipated capital cost of the Project is approximately \$34 million.

Environmental Impacts and Measures to Avoid or Minimize Harm

Potential adverse environmental impacts, best management practices, and mitigation measures are detailed in **Chapter 3** of the EA and are summarized in **Table ES-1**.

Table ES-1: Summary of Impacts, Benefits, and Measures to Avoid or Minimize Harm

Resource Area	No Build Alternative	Preferred Build Alternative
<p>Displacements and Relocations of Existing Uses <i>Section 3.1</i></p>	<p>No impacts.</p>	<p>Construction</p> <ul style="list-style-type: none"> A temporary construction easement of approximately 0.97 acres would be acquired from the Union Pacific Railroad. The easement would be limited to the unused land located between the Union Pacific Railroad (UPRR) and Metra Railroad tracks on the west side of the bridge, both north and south of the Fox River. There are no buildings or structures in the easement area and there would be no impacts to the UPRR tracks. <p>Permanent</p> <ul style="list-style-type: none"> Approximately 0.33 acres of land or permanent easement would be acquired from the UPRR. Land acquisition would be limited to the unused land located between the UPRR and Metra Railroad tracks on the west side of the bridge, both north and south of the Fox River. There are no buildings or structures in the easement areas and there would be no impacts to the UPRR tracks. <p>Measures to Avoid or Minimize Harm</p> <ul style="list-style-type: none"> Just compensation for easements, measured by the fair market value of the property, as determined by Metra through an appraisal process, would be provided to the affected property owner.
<p>Neighborhoods, Communities, and Businesses <i>Section 3.2</i></p>	<p>Minor temporary construction impacts would include noise, vibration, dust, temporary utility disruption, negative visual and aesthetic changes from demolition and construction, construction vehicle emissions, and truck</p>	<p>Construction</p> <ul style="list-style-type: none"> Minor temporary construction impacts would include noise, vibration, dust, temporary utility disruption, negative visual and aesthetic changes from demolition and construction, construction vehicle emissions, and truck traffic throughout the project area. This would affect a larger area than under the No Build Alternative. Improvements would be made to the grade

Resource Area	No Build Alternative	Preferred Build Alternative
	<p>traffic throughout the project area. No permanent impacts are expected.</p>	<p>crossing at Elgin Avenue.</p> <p>Permanent</p> <ul style="list-style-type: none"> No permanent impacts are expected. <p>Measures to Avoid or Minimize Harm</p> <ul style="list-style-type: none"> A temporary track crossing would be provided to serve The Alphabet Group (300 Elgin Ave., Elgin, IL) while improvements are made to the crossing at Elgin Avenue near the south project limit.
<p>Historic and Archaeological Resources (Section 106 Consultation) <i>Section 3.3</i></p>	<p>No impacts.</p>	<p>No impacts.</p>
<p>Water Resources <i>Section 3.4</i></p>	<p>Impacts for bridge repair are similar to the Preferred Build Alternative. The No Build Alternative involves work in the Fox River to maintain the pier encasements and repair the three western most spans. The work would also require the use of cofferdams and causeways to construct the improvements.</p>	<p>Construction</p> <ul style="list-style-type: none"> Temporary impacts to water quality related to cofferdams and causeways required to construct the bridge. Sediment within the Fox River is expected to be disturbed temporarily due to construction of the piers or through the construction of a causeway if required. <p>Permanent</p> <ul style="list-style-type: none"> No permanent impacts. <p>Measures to Avoid or Minimize Harm</p> <ul style="list-style-type: none"> Best Management Practices (BMPs) including dewatering, silt curtain, and working “in the dry” inside a cofferdam or causeway would limit the potential for sediment to be disturbed and released downstream.
<p>Flooding <i>Section 3.5</i></p>	<p>Temporary placement of fill within the floodway for a temporary causeway.</p> <p>The No Build Alternative would not require permanent fill within the floodway or floodplain.</p>	<p>Construction</p> <ul style="list-style-type: none"> Temporary placement of fill within the floodway for a temporary causeway. <p>Permanent</p> <ul style="list-style-type: none"> Approximately 4,392 cubic feet of concrete would be placed in the floodway below the 10-year floodway elevation for piers and abutments. Approximately 3,096 cubic feet of concrete would be placed between the 10-year and 100-year floodway for piers and abutments.



Resource Area	No Build Alternative	Preferred Build Alternative
		<p>Measures to Avoid or Minimize Harm</p> <ul style="list-style-type: none"> • Compensatory storage for floodway fill would be located on the west bank of the Fox River, adjacent to the existing Metra and Union Pacific Railroad (UPRR) bridges and at the east abutment of the bridge. A total of 4,999 cubic feet of compensatory storage would be created below the 10-year floodway elevation, creating an excess of approximately 608 cubic feet of compensatory storage. A total of 3,419 cubic feet of compensatory storage would be created between the 10-year and 100-year floodway, creating an excess of 323 cubic feet of compensatory storage.
<p>Biological Resources <i>Section 3.6</i></p>	<p>No permanent impacts. Temporary impacts may result from the minor rehabilitation or replacement of existing masonry piers, structural steel, and three western spans required under the No Build Alternative. Temporary impacts may also result from tree trimming/removal and the use of causeways or cofferdams for work in the river to repair the existing bridge piers and remove and replace bridge spans. An Incidental Take Authorization would be required from the IDNR.</p>	<p>Construction</p> <ul style="list-style-type: none"> • Construction of the Preferred Build Alternative would not result in adverse impacts to biological resources. • Impacts may result from tree trimming/removal and the use of causeways or cofferdams for work in the river to demolish the existing bridge and construct the new bridge. <p>Permanent</p> <ul style="list-style-type: none"> • Permanent impacts to threatened and endangered species are not anticipated. <p>Measures to Avoid or Minimize Harm</p> <ul style="list-style-type: none"> • The implementation of BMPs and an Incidental Take Authorization (ITA) for the spike mussel. The survey and relocation of any spike mussels found within the project area prior to construction would result in no adverse impacts from construction activities on the spike mussel.
<p>Noise <i>Section 3.7</i></p>	<p>The No Build Alternative would result in minor temporary impacts on the surrounding neighborhoods due to construction activities. Temporary construction noise impacts would be due to demolition and construction, and</p>	<p>Metra identified three noise-sensitive clusters within the project area.</p> <p>Construction</p> <ul style="list-style-type: none"> • Impacts from construction activities would be temporary in nature. • Temporary construction noise impacts would be due to demolition and construction, and construction vehicles. Truck traffic would be primarily present along major roads near the

Resource Area	No Build Alternative	Preferred Build Alternative
	<p>construction vehicles. Truck traffic would be primarily present along major roads near the project area and would use a defined access path to reach the project limits, likely along the existing right-of-way.</p>	<p>project area and would use a defined access path to reach the project limits, likely along the existing right-of-way.</p> <p>Permanent</p> <ul style="list-style-type: none"> • There would be no noise impacts associated with the proposed improvement. • The projected overall build noise levels would not change from the existing overall noise levels at any of the receptor locations since the number of trains is not anticipated to increase. <p>Measures to Avoid or Minimize Harm</p> <ul style="list-style-type: none"> • As there are no impacts expected from the Project, no mitigation would be required.
<p>Vibration <i>Section 3.8</i></p>	<p>No impacts.</p>	<p>Metra identified one vibration-sensitive cluster within the project area.</p> <p>Construction</p> <ul style="list-style-type: none"> • Construction vibration levels would not exceed the vibration risk of damage criteria at any receivers. <p>Permanent</p> <ul style="list-style-type: none"> • There would be no vibration impacts resulting from the proposed improvement since the number of trains is not anticipated to increase. <p>Measures to Avoid or Minimize Harm</p> <ul style="list-style-type: none"> • As there would be no impacts expected from the Project, no mitigation would be required.
<p>Hazardous Materials <i>Section 3.9</i></p>	<p>There would be the potential to encounter hazardous materials during construction. Additional environmental investigation would be needed prior to the start of construction to determine suitable BMPs to reduce risk.</p>	<p>Construction</p> <ul style="list-style-type: none"> • There would be the potential to encounter hazardous materials during construction. BMPs would be followed to reduce risk. <p>Permanent</p> <ul style="list-style-type: none"> • There would be no permanent impacts expected from the proposed improvement. <p>Measures to Avoid or Minimize Harm</p> <ul style="list-style-type: none"> • Additional environmental investigation would be needed prior to the start of construction to determine suitable BMPs to reduce risk in areas of potential hazardous waste.
<p>Environmental Justice <i>Section 3.10</i></p>	<p>No impacts.</p>	<p>No disproportionately high and adverse construction or permanent impacts are anticipated as a result of the Project.</p>



Resource Area	No Build Alternative	Preferred Build Alternative
Indirect and Cumulative <i>Section 3.11</i>	No impacts.	The Preferred Build Alternative would be expected to have no indirect impacts with the exception of incremental beneficial impacts on air quality. No cumulative impacts would be expected to land use, transportation or other resources in the project area.
Resources with Limited or No Impacts <i>Section 3.12</i>	No impacts.	The Preferred Build Alternative would have limited or no impacts on the following resource areas: transportation, air quality, land use and economic development, navigable waterways and coastal zones geology and soils, energy, safety and security, and visual and aesthetic conditions.
Section 4(f) Resources <i>Section 3.13</i>	No impacts.	No impacts.

Public Input Requested

A 30-day comment period will be established to receive formal comments. A copy of the EA will be available on the Metra website at <https://metrarail.com/about-metra/reports-documents/project-studies/current-project-studies/z-100-ea> in pdf format. Hard copies of the EA will be available for review during the public review period at:

Metra, 547 W. Jackson Boulevard, Chicago, IL 60661

Gail Borden Public Library, Information Desk, 2nd Floor, 270 N. Grove Avenue, Elgin, IL 60120

A public hearing will be scheduled to solicit comments from agencies and the public about findings presented in the EA. The hearing will be conducted in an open house format. Comments on the EA may be made verbally to a court reporter or in writing during the hearing. The location of the hearing will be compliant with the Americans with Disabilities Act (ADA) and accessible by public transportation. All substantive comments received during the hearing and the 30-day public comment period will be addressed, and will be incorporated into the final NEPA decision document.

Written comments will also be accepted at any time during the public comment period via U.S. mail to:

Metra
Grant Management & Accounting, 11th Floor
547 W. Jackson Boulevard
Chicago, IL 60661
Attn: Milwaukee West Line Fox River Bridge Improvement Project (Metra Bridge Z-100)

Comments will also be accepted at any time during the public comment period via email to:
ProjectZ100NEPA@metrarr.com

Chapter 1 Purpose and Need

1.1 Introduction

The Project undertaking described within this document is for the improvement of the Milwaukee West Line bridge over the Fox River in the City of Elgin, Kane County, Illinois. The Metra Milwaukee West Line is one of 11 commuter rail lines that Metra operates in northeastern Illinois (See **Figure 1-1**). The Milwaukee West Line is 39.8 miles long and operates between the Big Timber Road Station in the City of Elgin and the Chicago Union Station in the City of Chicago. Fifty-four Metra Milwaukee West Line commuter rail trains operate over the bridge daily carrying over 6.8 million passengers per year. In addition to commuter rail trains, up to eight Canadian Pacific (CP) freight trains use the bridge daily. The CP Railroad has trackage rights on this portion of the Milwaukee District West Line. CP uses this portion of track to connect its yard in Bensenville, Illinois to Northern Iowa and Kansas City.

The Project is known as the **Milwaukee West Line Fox River Bridge Improvement Project (Metra Bridge Z-100)** (the Project). The existing bridge provides a river crossing for the Metra Milwaukee West Line commuter rail service and the CP Railroad. The existing bridge carries one mainline track over the river and connects to two tracks beyond both ends of the bridge.

The Project is sponsored by Metra and the Federal Transit Administration (FTA). Metra proposes to improve the existing bridge to address its poor and deteriorated condition and to provide a second mainline track across the Fox River. Funding for the project will come from a combination of local and federal sources, including Metra, the CP Railroad, and a TIGER Discretionary Grant. The federal funds from the TIGER Grant are administered by FTA.

The National Environmental Policy Act of 1969 (NEPA) mandates the consideration of environmental impacts before approval of any federally funded project that may have significant impacts on the environment or where impacts have not yet been determined (42 U.S.C. § 4321 et seq.). FTA and Metra prepared this Milwaukee West Line Fox River Bridge Improvement Project (Metra Bridge Z-100) Environmental Assessment (EA) in accordance with NEPA and other applicable regulations, including Section 106 of the National Historic Preservation Act (NHPA), and other agency regulations and guidelines.

The EA considers the impacts of implementing the proposed Project on the physical, human, and natural environments in the project area. FTA will issue a finding on the proposed Project based on the significance of impacts identified during the NEPA process. FTA's finding will guide future design and implementation of the Project.

1.2 Project Background

1.2.1 Project Limits and Project Area

Metra Bridge Z-100 is located approximately 35 miles northwest of downtown Chicago. It is a single-track, 12-foot wide, 504-foot long railroad bridge structure over the Fox River. The Fox River is a tributary of the Illinois River, flowing from southeastern Wisconsin to Ottawa, Illinois³. The Project is located in Township 41N, Range 8E, in Section 24 within the City of Elgin, Kane County, Illinois. Approximately 50 feet west of (downstream) and parallel to the project bridge, is another railroad bridge which is owned and operated by the Union Pacific Railroad (UPRR). US Route 20 is located adjacent to and over the two railroad bridges (**See Figures 1-2 and 1-3**). The *project limits* are along the existing railroad corridor right-of-way (ROW) and extend from just south of the National Street Station to just north of Elgin Boulevard. The *project area* covers a broader area, which includes locations beyond the existing ROW and extends into the surrounding community.

1.2.2 Project History

The existing, single-track bridge was constructed in 1881, consisting of six steel spans resting on masonry abutments and five piers. Extensive modifications to the bridge were made in 1905 and 1926. Three of the original spans were replaced in 1905, and the other three were replaced in 1926. The piers and abutments date from the original 1881 construction, with cast-in-place concrete modifications as required to accommodate the new span beams from 1905 and 1926. The existing bridge is owned and maintained by Metra.

Due to the age of the bridge, visual and hands-on inspections are conducted on an annual basis. Given the physical age and condition of the bridge, planning studies have been conducted, including a 2009 bridge inspection and 2009 underwater bridge inspection study. The intent of the studies was to determine a course of action to improve the condition of the bridge in a cost-effective manner. The conclusion was the existing bridge must be significantly repaired, or replaced, in order to address the structural and operational inadequacies of this single-track bridge. In addition, the existing bridge is currently the only single-track segment on the Milwaukee West Line. The existing bridge crossing can accommodate only one train at a time – constraining the capacity and operational flexibility of the line. The existing bridge condition and the single-track bottleneck at either end of the bridge prioritized the Project.

Design and NEPA coordination for the new double-track bridge began in 2010. Due to a lack of construction funding, design and NEPA work for the new bridge progressed slowly. Currently,

³ The Fox River enters Illinois in McHenry County where it flows south, eventually joining the Illinois River at Ottawa, IL. Major Illinois towns and communities that are on the Fox River include (from north to south) Elgin, Aurora, Oswego, Yorkville, and Ottawa. Although the river has a number of dams along it, it is navigable and is used for recreational boating and fishing.

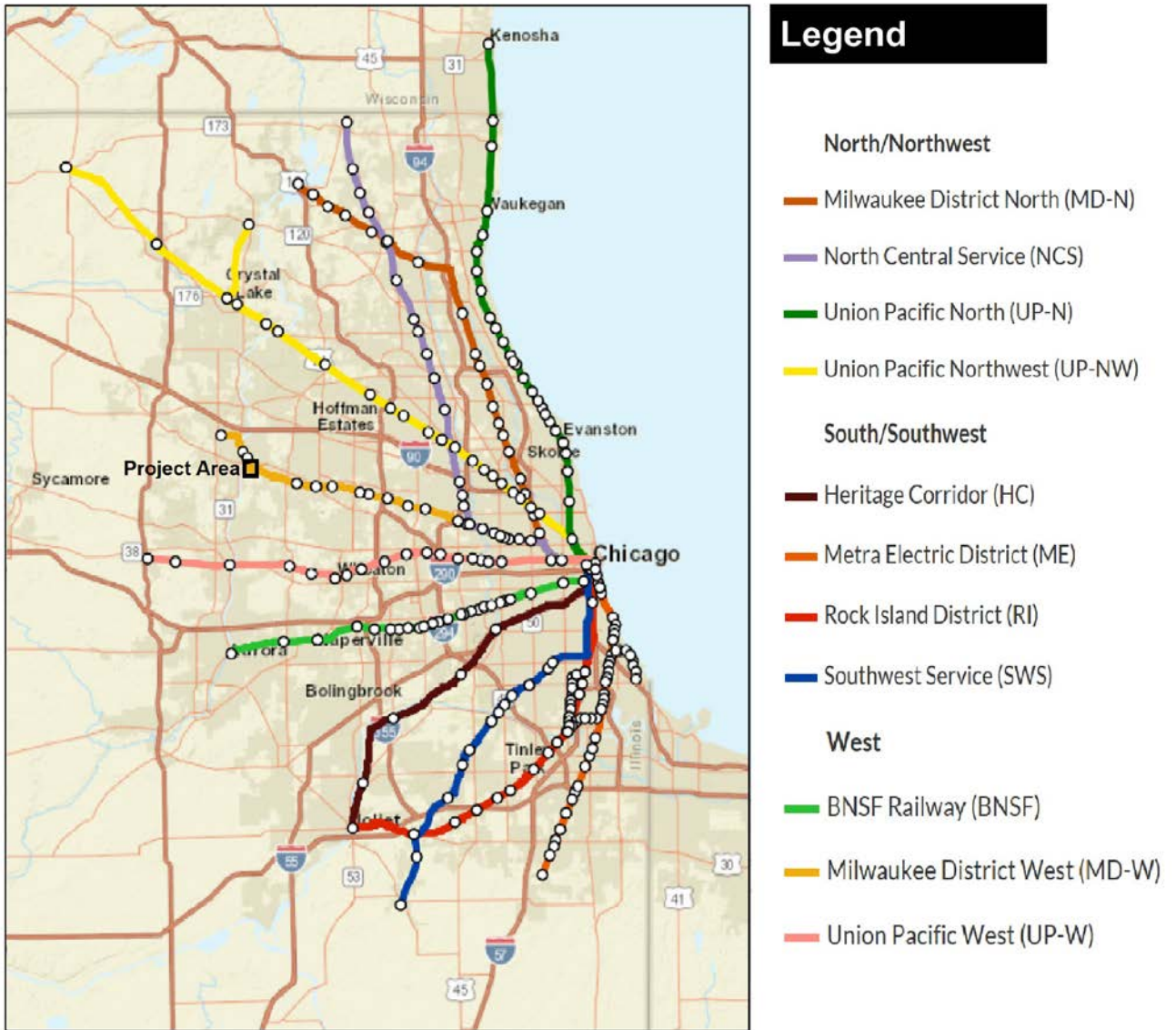
design is approximately 30% complete. Since 2010, Metra prepared multiple TIGER Discretionary Grant Applications for this Project as replacing the single-track bottleneck with a double-track bridge is one of several pre-conditions needed for future core capacity improvements. The Project has received \$14 million in TIGER grant funding. Now, Metra is moving forward to finalize the design and the Environmental Assessment (EA) approvals required to begin construction.

1.2.3 Bridge Condition

Based on field inspections conducted in 2009, the bridge is in overall poor condition. This means that some bridge elements have advanced deficiencies and that these weaknesses affect the overall structural capacity and serviceability of the bridge. The bridge's steel spans, which date from 1905 and 1926, have been significantly corroded by moisture and salt. In addition, the masonry piers and abutments need to be strengthened to bring them into compliance with current railroad design criteria regarding resistance to forces generated by train movement on the structure. The bridge has reached the point where further repairs would not be economically feasible. Each year, speed restrictions due to existing switches⁴, train delays, and signal problems at the bridge add 36,000 passenger hours to travel times of Metra riders. If the bridge structure and signal equipment are allowed to continue to degrade, delays would continue to increase in frequency and duration.

Figure 1-1 through Figure 1-3 show maps of the Metra System and Project Area.

⁴ A railroad **switch** or turnout is a mechanical installation enabling trains to be guided from one track to another.



Source: Metrarail.com

Figure 1-1: Metra System Map

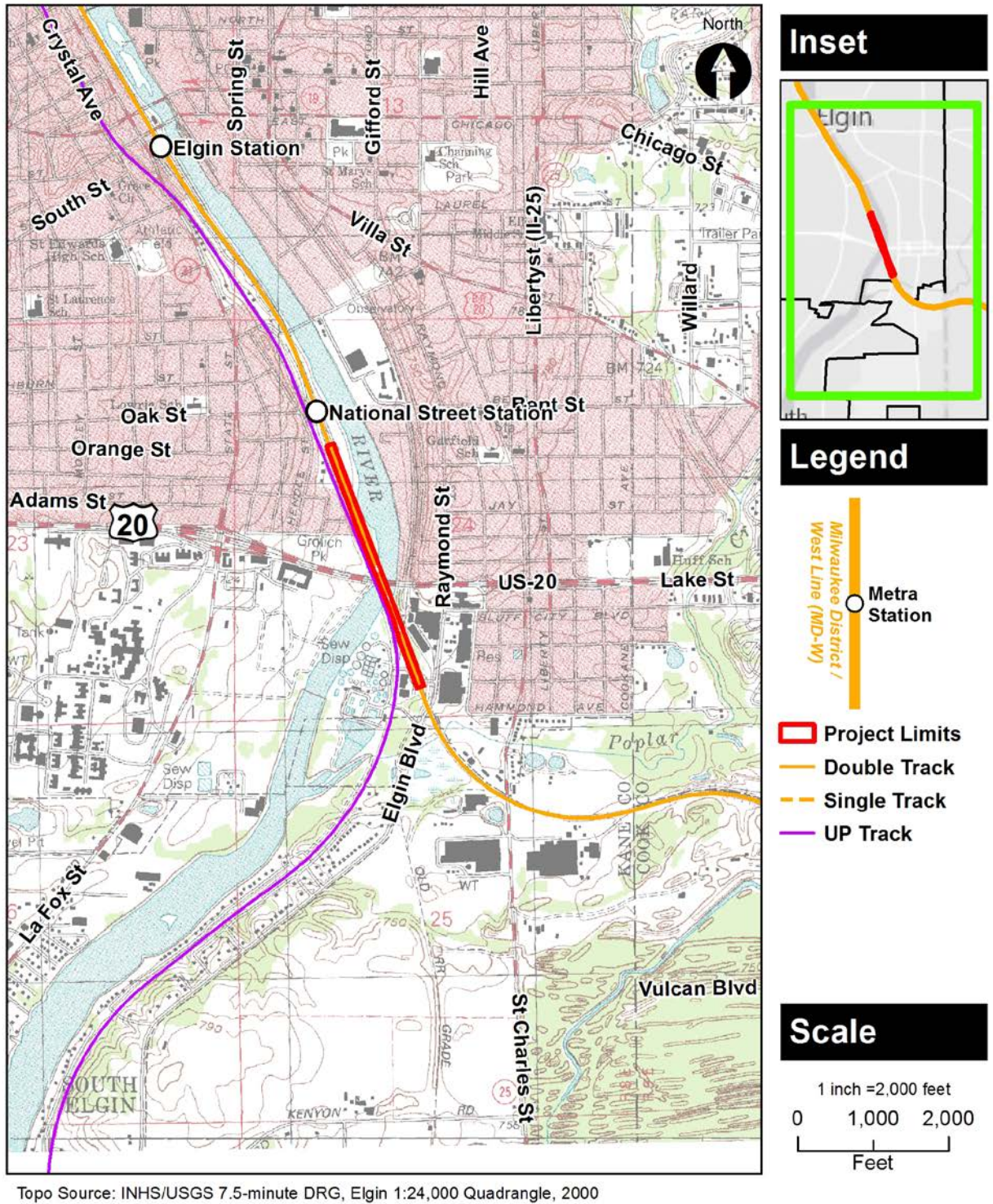
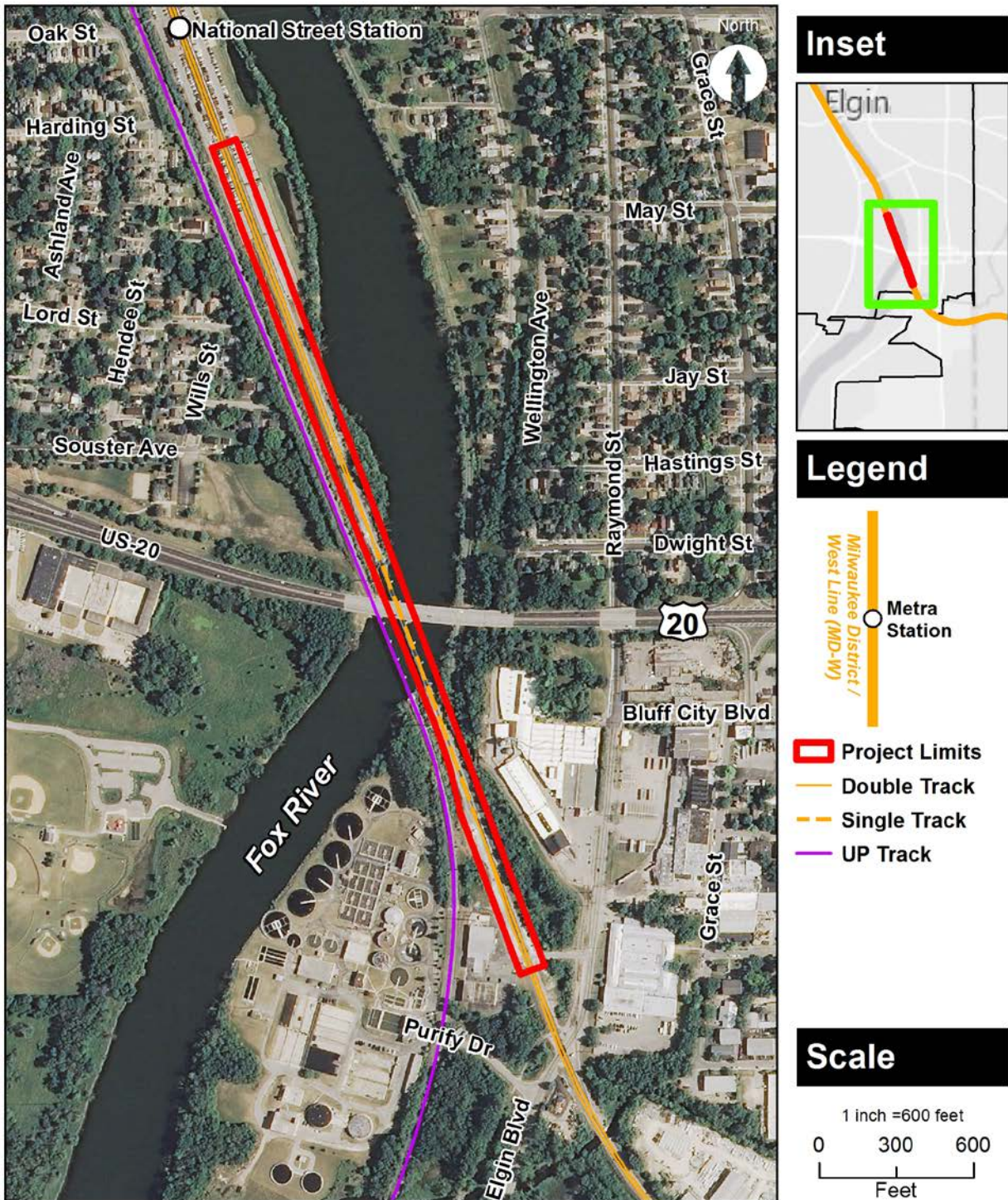


Figure 1-2: Project Area Map



Aerial Source: ESRI Online World Imagery

Figure 1-3: Project Limits Map

1.2.4 Surrounding Community and Zoning

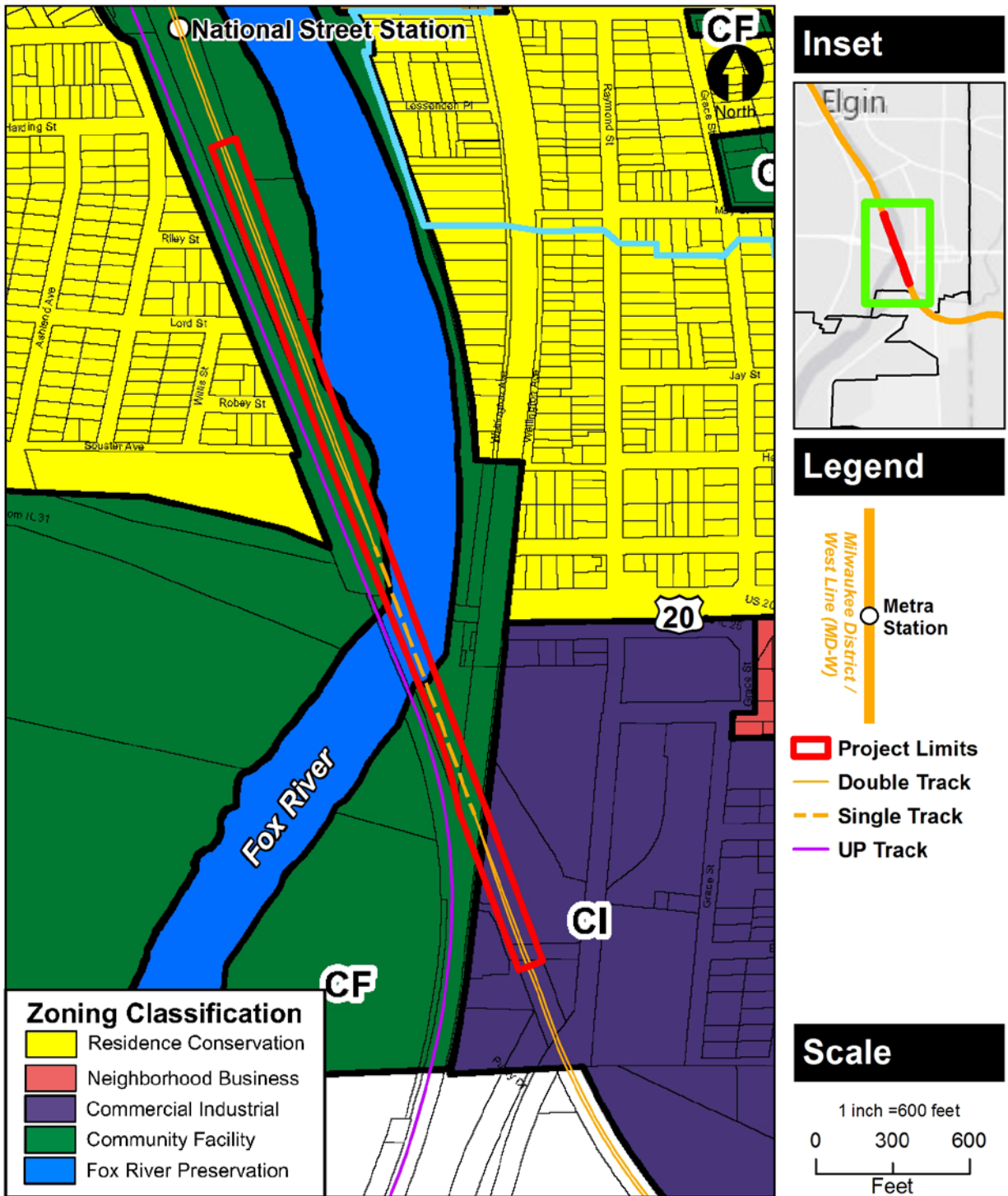
The project area is currently zoned as CF – “Community Facility” on the 2010 City of Elgin Zoning Map. The following enumerated “land uses” are the most common land uses allowed as a “permitted use” or as a “conditional use” in the CF Community Facility District. Permitted uses include: parks and recreation facilities; public buildings, such as libraries, fire stations, police stations, and government facilities; cemeteries, churches, hospitals, museums, nursing homes, schools (including colleges); and post offices, broadcasting antennas and transmitters, wind energy conversion towers. Conditional uses include: daycares; railroad tracks, railroad bridges, and railroad stations in use as a public transit facility; arenas and sports stadiums; job training and vocational facilities; sporting and recreational camps; airports; electrical power generation; water and wastewater works; garbage dumps; and school bus operators offices. The Project is considered to be a railroad track and a railroad bridge (both conditional uses). A zoning change is not expected, as the railroad bridge would remain a public transit facility.

A small portion of the southern end of the Project area is currently zoned as CI – “Commercial Industrial” on the 2010 City of Elgin Zoning Map. This zoning district is noted as the least restrictive type of zoning, and allows railroad tracks as a conditional use. The project area zoning map is provided below (**See Figure 1-4**).

The Project limits are already occupied by the existing bridge and track alignment. However, replacement and expansion of the bridge would require the acquisition of land or a permanent easement of approximately one-third of an acre of land along the west side of the existing railroad right-of-way. A temporary easement would be necessary to access an additional acre west of the Metra tracks during construction. This land is currently owned by the Union Pacific Railroad (UPRR).

1.2.5 Regional Planning

The Milwaukee West Line Fox River Bridge Improvement Project is consistent with the goals and objectives of the regional Long-Range Transportation Plan (Chicago Metropolitan Agency for Planning’s GO TO 2040 Plan), the region’s Congestion Management Process (CMP) and Transportation Demand Management (TDM) strategies. GO TO 2040 outlines a series of recommendations for improving regional mobility that are consistent with the proposed project and which the proposed project supports. These recommendations for the regional transit system include making strategic transportation investments that increase the region’s commitment to public transit and prioritizing modernization of existing significant assets over system expansion plans. The region’s CMP and associated TDM strategies seek to reduce demand for single-occupancy vehicle use on the regional transportation network. This Project is consistent with these approaches and provides needed maintenance and modernization of existing public transit infrastructure to support more efficient ways to move a greater number of people throughout the region.



Zoning Source: City of Elgin, Department of Community Development

Figure 1-4: Project Zoning Map

1.3 Purpose of this Project

The purpose of the Project is to provide an improved railroad bridge for the Metra Milwaukee West Line crossing of the Fox River – replacing the existing bridge and adding a second mainline crossing. This would be accomplished by addressing the facility condition and improving reliability and operations by improving efficiency, flexibility and system continuity.

Successful completion of the Project would eliminate an existing bottleneck at the bridge crossing, increase the reliability of the Milwaukee West Line service, accommodate future passenger ridership and freight operations, and allow better operational flexibility with a second mainline track. A second mainline track at the bridge crossing would eliminate the existing switches, which currently restrict train speed at this location.

Other benefits include reduced passenger and freight train delays, travel times and costs. When the Project is complete, the increase in the number of riders, the reduction in train idling time and the resulting increase in fuel efficiency (by running trains at the optimum speed) would provide environmental benefits for the region. The Project would bring this bridge into a state of good repair with a minimum of disruptions to freight and passenger rail operations, result in the least possible environmental impact, and keep any track alignment changes to a minimum.

1.4 Needs to be Addressed

1.4.1 Improve Bridge Condition

The existing bridge shows significant deterioration, and is overall in poor condition. Spray from deicing salt on the US Route 20 highway bridge above has contributed to steel corrosion on the railroad bridge.

Recent inspections have found that some of the beam flanges have lost up to 25% of their steel from rust and corrosion. There is a crack in the top flange, and holes have rusted through the beam webs. In the past few years, structural steel on the bridge has required repairs on several occasions. However, even with the repairs the bridge is not compliant with current design standards and requirements. As originally designed, it is estimated that the existing bridge would

The basic parts of the structural support system of the existing bridge are provided by two large steel I-beams, also called girders, which run parallel to and support the tracks above. The parts of an I-beam (or girder) include **the web** (the vertical part of the “I”), and **the flanges** (the flat part at the top and bottom of the “I”). The beams span the river and each beam end “sits” on a pier. At the ends of the bridge, the beam ends “sit” on an abutment. The existing bridge has six beam spans, five piers and an abutment at each end.

rate at about an E-48 loading on the Cooper System, the scale used by railway bridge engineers to indicate the maximum allowable load the bridge would carry. Currently, AREMA⁵ recommends an E-80 loading as the design standard on most railroad mainlines. The current load carrying capacity requirement is 67% greater than the existing bridge’s capacity in a like-new condition.

The existing piers and abutments are made with stone masonry and were constructed in 1881. In 1926 and 1941, concrete encasements, or coverings, were added to protect the stone masonry of the piers where it is below the river water line. Though tests show that the piers are in “fair” to “good” condition, they do not meet current AREMA standards for resisting the back and forth stresses generated by the braking and acceleration of trains on the bridge.

1.4.2 Improve Reliability and Operations

A single-tracked bridge is not typical of most mainline river crossings. Most new rail crossings provide two mainlines, or double-tracked bridges, which allow for greater flexibility in operations and maintenance activities, and provide improved capacity and reliability along the line. Additionally, there are already two mainline tracks to the north and south of the existing bridge. Metra needs a two-track bridge over the Fox River in order to increase operational efficiency and reduce delays. With a single-track bridge, trains in opposite directions need to wait for the other train to pass.

Fifty-four Metra trains and up to eight CP freight trains per day cross the Fox River on the existing bridge. With only one track across the bridge, train service schedules are unreliable. The current

⁵ American Railway Engineering and Maintenance-of-Way Association (AREMA)

demand cannot be met without delays. The unreliable train service schedules and delays result in wasted fuel and additional emissions. Without double-tracking the bridge and removing the existing switches, current speed restrictions could increase in severity and a critical point on the line would continue to be vulnerable to blockage. The frequency of outages would be reduced on a rebuilt, double-tracked bridge, decreasing the need for trains to idle while waiting for their turn to use the bridge, or for freight traffic to travel via an alternate, more circuitous route—also reducing unnecessary fuel use and emissions.

Increasingly frequent delays and unreliable service schedules would discourage riders from using passenger rail as an alternative to the automobile, and businesses and employees in the Milwaukee West Line corridor would lose much of the economic benefit currently provided by Metra service. Failure to complete this Project may damage the ability of communities in the corridor to attract new investment in transit-oriented development (TOD), and diminish the value of existing station-area investment. In addition, track and signal maintenance projects on the existing bridge do not have the flexibility of those on a two track bridge. Construction activities must take place during gaps between trains, since the bridge has only one track and cannot be removed from service. The bridge must be cleared of workers and equipment each time a train passes. With so much time spent moving workers and supplies on and off the track, a project that would be completed in a few hours of uninterrupted work can extend to several days. The availability of a second track would provide longer work windows, resulting in reduced labor costs and fewer service disruptions. The existing bridge is a critical link between CP's Bensenville classification yard near Chicago and western portions of their network. The bridge has been used on occasion in the past as a detour route for Iowa Interstate (IAIS) and Union Pacific Railroad (UPRR) trains when there have been disruptions to their Iowa and Western Illinois operations. However, with continued use of the aging bridge, the burden caused by outages due to maintenance activities would grow, lengthening travel times and increasing the frequency of delays. Costs to businesses, shippers, and consumers would increase, and freight users of the bridge may eventually need to utilize an alternate route, which would add an estimated 116 miles per train or more, depending on the specific origin/destination of the train.

The bridge currently links many reverse commuters with jobs in Elgin. Nearly 20% of passengers using Metra's Chicago Street Station in Elgin during the morning peak period alight rather than board, as commuters travel to Elgin municipal offices, the Grand Victoria Casino, and other nearby employers. Dependable transportation links between jobs and qualified workers are particularly important to the City of Elgin, which qualifies under federal guidelines as an Economically Distressed Area⁶. The population in the Milwaukee West Line corridor is projected to increase by 260,000 residents between 2010 and 2040, and nearly 200,000 jobs are expected to be added during the same period. Double tracking the river crossing equates to quicker, more

⁶ The unemployment rate in the City of Elgin has been at least 1% greater than the national average unemployment rate during the 24-month period from January 2013-December 2014, and Elgin meets the unemployment rate threshold required to document economic distress, as set forth in 42 US Code, § 3161.

reliable trips, which in turn could attract more ridership to meet growing demand in the Milwaukee West Line corridor.

1.5 Organization of the Document

NEPA documents such as this EA must provide sufficient technical details to meet a range of legal requirements and are required to be organized in a specific way. **Figure 1-5** provides an overview of the chapters and the major topics covered in this document for ease in navigating through the document.

<p>Chapter 1</p> <p>Purpose and Need</p>	<p>This chapter is the foundation of the document. It introduces the project, provides background information on the project, and provides information on why the project is proposed and important.</p>
<p>Chapter 2</p> <p>Alternatives Considered</p>	<p>This chapter reviews the planning process and alternatives considered in developing the proposed project and describes the alternatives under further consideration in this Environmental Assessment.</p>
<p>Chapter 3</p> <p>Environmental Resources, Impacts, and Mitigation Measures</p>	<p>This chapter presents the potential for impacts on the transportation network and discusses the social, economic, and environmental resources that could be affected by the construction and implementation of the project. This chapter also discusses measures to avoid or minimize those impacts.</p>
<p>Chapter 4</p> <p>Public and Agency Coordination</p>	<p>This chapter discusses the processes for public involvement and agency coordination. The chapter addresses the public comments and suggestions.</p>

Figure 1-5: Environmental Assessment Document Organization

Chapter 2 Alternatives Considered

This chapter summarizes the alternatives considered in this EA, which include the No Build Alternative and five Build Alternatives. Reasonable alternatives are assessed based on their ability to satisfy the Project’s Purpose and Need. They are also assessed based on their ability to avoid and minimize impacts to identified resources (human and natural). Performance of the viable alternatives is judged against each other leading to selection of the Preferred Build Alternative. The No Build Alternative and the Preferred Build Alternative are evaluated and documented in **Chapter 3**.

2.1 Alternatives Development Process

The project limits provide challenges to the development of viable build alternatives. The existing bridge is parallel to the UPRR bridge, located only 50 feet downstream. Both rail bridges are spanned overhead by US Route 20 (See photo to the right). The close proximity of the bridge piers to each other, the vertical clearance under US Route 20, and the low waterway clearance must be taken into consideration in the development of any alternative.

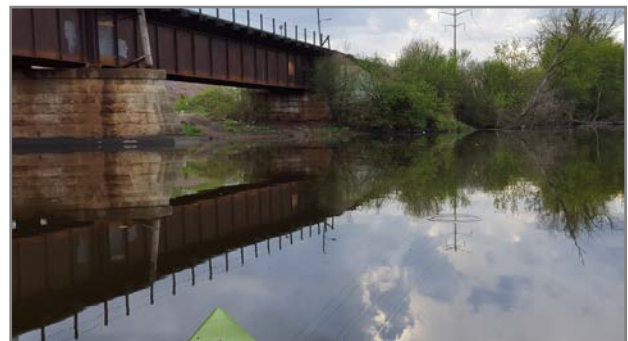


Looking North: The existing Metra bridge is on the right, the UPRR bridge is on the left, with the US Route 20 bridge overhead.

Existing overhead utilities also constrain potential alternatives. Measures must be considered to avoid the utility facilities or incur expensive relocation costs (See photo below to the left). Alternatives must minimize fill in the river floodway and floodplain, and avoid or minimize new fill from embankment slopes and new bridge abutments within the adjacent Fox River floodplain and floodway. Measures to avoid and limit fill in these sensitive areas are important to reduce impacts (see photo below to the right).



Looking South: Overhead high-tension powerline towers are located in between the two rail bridges.



Looking West: The north end of the bridge is in close proximity to the river’s edge at the northeast quadrant of the bridge, near the bend in the river.

Metra considered a number of alternatives to address the replacement of the existing bridge.

The No Build Alternative and five Build Alternatives were evaluated for the bridge replacement:

1. No Build: Continue to provide extensive maintenance measures and repairs
2. Alternative 1A: Construct a new double-track bridge on new alignment to the east
3. Alternative 1B: Construct a new double-track bridge on new alignment to the west
4. Alternative 2: Construct a new single-track bridge on the existing alignment
5. Alternative 3: Construct a new single-track bridge on an upstream alignment
6. Alternative 4: Construct a new double-track bridge on existing and downstream alignment (Preferred Build Alternative)

Each alternative is described below. The findings of the alternatives analysis are summarized in the Alternatives Comparison Matrix in **Table 2-1** and each alternative is shown in a figure after each description of an alternative. More detailed design drawings are included in **Appendix A**. The detailed design drawings include plan and profile sheets, bridge plan and elevation sheets, and bridge cross sections.

For purposes of discussion of the following alternatives, the orientation of the existing bridge, Milwaukee West Line and existing trackage is assumed to be in a north-south direction. US Route 20 and the Fox River are assumed to be in an east-west orientation.

2.1.1 No Build Alternative

The No Build Alternative maintains the Bridge Z-100 as it currently exists. Repair and maintenance on the existing bridge would continue. However, the nature and extent of the repairs would become greater, more frequent, and costly. Detailed repairs (as specified by Metra Engineering) would include rehabilitation of the existing masonry piers, including repair of spalled/damaged stone, tuck pointing masonry joints, and pressure grouting to assure internal masonry joints are solid. The underwater concrete encasement (or covering) is exhibiting minor hairline cracks which would require future underwater inspections. The three western spans located under US Route 20 would be replaced in the near future due to accelerated steel corrosion caused by salt spray and drainage from the highway facility above. Structural steel would require rehabilitation where section loss (i.e. corrosion of the steel such that the beams or girders are weakened) is extensive and cross braced connections have failed or are near failure. Lastly, a crack in the top flange has been identified which would require strengthening with additional installation of steel plates bolted to the top and bottom of the top flange. It is important to note that as a result of these required repairs, some of which are extensive, the No Build Alternative does not mean no construction would occur on the existing bridge.

The No Build Alternative would be the least environmentally disruptive alternative; however, the No Build Alternative does not meet the Project's Purpose and Need. If the bridge is not replaced, the current bridge would continue to deteriorate. The condition of the bridge has reached a point where further repairs are not economically feasible. If the bridge is not replaced, repairs—such as those required in 2010 to address deteriorated girder webs and seat bearings—would have to be made more frequently. Piecemeal repairs, especially unplanned projects, are an inefficient use of labor and may disrupt train schedules. Without replacement and upgrade of the existing bridge, speed restrictions could be implemented and a critical point on the line would continue to be vulnerable to blockage. Increasingly frequent delays and unreliable service schedules would discourage riders from using passenger rail as an alternative to the automobile, and businesses and employees in the Milwaukee West Line corridor would lose much of the economic benefit from the nearby Metra service.

The condition of the existing bridge cannot be entirely brought into current, modern-day design requirements. Remaining a single-tracked crossing does not address the operational needs to improve reliability, efficiency and flexibility of the service and system for commuter and freight rail. Lastly, it does not address the need for system continuity, as it is still a bottleneck over the river. Under this alternative the existing single-track bridge would remain the only single-track segment on the double and triple track mainline alignment between Elgin and Chicago. For these reasons, the No Build Alternative does not meet the Purpose and Need, and is not considered a viable alternative for the Project.

The No Build Alternative is a required alternative as part of the NEPA environmental analysis and is used for comparison purposes to assess the relative benefits and impacts of implementing the Project (40 CFR 1502.14). This alternative would maintain the status quo and is carried through to the end of the study as a means of comparison.

2.1.2 Alternatives 1A and 1B: Construct a New Double-Track Bridge on New Alignment Under Alternatives 1A and 1B, Metra would remove the existing bridge and construct a new double-track bridge on a new alignment east (upstream) or west (downstream) of the existing bridge, respectively. The proposed bridge would have five spans resting on four cast-in-place concrete piers and abutments. This would require a double shift in the track alignment of both mainline tracks north and south of the bridge to connect back to the existing double mainline tracks. The proposed curvature in the mainline track within the double shifts would incur more wear and tear on equipment wheels, and require increased track and rolling stock⁷ wheel maintenance above that of typical maintenance along straighter, non-curvilinear, alignments. Over time, it would add maintenance labor and material costs.

Alternatives 1A and 1B would also have the greatest amount of floodplain fill impacts of the five

⁷ The term **rolling stock** refers to any vehicles moving on a railway, including both powered and unpowered vehicles, i.e. locomotives, railroad cars, coaches, and wagons.

build alternatives considered. The close proximity of the overhead US Route 20 bridge makes the upstream alignment of a double-track bridge infeasible. The close proximity of power line towers and the UPRR bridge structure makes the west (downstream) alignment infeasible.

Alternatives 1A and 1B do meet the Purpose and Need of the Project by delivering an improved bridge facility that is double-tracked and provides for the needed reliability and flexibility. These two alternatives are the most expensive and involve the biggest impact on the natural environment.

Therefore, Alternatives 1A and 1B are removed from further consideration.

For the components of these alternatives, see **Figure 2-1A** for the east (upstream) alignment and **Figure 2-1B** for the west (downstream) alignment. Also, see **Appendix A** for more detailed design drawings of each alternative.

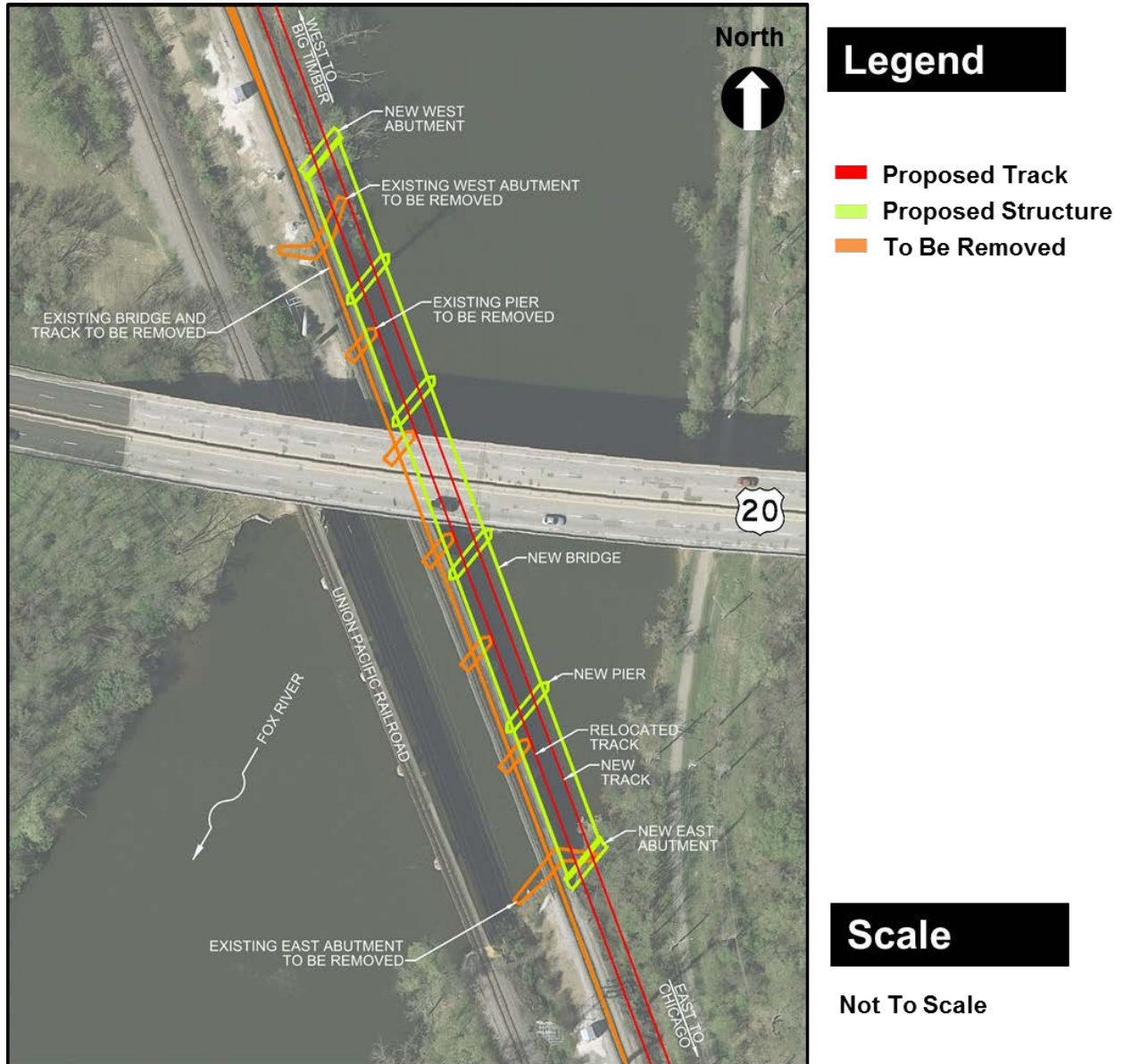


Figure 2-1A: Components of Alternative 1A – Construct a New Double Track Bridge on New Alignment East (Upstream)

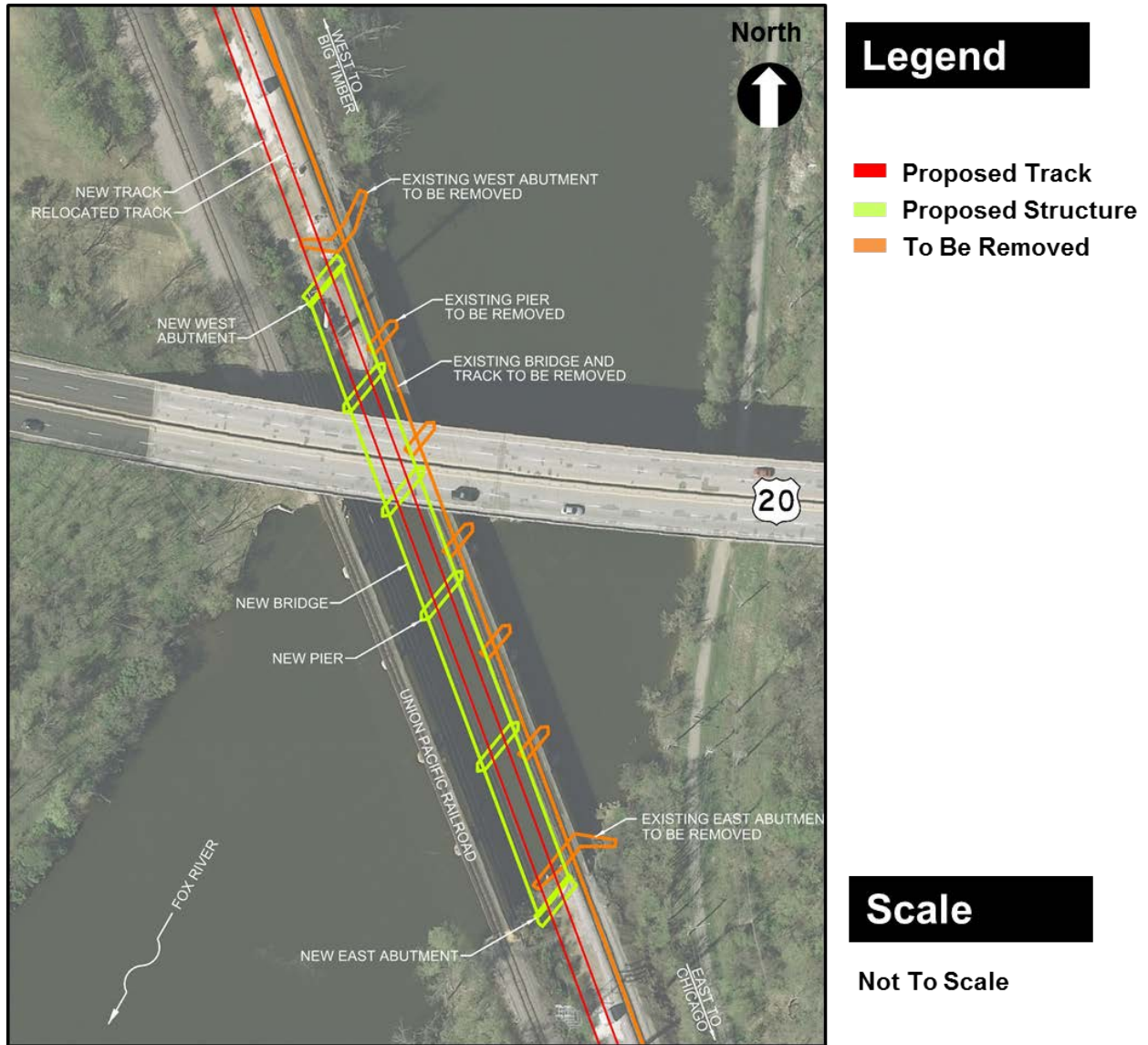


Figure 2-1B: Components of Alternative 1B – Construct a New Double Track Bridge on New Alignment West (Downstream)

2.1.3 Alternative 2: Construct a New Single-Track Bridge on the Existing Alignment

Improvements under Alternative 2 would remove the existing bridge and construct a new bridge on the existing alignment while re-using the original piers. Except for the No Build Alternative, this would be the least environmentally disruptive alternative, and has the advantage of keeping the current track alignment. The same number of piers (five) would be used as the existing bridge. The masonry piers and abutments are in fair condition. However, the piers would need to be strengthened to bring them into compliance with current railroad design criteria before the new spans (or beams) could be placed on top of the existing piers. Specifically, the current AREMA⁸ standards require that the piers handle significant load stresses from train braking and acceleration maneuvers. The cost required to upgrade the existing piers to handle these loads was found to be substantial and nearly the same price as a complete new bridge.

Also, a conservative analysis of the vertical load carrying capacity of the existing pier foundations indicated that, while the piers had adequate capacity to support the weight of the Metra trains, their capacity was not up to the requirements of current AREMA standards. Three existing piers would need to be enlarged to handle the current train design load requirements.

Constructing the new bridge on the existing alignment would require that the six new bridge spans (or beams) be assembled off line. For each span, a weekend closure would be needed to install them into place. The closure would involve a track outage and train service disruptions for a total of six weekends. During each weekend of track and train service disruption, one existing bridge span would be removed and a new span installed in its place. As this alternative proposes only a single-track bridge, it would also do nothing to address the bottleneck and capacity constraints of the current single-track bridge for future additional passenger or freight rail service on the Milwaukee West Line.

Because this alternative does not sufficiently improve operational efficiencies as stated in the Purpose and Need, it was decided to pursue an alternative for a completely new bridge, and this alternative was dropped from further consideration.

See **Figure 2-2** for the components of this alternative. Also, see **Appendix A** for more detailed design drawings of this alternative.

⁸ American Railway Engineering and Maintenance-of-Way Association



Legend

- Proposed Track
- Proposed Structure
- To Be Removed

Scale

Not To Scale

Figure 2-2: Components of Alternative 2 – Construct a New Single-Track Bridge on the Existing Alignment

2.1.4 Alternative 3: Construct a New Single-Track Bridge on an Upstream Alignment

Improvements under Alternative 3 would construct a new bridge on an alignment east (upstream) of the existing bridge. The new bridge would have five spans resting on four new cast-in-place concrete piers, and two new end abutments. The existing bridge, its piers and abutments would be removed after the new bridge structure was completed and service is transferred over to the new bridge.

The advantage of the upstream alignment is that the new bridge could to be built adjacent to the existing bridge and require only two weekend train service disruptions to realign the tracks on both ends of the new bridge to the new alignment.

However, a disadvantage of this alternative is that a double jog in the track alignment would be needed at each end of the new bridge to connect with the existing mainline track. Like Alternatives 1A and 1B, more wear and tear on equipment wheels would occur, and require increased track and rolling stock wheel maintenance, adding maintenance labor and material costs, over time. Operationally, a straighter alignment is preferred over an alignment with curves.

Similar to Alternative 2, this alternative proposes only a single-track bridge. It would do nothing to address the bottleneck and capacity constraints of the current single-track bridge for existing or additional passenger or freight rail service on the Milwaukee West Line as described in the Purpose and Need.

Another disadvantage, or concern, for this alternative is the proximity of its construction area to the existing US Route 20 bridge piers and supports. The upstream alignment brings construction activities very near the US Route 20 bridge piers. Construction activities near the existing piers could undermine the stability of the US Route 20 bridge pier foundations and compromise the structural integrity of the bridge.

Lastly, because of the bend in the river and jog in the track alignment near the proposed bridge location, this alternative would require additional embankment filling in the floodway upstream of the new bridge. The alignment in this alternative would require an additional span and pier in the riverbed compared to a downstream alignment. The fact that this bridge alignment would have more fill and one additional pier in the floodway compared to the downstream alignment bridge (Alternative 4) would be a potential issue during the permitting process with the regulatory agencies. The additional bridge span would also increase the construction cost.

For the reasons stated above, this alternative does not adequately meet the Project's Purpose and Need, and was therefore dropped from further consideration.

See **Figure 2-3** for the components of this alternative. Also, see **Appendix A** for more detailed design drawings of this alternative.

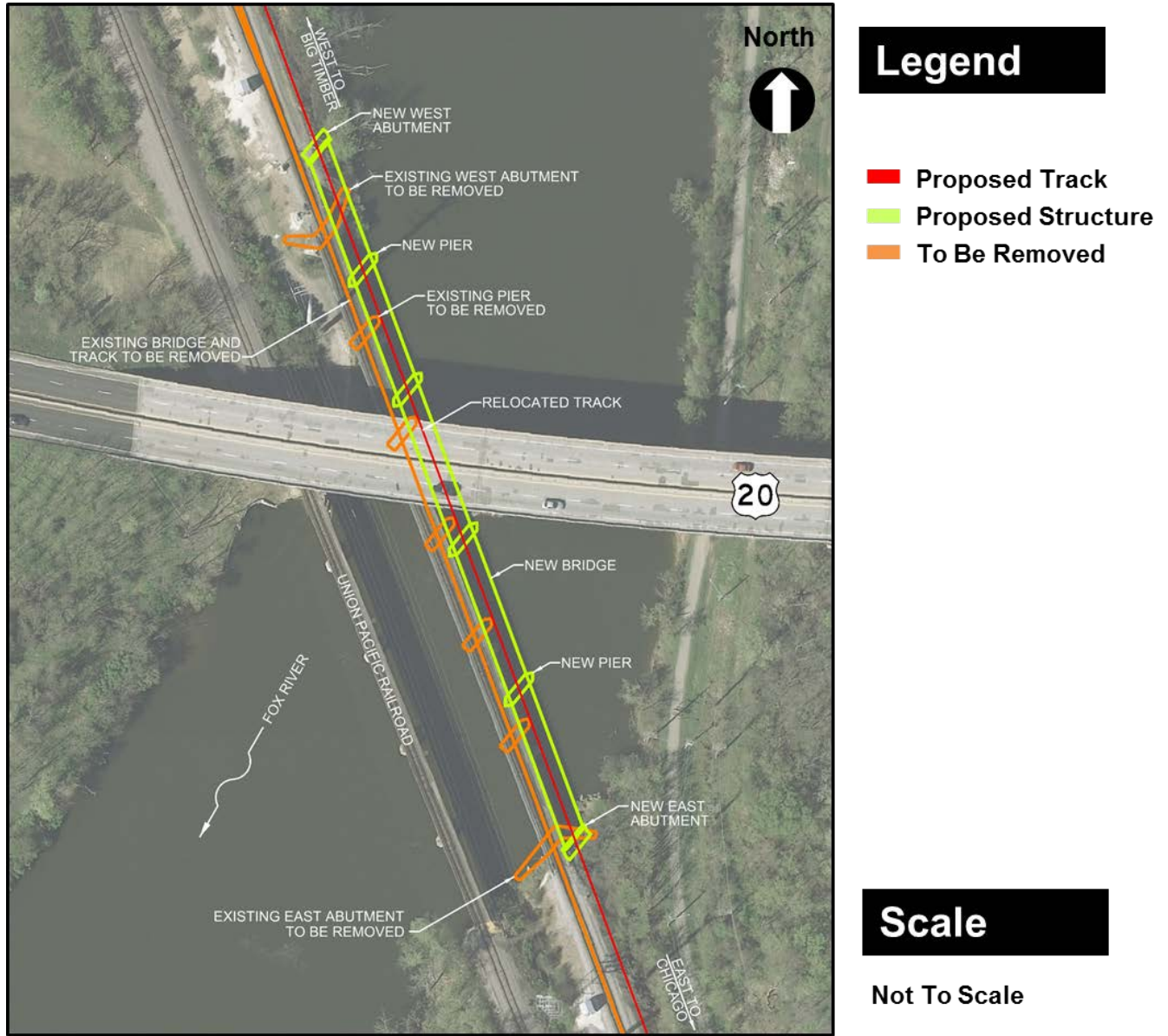


Figure 2-3: Components of Alternative 3 – Construct a New Single-Track Bridge on an Upstream Alignment

2.2 Preferred Build Alternative

Alternative 4: Construct a New Double-Track Bridge on Existing and Downstream Alignment (Preferred Build Alternative)

Improvements under Alternative 4 would construct a new bridge immediately west (downstream) of the existing bridge. This new bridge would be aligned with Track #2 and located between the existing bridge and the Union Pacific Railroad bridge. The new bridge proposes to have four spans (or beams), three piers, and two end abutments.

For purposes of discussion and clarity, the two mainline tracks that approach the existing single-track bridge over the Fox River are numbered. The tracks are aligned in a north-south direction. The east track is Track #1 and the west track is Track #2. The existing bridge aligns with Track #1 and carries it over the river.

Based on projected environmental impacts, reduced service disruption and increased bridge capacity, Metra determined that constructing a new double-track bridge on a combination of the existing alignment and a west (downstream) alignment is the Preferred Build Alternative. This design configuration meets the Project's Purpose and Need.

After the new bridge is constructed and the Track #2 connections are made at the ends of the bridge, the service would be transferred from the existing bridge to the new bridge. The existing bridge, its five spans, four piers and two end abutments, would then be demolished.

Next, the three piers on the new bridge would be extended easterly to the location of the demolished existing bridge. These piers would support the spans (or beams) for the new bridge along Track #1, which adds a second track crossing. This second track would become the outbound track and would align with the existing outbound Track #1 on both sides of the Fox River. The Preferred Build Alternative would result in the most direct alignment by minimizing track curvature for both tracks. It eliminates the double shift found in Alternatives 1A and 1B.

Because the new downstream bridge location is farther from the bend in the river, the length of bridge required is one span shorter than the Alternative 3 bridge on a new upstream alignment. The Preferred Build Alternative bridge location would also require less earthwork filling of the floodway. This bridge would have three new piers in the waterway, which is two fewer than the existing bridge, and would present less of an obstruction to water flow than the existing bridge.

The new bridge on the new downstream alignment would allow this bridge to be built adjacent to the existing bridge. The construction would require only two weekend train service disruptions to connect the extended Track #2 on the new bridge to the existing Track #2 on both ends of the bridge. Some minor track alignment work would be required to make this connection. After the existing bridge is removed and replaced, the existing mainline track (Track #1) would require no

track realignment to connect to the existing track off the bridge ends.

The Preferred Build Alternative Bridge is slightly more costly than Alternative 2, but it results in a completely new double-track bridge that would be designed with the latest AREMA standards. Alternative 2 only provides a new single-track bridge. The Preferred Build Alternative also minimizes track alignment impacts and has a minimal amount of train service disruption.

The existing single-track bridge is the only single-track segment on the double and triple track mainline alignment between Elgin and Chicago. The new bridge would allow a second track to be installed thus creating a new double-track river crossing consistent with the rest of the alignment between Elgin and Chicago.

Because this alternative most fully addresses the Project's Purpose and Need, while minimizing other negative impacts, this alternative is considered the Preferred Build Alternative, and is further reviewed in this EA.

See **Figure 2-4** for the components of this alternative. Also, see **Appendix A** for more detailed design drawings of this alternative.

A comparison of the alternatives has been summarized below in **Table 2-1**, Alternative Comparison Matrix.

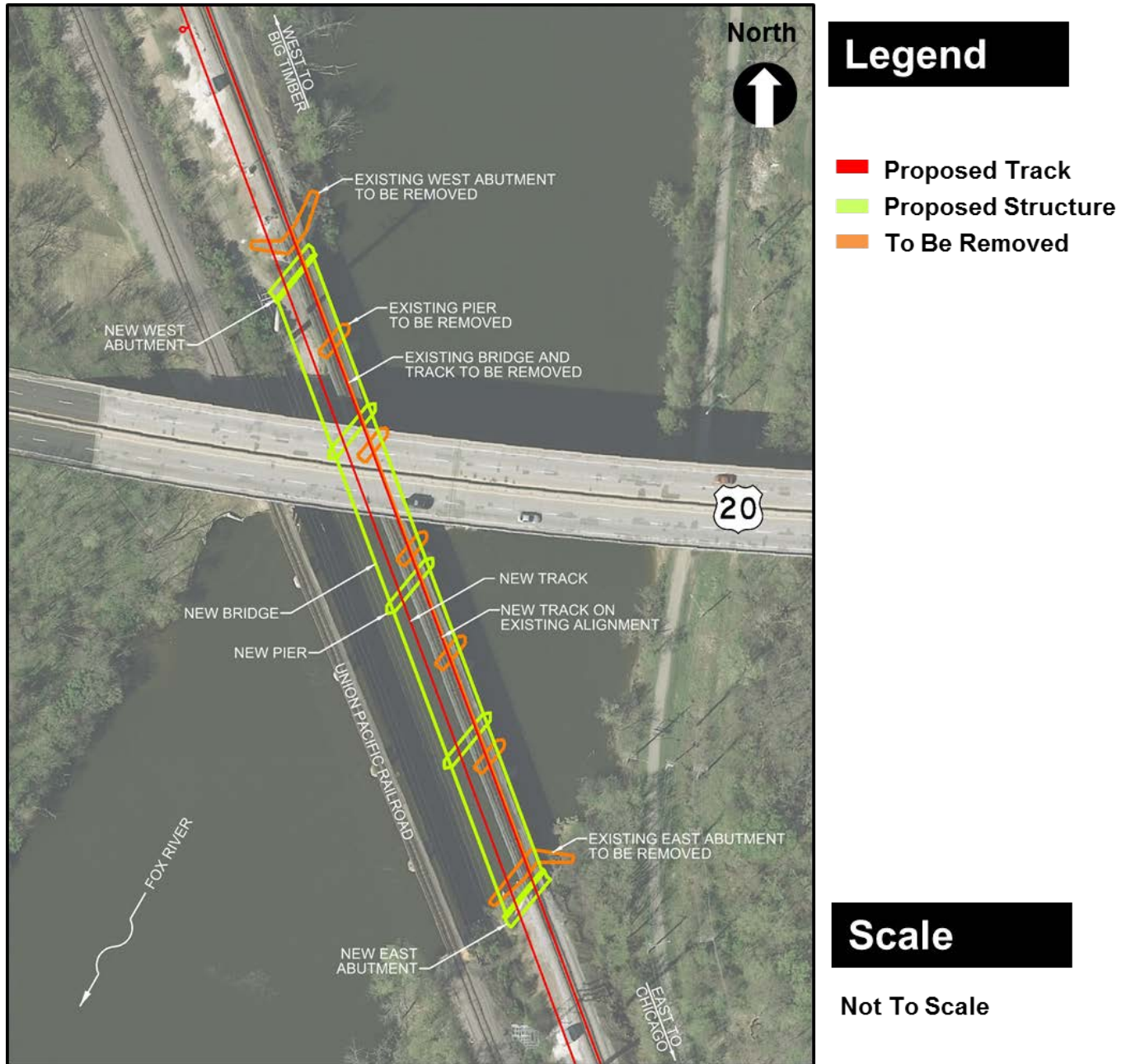


Figure 2-4: Components of Alternative 4 – Construct a New Double-Track Bridge on Existing and Downstream Alignment (Preferred Build Alternative)

Table 2-1: Alternative Comparison Matrix

Selection Criteria and Design Considerations	Alternatives					
	No Build	Alternative 1A	Alternative 1B	Alternative 2	Alternative 3	Alternative 4
	Continue to Provide Extensive Maintenance Measures and Repairs	New Double-Track Bridge on New Alignment		New Single-Track Bridge on the Existing Alignment	New Single-Track Bridge on an Upstream Alignment	New Double-Track Bridge on Existing and Downstream Alignment
Upstream		Downstream				
Design Speed	50 mph	70 mph	70 mph	70 mph	40 mph	70 mph
Tracks over Bridge	1	2	2	1	1	2
Operation Impacts / Crossovers⁹	Single Track Bridge Crossing. No improvements possible.	Due to the double shifts in the tracks, the crossovers are further away from the bridge ends.	Due to the double shifts in the tracks, the crossovers are further away from the bridge ends.	Single Track Bridge Crossing. No improvements possible.	Single Track Bridge Crossing. No improvements possible.	Provides operational flexibility. A double crossover is provided at each approach (end) of the bridge.
Bridge Condition & Safety Impacts	Replacement of three spans would be required at the bridge because of their poor condition. The repaired bridge would not fully meet current AREMA standards.	The new bridge would meet current AREMA standards and safety would improve		The new bridge would meet current AREMA standards and safety would improve.	The new bridge would meet current AREMA standards and safety would improve.	The new bridge would meet current AREMA standards and safety would improve.
Track Alignment Impacts	No Change	Adds four curves to the existing track alignments.		No Change	Adds two curves to existing track alignments.	No Change to one Track Alignment and adds two minor curves to the second track alignment.

⁹ Crossovers are track-to-track crossings between continuous tracks. It is desirable to place these on the approaches (near the ends) of the bridge.

Table 2-1: Alternative Comparison Matrix (continued)

Selection Criteria and Design Considerations	Alternatives					
	No Build	Alternative 1A	Alternative 1B	Alternative 2	Alternative 3	Alternative 4
	Continue to Provide Extensive Maintenance Measures and Repairs	New Double-Track Bridge on New Alignment		New Single-Track Bridge on the Existing Alignment	New Single-Track Bridge on an Upstream Alignment	New Double-Track Bridge on Existing and Downstream Alignment
Upstream		Downstream				
Long Term Train Operations Impacts	No improvement of train delays at single track bridge.	Double track bridge would substantially reduce train delays and provide redundancy for train operations for maintenance.		No improvement of train delays at single track bridge.	No improvement of train delays at single track bridge.	Double track bridge would substantially reduce train delays and provide redundancy for train operations for maintenance.
Constructability Issues	Increased maintenance	Close proximity between piers for US Route 20 and existing bridges. Minor modifications to existing east abutment required for construction.	Overhead power lines, close proximity to the UPRR bridge, and close proximity between piers for US Route 20 and existing bridge.	No impact	Minor modifications to existing east abutment required for construction.	Overhead power lines, and close proximity between the existing US Route 20 center bridge pier and the existing and proposed Metra bridge piers.
Roadway Impacts	No impact	Replace two at-grade railroad crossings at Elgin Avenue and gravel access road northeast of existing bridge.	Replace two at-grade railroad crossings at Elgin Avenue.	No impact	Replace two at-grade railroad crossings at Elgin Ave. and gravel access road northeast of existing bridge.	Replace one at-grade railroad crossing at Elgin Avenue.
Weekend Track Outage and Train Service Disruptions during Construction	A total of 3 weekends.	A total of 2 weekends.		A total of 6 weekends.	A total of 2 weekends.	A total of 2 weekends.

Table 2-1: Alternative Comparison Matrix (continued)

Selection Criteria and Design Considerations	Alternatives					
	No Build	Alternative 1A	Alternative 1B	Alternative 2	Alternative 3	Alternative 4
	Continue to Provide Extensive Maintenance Measures and Repairs	New Double-Track Bridge on New Alignment		New Single-Track Bridge on the Existing Alignment	New Single-Track Bridge on an Upstream Alignment	New Double-Track Bridge on Existing and Downstream Alignment
Upstream		Downstream				
Commonwealth Edison (Electric Power Utility) Impacts	NA	NA	Two large high voltage towers to be relocated; one standard power pole to be relocated.	NA	NA	Two retaining walls required at high voltage towers; one standard power pole to be relocated.
Other Utility Impacts	NA	Signal foundation and two communication poles to be relocated.	One signal cabinet and transformer, two signal bungalows, one communication tower, and tower B-35 signal equipment to be relocated.	NA	Signal foundation and one communication pole to be relocated.	Signal cabinets, two signal bungalows, transformer and tower B-35 signal equipment to be relocated.
Impacts to Fox River and Surrounding Context	Temporary impacts during bridge rehabilitation.	Greatest floodplain fill impacts of the five build alternatives. Adjacent U.S. 20 Bridge to the north and powerline towers and UPRR bridge to the south make this alignment infeasible.		Enlargement and improvements to the existing piers would impact the Fox River.	Additional embankment filling in the floodway upstream would be required. Adjacent U.S. 20 Bridge to the north would complicate construction. Construction of four piers would impact the Fox River more than Alternative 4.	Additional embankment filling in the floodway upstream would be required. Construction of three piers would impact the Fox River less than Alternative 3. New bridge would have two fewer piers than the existing and would present less of an obstruction to water flow.

Table 2-1: Alternative Comparison Matrix (continued)

Selection Criteria and Design Considerations	Alternatives					
	No Build	Alternative 1A	Alternative 1B	Alternative 2	Alternative 3	Alternative 4
	Continue to Provide Extensive Maintenance Measures and Repairs	New Double-Track Bridge on New Alignment		New Single-Track Bridge on the Existing Alignment	New Single-Track Bridge on an Upstream Alignment	New Double-Track Bridge on Existing and Downstream Alignment
Upstream		Downstream				
Cost¹⁰	\$14 million	\$44 million		\$21.5 million	\$22 million	\$34 million
Meets Project Purpose and Need	Minimally: This alternative would repair existing portions of the bridge in poor condition, but would not improve train delay issues with the existing single track condition.	Fully: This alternative would address the existing bridge’s poor condition by constructing a new bridge. It would improve operations, service reliability and flexibility for maintenance activities by providing two tracks. However, it involves greater impacts to the Fox River floodway than Alternative 4. The proposed bridge has 4 piers, 5 spans, and 2 end abutments. It also has the highest cost of all the alternatives.		Partially: This alternative would address the existing bridge’s poor condition by constructing a new bridge, but would not improve train delay issues with the existing single track condition.	Partially: This alternative would address the existing bridge’s poor condition by constructing a new bridge, but would not improve train delay issues with the existing single track condition.	Fully: This alternative would address the existing bridge’s poor condition by constructing a new bridge. It would improve operations, service reliability and flexibility for maintenance activities by providing two tracks. Identified impacts are less than Alternatives 1A and 1B. The proposed bridge has 3 piers, 4 spans, and 2 end abutments.
Preferred Alternative	No	No		No	No	Yes

¹⁰ Cost estimate information provided by Metra’s Engineering Department.

Chapter 3 Environmental Resources, Impacts, and Mitigation Measures

One of the primary purposes of NEPA is to provide the public and decision-makers with relevant information on the potential environmental impacts of a proposed project (42 U.S.C. §4321 et seq.). This chapter describes existing conditions and the impacts of both the No Build Alternative and the Preferred Build Alternative (construction and operation) on different aspects of the social, cultural, and natural environment. The following major topics (called resource areas) are considered: displacements and relocations; neighborhoods, communities and businesses; historic and archaeological resources; water resources; flooding; biological resources; noise; vibration; hazardous materials; environmental justice (EJ); indirect and cumulative impacts; resources with limited or no impacts including transportation, air quality, land use and economic development, navigable waterways and coastal zones, geology and soils, energy, safety and security, and visual and aesthetic conditions; and Section 4(f) resources. This chapter summarizes the findings of the resource area evaluations.

Each resource area discussion includes an overview of the resource area, a description of the major considerations and laws or regulations governing the analysis, a description of the impact analysis method, a summary of existing conditions, and anticipated temporary construction and permanent environmental impacts from the No Build Alternative and Preferred Build Alternative. Within this NEPA document, resource areas are discussed in terms of impacts being either “beneficial” or “adverse.” Where adverse impacts are noted, standard measures (often described as “best management practices” or BMPs) to avoid or minimize impacts are discussed. Additional mitigation measures are described where needed to minimize impacts.

3.1 Displacements and Relocations of Existing Uses

Displacements and relocations of residents or businesses may occur when land and/or structures are needed to accommodate construction or the permanent footprint of a project. This section describes the Metra right-of-way expansion needed for the Project, including acquisition of private property for permanent easements.

3.1.1 Regulatory Framework/Methods

The Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended (“Uniform Act,” 42 United States Code [USC] § 4601, et seq.) mandates that relocation services and payments be made available to eligible residents, businesses, and non-profit organizations displaced as a direct result of any project undertaken by a federal agency or with federal financial assistance.

While there are no specific NEPA thresholds for assessing displacement impacts, compliance with the Uniform Act includes provisions for uniform and equitable treatment of persons displaced from their homes or businesses by establishing uniform and equitable land acquisition policies to address impacts.

Metra utilized existing aerial photography and site visits to survey the area surrounding the proposed improvement and the required construction staging areas. The objective of the survey was to determine whether any properties were close enough to either the proposed improvement or construction staging areas that they would need to be either permanently acquired or temporarily acquired for the duration of the construction.

3.1.2 Existing Conditions

The Preferred Build Alternative for the Milwaukee West Line Fox River Bridge Improvement Project (the Project) would occur within the City of Elgin, Kane County, Illinois. The general area of the Project is a transportation and utility corridor south of the Central Business District of Elgin. The existing Metra right-of-way extends in a northwest to southeast direction across the Fox River. The Union Pacific Railroad (UPRR) owns a single-track line immediately west of and parallel to the existing Metra single-track bridge. The US Route 20 bridge over the Fox River extends over both the Metra and UPRR tracks and bridges.

Land use immediately adjacent to the Project includes the UPRR, the Fox River, and the National Street Metra Station on the north side of the river. South and east of the Fox River, land use includes the Fox River Trail, undeveloped land and industrial structures. No structures are located close enough to the Project that would require displacement. Residential land is located west of the UPRR tracks, northwest of the Fox River along with the Marie Grolich Park. The City of Elgin wastewater treatment plant is located west of the UPRR tracks, southeast of the Fox River. All proposed work is located east of the UPRR tracks. **Figure 1-3**, Project Limits Map, shows the project limits within the existing land use.

3.1.3 Environmental Impacts

The following sections summarize the potential displacement and relocation impacts of the No Build Alternative and Preferred Build Alternative.

No Build Alternative

The No Build Alternative would not displace any structures; no permanent displacement or relocation impacts would occur.

Preferred Build Alternative

The Preferred Build Alternative would not displace any residences, businesses, or other buildings. The Preferred Build Alternative would, however, require temporary easements for construction staging and the acquisition of land (a permanent easement) west of the existing bridge over the Fox River for the construction and operation of the new bridge. Approximately 0.97 acres of temporary easement would be required from the Union Pacific Railroad. Approximately 0.33 acres of permanent easement would be acquired from the Union Pacific Railroad near the temporary easements. Land acquisition would be limited to the unused land located between the Union Pacific Railroad and Metra Railroad tracks. There are no structures in the easement areas and the Union Pacific Railroad tracks are outside of the easement areas.

Figure 3-1 shows the easement areas that would be required.

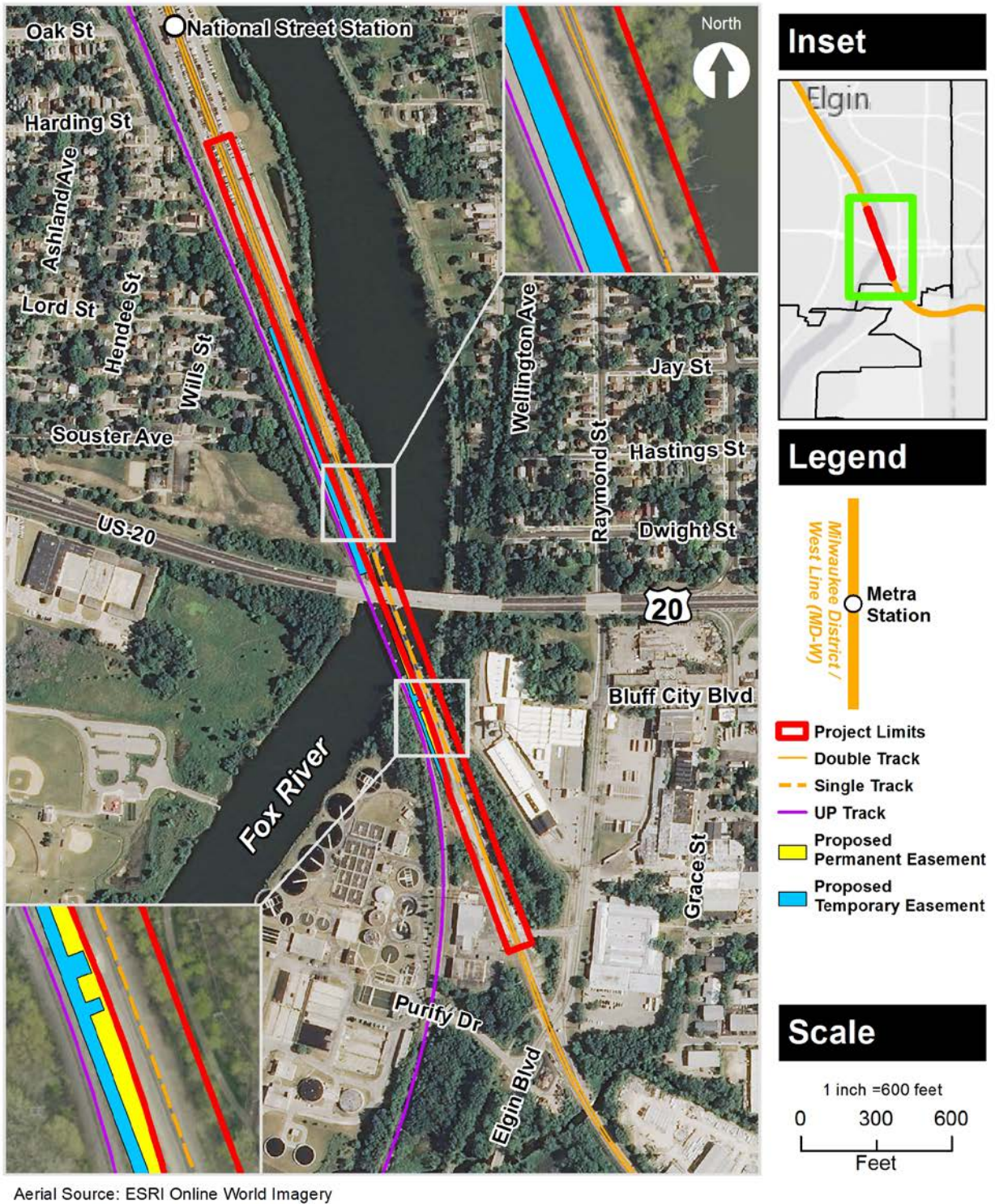


Figure 3-1: Easement Areas

3.1.4 Measures to Avoid or Minimize Harm

To address the impacts for all private property acquisitions, the following requirements in compliance with the Uniform Act would apply:

- Compensation - Just compensation for property acquisition and easements, measured by the fair market value of the property, as determined by Metra through an appraisal process, would be provided to the affected property owner.
- Relocation assistance – If a business or residence were displaced, relocation assistance would be provided to the affected property owner.

3.2 Neighborhoods, Communities, and Businesses

This section discusses project impacts on the surrounding neighborhood, community, and businesses. The analysis considered the surrounding community character and cohesion, mobility, and community resources, such as schools, parks, and community centers near the project area.

3.2.1 Regulatory Framework/Methods

The U.S. Department of Transportation (USDOT) and the Illinois Department of Transportation (IDOT) both have Community Impact Assessment manuals, which Metra used to look at potential neighborhood, community, and business impacts of the Project (USDOT 1996, IDOT 2007). The analysis considered the following types of impacts:

- Community Character and Cohesion - Impacts due to commercial and residential displacements and changes in land use, visual/aesthetics, noise levels, and population/demographics. Community character is an attribute of a geographic area with identifiable characteristics that make it unique. Community cohesion is an attribute of a geographic area where segmentation or division of the area would reduce its desirability to current and future residents.
- Mobility - Overall community impacts of changes in transportation options, station access, travel patterns, parking, physical barriers, and access for emergency service providers.
- Community Resources - Impacts on key facilities in the project area that play an important role in shaping and defining the community, such as landmarks, parks, community centers, and other places that serve as focal points or provide community services.

The community area was profiled using 2014 census data and key community resources within a quarter mile of the project limits. Potential for displacements, impacts to community facilities, and effects on mobility were assessed by reviewing project plans and aerial photographs of the project area¹¹. No displacements, severances, changes to existing travel patterns, or changes to existing land use are anticipated as a result of this Project.

¹¹ The **project limits** are along the existing railroad corridor right-of-way (ROW) and extend from just south of the National Street Station to just north of Elgin Boulevard. The **project area** covers a broader area and includes locations beyond the existing ROW and the surrounding community.

3.2.2 Existing Conditions

The project area is within the City of Elgin, which contains suburban-type development with a diverse population. By providing convenient access to Chicago and other regional employment centers, Metra and its predecessor railroads have helped induce new commercial and residential development. **Table 3-1** provides an overview of Elgin’s demographics. The project area is defined as half a mile beyond the project limits.

Table 3-1: Elgin Community Area Profile

Category	Project Area Total	City of Elgin Total
Population ¹	17,248	110,906
% Employment ²	63.1%	64.2%
% Minority Population ¹	61.2%	58.7%
% Elderly Population ²	6.78%	9.4%
% Renter-Occupied Households ²	38.7%	67.9%
% Owner-Occupied Households ²	61.3%	32.1%
Median Home Value ²	\$159,500	\$171,000
Average Household Size (# persons ²)	3.09	3.14
Average Gross Rent per Month ²	\$785	\$971

1 Project area calculated using block groups within ½ mile

2 Project area calculated using census tracts within ½ mile

Source: U.S. Census Bureau 2014

The major roadway in the project area is US Route 20, which extends east-west through the middle of the project area. Other major roadways include IL Route 31 to the west, Elgin Boulevard to the south, and Raymond Street to the east. Smaller residential streets are present in the residential areas to the northwest and northeast of the Project.

The area surrounding the southern portion of the project limits consists of industrial uses including the Fox River Water Reclamation District, The Alphabet Shop, and Plastic Specialties. Residential areas are located near the northern portion of the project limits. Commercial areas near the project limits are located along US Route 20 to the east of the Project.

Parks and recreational facilities within a quarter mile of the project area include Marie Grolich Park, Elgin Shores Forest Preserve, and the Fox River Trail. Marie Grolich Park is located west of the UPRR tracks on the west side of the Fox River outside of the project limits. Marie Grolich Park is owned and maintained by the City of Elgin Parks and Recreation Department. There are recreational facilities at the park including a playground, practice fields, and a quarter-mile path. The Fox River Trail is to the east of the Metra tracks on the east side of the Fox River. The Fox River Trail is outside the project limits. The trail is over 40 miles long

and extends along the Fox River from the City of Aurora to the Village of Algonquin. This portion of the Fox River Trail is maintained by the Forest Preserve District of Kane County, which also operates the Elgin Shores Forest Preserve. The Elgin Shores Forest Preserve is located south of US Route 20 and on both sides of the Fox River outside of the project limits. East of the Fox River, recreational facilities include a trail system and the area west of the Fox River that has been leased to the City of Elgin for little league baseball fields. **Figure 3-2** illustrates the location of park resources near the Project.

The Metra National Street Station, which is approximately 1,500 feet north of the project limits, averaged 700 weekday boardings and 657 weekday alightings (RTAMS, 2014).

3.2.3 Environmental Impacts

The following sections summarize the potential neighborhood and community impacts of the No Build Alternative and Preferred Build Alternative.

No Build Alternative

Construction Impacts

Under the No Build Alternative, maintenance construction activities would still be required to maintain the existing structure. The No Build Alternative would result in minor temporary impacts on the surrounding neighborhoods due to construction activities. Temporary construction impacts could include noise, vibration, dust, temporary utility disruption, negative visual and aesthetic changes from demolition and construction, and construction vehicle emissions. Truck traffic would be primarily present along major roads near the project area and would use a defined access path to reach the project limits, likely along the existing right-of-way.

Permanent Impacts

No permanent impacts would occur under the No Build Alternative.

Preferred Build Alternative

Construction Impacts

The Preferred Build Alternative would result in minor temporary adverse impacts on the surrounding neighborhoods due to construction activities. Temporary construction impacts could include noise, vibration, dust, temporary utility disruption, negative visual and aesthetic changes from demolition and construction, and construction vehicle emissions. Truck traffic would be primarily present along major roads near the project area and would use a defined access path to reach the project limits, likely along the existing right-of-way.

Construction would take place within existing Metra right-of-way and on land acquired from the Union Pacific Railroad. There would be improvements to the crossing at Elgin Avenue and planned mitigation includes maintaining vehicle access to the adjacent business (The Alphabet Group) through a temporary track crossing and by performing weekend work. **Figure 3-3** shows the existing crossing location. The temporary track crossing would be located near the existing track crossing.

Permanent Impacts

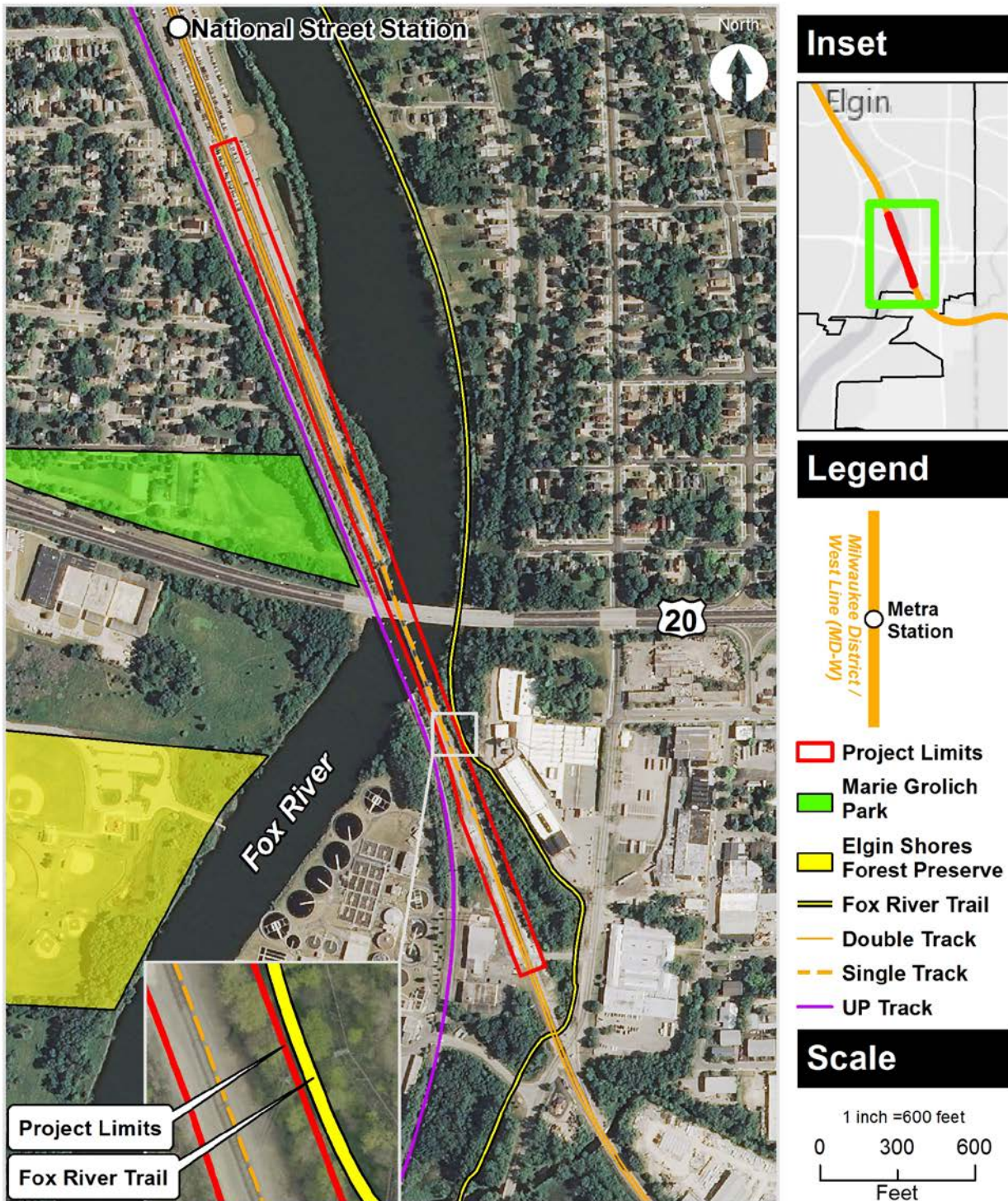
No displacements would occur as a result of the Preferred Build Alternative and no permanent impacts to the community are expected. The additional land required for the Project would be acquired from the adjacent Union Pacific Railroad and would therefore not change in overall type of usage or have an effect on the community.

The Preferred Build Alternative would improve mobility including providing for faster train speeds. The Preferred Build Alternative would provide more reliable commuter rail access to jobs in the project area and elsewhere on the Metra train system. Access to nearby community resources would be enhanced as a result of the mobility improvements.

3.2.4 Measures to Avoid or Minimize Harm

As no neighborhood, community or business impacts, except for those generally associated with construction, are anticipated, no mitigation measures for permanent impacts are required. Efforts to minimize community disruptions from construction would be undertaken through coordination with the City of Elgin. The construction activities would be limited to daytime hours where feasible, though night and/or weekend work may be needed during track cutover, piling, excavation, deep foundation work, or other activities. If any planned work conflicts with the City of Elgin's noise ordinance, Metra will coordinate with the City. Truck traffic would be primarily present along major roads near the project area and would use a defined access path to reach the project limits, likely along the existing right-of-way.

Maintaining access to any businesses that would potentially be impacted would be a high priority. One grade crossing at Elgin Avenue would be replaced by the Project. This crossing provides access to a business called The Alphabet Shop. A temporary track crossing would be provided to ensure access would be maintained to The Alphabet Shop during construction. **Figure 3-3** shows the location of the existing track crossing. The temporary track crossing would be located near the existing track crossing.



Aerial Source: ESRI Online World Imagery

Figure 3-2: Parks and Recreational Facilities

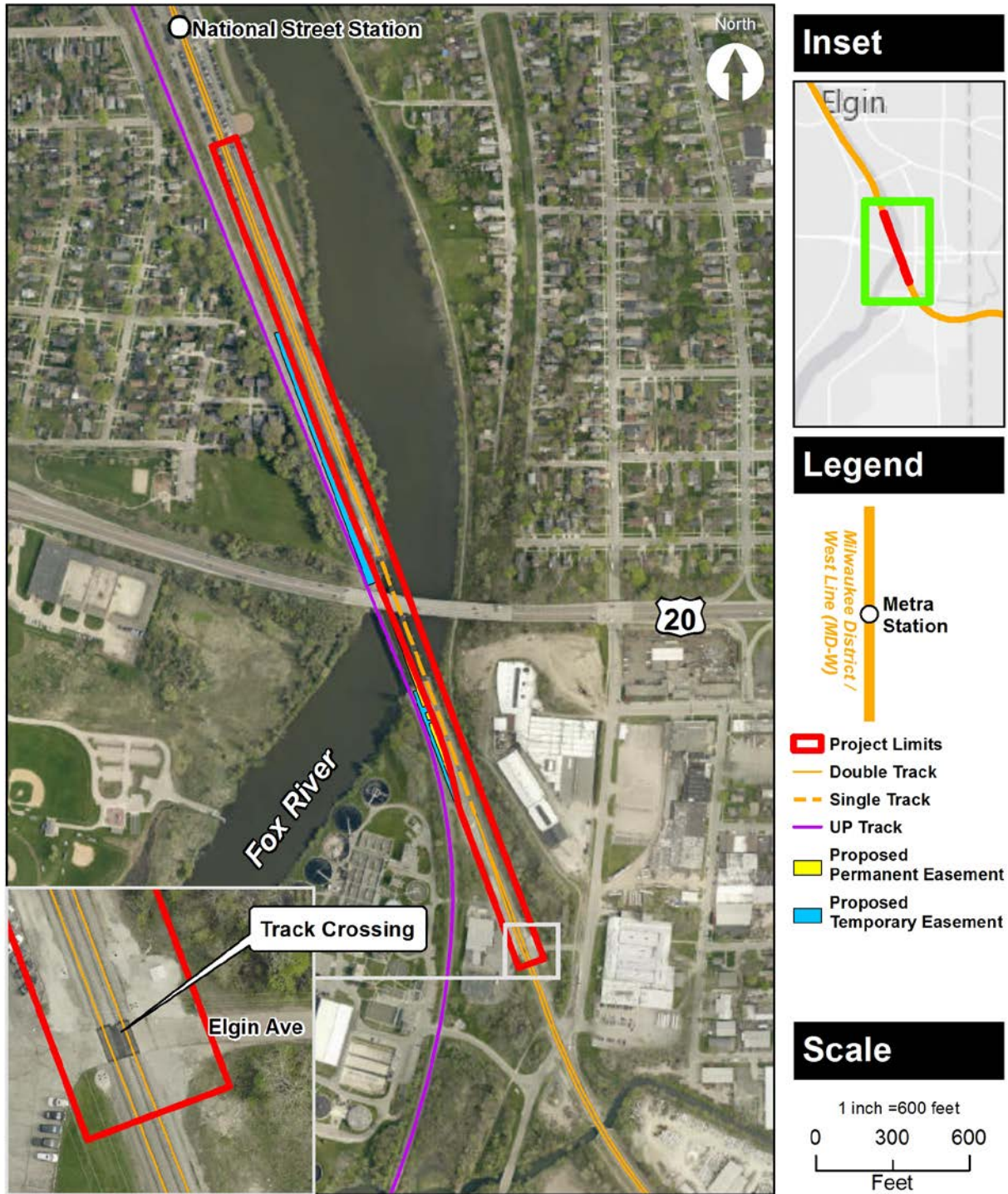


Figure 3-3: Track Crossing

3.3 Historic and Archaeological Resources (Section 106 Consultation)

This section summarizes findings under Section 106 of the National Historic Preservation Act (NHPA) and in coordination with the State Historic Preservation Officer (SHPO) of the Illinois Historic Preservation Agency (IHPA) and consulting parties to the Section 106 process.

The structure of this section is slightly different than other sections within Chapter 3 of the EA to fully document the process and consultation required under Section 106. In addition, the term “effects” is used in this section rather than “impacts” because of the unique requirements and terminology related to historic resources. Section 4.1 and 4.2 summarizes Section 106 coordination efforts to date.

3.3.1 Regulatory Framework/Methods

Cultural and historic resources are protected by various federal regulations. Section 106 of the National Historic Preservation Act (NHPA) requires federal agencies to consider effects on historic resources from their actions and to balance preservation needs with the need for the action (54 U.S.C. § 300101, et seq.). As provided in 36 CFR § 800, the Section 106 process “seeks to accommodate historic preservation concerns with the needs of federal undertakings through consultation” [36 CFR § 800.1(a)]. The goal of the consultation is to identify historic properties potentially affected by the undertaking, assess project effects, and seek ways to avoid, minimize, or mitigate any adverse effects on historic properties [36 CFR § 800.1(a)].

For the assessment of historic and archaeological resources, Metra conducted a four-step process following requirements of 36 CFR § 800:

1. **Define the Area of Potential Effects** - FTA first determined an Area of Potential Effects (APE) for cultural/historic resources. The APE is defined as the geographic area within which the project may cause alterations in the character or use of historic properties. Development of the APE involved site visits and a review of aerial maps and conceptual engineering drawings for the Preferred Build Alternative. The APE boundaries were based on the area directly affected by construction, the height of the proposed structures, and the indirect area of potential visual effects. Generally, the APE contains parcels that are adjacent to either side of the existing rail line, plus a buffer to account for potential indirect effects.
2. **Identify Historic and Archaeological Resources** - The APE was then field surveyed for historic architectural resources that meet National Register of Historic Places (NRHP) criteria. Further research using the Historic and Architectural Resources Geographic Information System (HARGIS) was conducted to determine whether there were documented findings of archaeological resources within the APE. NRHP criteria are defined in 36 CFR § 60.4 and apply to districts, sites, buildings, structures, or objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association with one or more of the following four criteria:
 - Criterion A - Events that have made a significant contribution to the broad patterns of American history on a federal, state, and/or local level.
 - Criterion B - Lives of persons significant in the history of the city, state, and/or the United States.

- Criterion C - Distinctive characteristics of a type, period, or method of construction, or the work of a master, or high artistic values, or a significant and distinguishable entity whose components may lack individual distinction.
- Criterion D - Information important in prehistory or history.

Within the Area of Potential Effect (APE), Metra’s analysis indicated that there are no structures eligible for listing on the NRHP. Metra did not find any information suggesting that a historic event is associated with the bridge. The existing bridge was designed and constructed by railroad staff of a predecessor railroad. The structure is not unique and is not representative of a style or school of design that is no longer available. Instead, the bridge is simply six steel spans sitting atop masonry and concrete piers and abutments. Based on the review and analysis, which indicates that there are no structures eligible for listing on the NRHP within the APE that would be directly impacted by demolition or indirectly impacted by noise or visual impacts, Metra proposed a determination of “No Historic Properties Affected”. The proposed determination was supported by past IHPA findings that no historic properties are affected on September 24, 2010 and August 17, 2011. Both of those findings are referenced under IHPA Log #010082310. The Section 106 Coordination documentation is provided in **Appendix C**. The Cultural Resources Inadvertent Discovery Plan is included in **Appendix I**.

FTA initiated consultation with the following tribal nations: Forest County Potawatomi Community, Citizen Potawatomi Nation, Prairie Band of Potawatomi Nation, and Hannahville Indian Community. FTA notified these nations of the Project and invited them to participate in consultation through correspondence sent on August 17, 2012. The Forest County Potawatomi Community responded on September 27, 2012 requesting additional information. That request was forwarded to Metra which responded on November 9, 2012 with additional details about the Project. The Forest County Potawatomi Community has not requested any further information. The other tribes listed above did not respond to the August 17, 2012 invitation letter. Correspondence with the Tribal Nations is provided in **Appendix C**.

In October 2015, FTA reinitiated the consultation process with IHPA because such a long length of time had passed since the previous consultation was initiated in 2010. FTA established the APE and determined that the proposed Milwaukee West Line Fox River Bridge Improvement Project (Metra Bridge Z-100), based on the APE boundaries, would not affect historic properties. No properties within the APE are on, or are eligible for the National Register of Historic Places.

On October 30, 2015, the Illinois State Historic Preservation Officer (SHPO) concurred with the FTA determination of the APE boundaries, that there are no properties on or are eligible for the National Register of Historic Places within the APE, and that the Project would not affect any historic properties.

3. **Assess Effects on Historic and Archaeological Resources** – Since there are no properties on or are eligible for the National Register of Historic Places within the APE, there would be no effects on these resources.
4. **Resolve any Adverse Effects** - Since there are no properties on or are eligible for the National Register of Historic Places within the APE, there are no adverse effects to resolve. No mitigation measures or a Memorandum of Agreement (MOA) are required.

3.3.2 Existing Conditions (Section 106 Eligibility Determinations)

FTA determined that there are no properties on or are eligible for the National Register of Historic Places within the APE on October 14, 2015 and the SHPO concurred on October 30, 2015.

3.3.3 Environmental Effects (Section 106 Effects Determinations)

No Build Alternative

Since there are no historic properties in the APE, the No Build Alternative would not directly result in adverse effects on historic and cultural resources.

Preferred Build Alternative

Since there are no historic properties in the APE, the Preferred Build Alternative would not directly result in adverse effects on historic and cultural resources.

3.3.4 Measures to Avoid or Minimize Harm

No historic properties were identified with the in APE; therefore, no additional measures to avoid or minimize harm are necessary as no adverse impacts are present.

3.4 Water Resources

This section discusses water resources and how the proposed project may impact surface, groundwater quality, and wetlands.

3.4.1 Regulatory Framework/Methods

Water quality is regulated by several laws and agencies at both the state and federal level.

Federal

The **Clean Water Act** (CWA; 33 U.S.C. §§1251-1387) establishes the basic structure for regulating discharges of pollutants into the “Waters of the United States” (WOUS) and regulating quality standards for surface waters. WOUS is a broad term that includes surface waters that are used or could be used for interstate commerce. This includes wetlands, ponds, lakes, territorial seas, rivers, tributary streams, and other linear drainageways below the ordinary high water mark (OHWM). Man-made water bodies, such as quarries and ponds that are no longer actively being mined or constructed, can also be considered WOUS. A specific, detailed definition of WOUS can be found at 33 Code of Federal Regulations 328.3. WOUS are within the jurisdiction of the United States Army Corps of Engineers (USACE) under Section 404 of the Clean Water Act (33 U.S.C. §1344). Navigable-in-fact WOUS are also regulated by the USACE under Section 10 of the Rivers and Harbors Act of 1899 (33 U.S.C. §403).

The CWA made it unlawful to discharge any pollutant from a point source into navigable waters, unless a **National Pollutant Discharge Elimination System** (NPDES; 33 U.S.C. §1342) permit was obtained. The NPDES program controls point source discharges. Point sources are discrete conveyances such as pipes or man-

made ditches. The NPDES program is administered by the Illinois Environmental Protection Agency (IEPA) in Illinois per CWA Section 402(b). A general NPDES Permit for Storm Water Discharges from Construction Site Activities is required for any construction site that would result in the disturbance of soil of one or more acres total land area (40 CFR §122, et seq.).

The **National Wild and Scenic Rivers System** was created by Congress in 1968 (Public Law 90-542; 16 U.S.C. 1271 et seq.) to preserve certain rivers with outstanding natural, cultural, and recreational values in a free-flowing condition for the enjoyment of present and future generations. The August 2, 1979 Presidential Directive directed federal agencies to avoid or mitigate actions that would adversely affect a Nationwide Rivers Inventory (NRI) segment [National Park Service (NPS), 2011]. The NRI is a compilation of free-flowing rivers and river segments that appear to have one or more “Outstandingly Remarkable Values” that could qualify them for inclusion in the National Wild and Scenic Rivers System. “Outstandingly Remarkable Values” include criteria such as scenery, recreation, geology, fish/wildlife value, and historic/cultural significance. The NRI is managed by the National Park Service Rivers, Trails, and Conservation Assistance Program.

State

Under **CWA** Section 303(d) (33 U.S.C. §1313), states are required to classify waters with respect to impairments. Waters that do not, or are not anticipated to, meet applicable water quality standards are considered impaired and are cataloged in the 303(d) list, requiring state regulators to develop total maximum daily loads (TMDLs). TMDLs establish pollution reduction goals to improve the quality of impaired waters. In Illinois, waters are protected and evaluated under the **General Use Water Quality Standards** (Title 35 Illinois Administrative Code, Subtitle C, Chapter I, Part 302, Subparts A and B). Waters that do not fully support their designated uses are considered impaired. Designated uses include: aquatic life (AL), fish consumption (FC), primary contact (PC) (e.g., swimming and water skiing), secondary contact (SC) (e.g., fishing and commercial/recreational boating), and aesthetic quality (AQ) (Title 35 Illinois Administrative Code, Subtitle C, Chapter I, Part 302, Subparts A and B). A list of impaired waters is published by the Illinois Environmental Protection Agency (IEPA) bi-annually in the Illinois Integrated Water Quality Report and 303d List. A use designation of “non-support” indicates that water quality is not sufficient to support a particular use, such as “aquatic life” or “primary contact”.

The State's **Public Bodies of Water** are regulated by the IEPA to protect the public's interests, rights, safety and welfare (Title 17 Illinois Administrative Code, Part 3704). In addition, the Illinois Pollution Control Board has the authority to designate Outstanding Resource Waters (35 Ill. Adm. Code §§302.105, 303.205). Outstanding Resource Waters include water bodies or water body segments that are of exceptional or unique/special ecological, recreational, or aesthetic significance.

In 2008, the Illinois Department of Natural Resources (IDNR) released biological stream ratings for Illinois (IDNR, 2008). The IDNR stream ratings can be used to evaluate aquatic resource quality, including biologically diverse streams and those with a high degree of biological integrity. The diversity and integrity scores fall within one of five ratings ranging from A to E. Streams that are rated as Class A or B are considered to be high quality with the highest biological integrity or diversity. Different segments (or reaches) of the same river/creek can have different ratings for diversity or integrity.

The IDNR's **Ecological Compliance Assessment Tool**¹² (EcoCAT) provides information on the presence or

¹² EcoCAT is available at the following web site: <http://www.dnrecocat.state.il.us/ecopublic/>.

absence of natural resources such as wetlands.

The **Advanced Identification Program**¹³ [(ADID); 40 CFR Part 230.80] was adopted by Kane County¹⁴ in 2004. The ADID Wetland program was developed by the U.S. Environmental Protection Agency (EPA) to identify high quality wetlands. These Advanced Identification wetlands are areas of higher quality that should be avoided.

Methodology

The information sources listed above were reviewed to collect data on the Fox River and other natural features in the project area, and coordination was conducted with the IDNR to receive additional information. A field visit was also conducted to identify potential wetlands and other resources that may not have been listed in sources reviewed. Potential for impacting identified resources was then assessed based upon project specifications. The field assessment used current USACE guidelines¹⁵ for identifying and delineating wetlands and waters of the United States. As part of the assessment of stormwater runoff and overall water quality, the area of the proposed improvements was compared to the total drainage area of the Fox River upstream from the project limits to assess potential for water quality impacts to the Fox River.

¹³ The final Advanced Identification (ADID) Study was published by Kane County, Illinois, et. al., in August 2004.

¹⁴ The ADID Program was adopted by Kane County Department of Environmental Management, Northeastern Illinois Planning Commission, U.S. Fish and Wildlife Service Chicago Illinois Field Office, and U.S. EPA Region 5 in August 2004.

¹⁵ U.S. Army Corps of Engineers. Technical Report Y-87-2, Corps of Engineers Wetland Delineation Manual, Environmental Laboratory, Department of the Army, 1988.

U.S. Army Corps of Engineers. Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Midwest Region (2010 USACE Midwest Region Manual), Environmental Laboratory, Department of the Army, 2010.

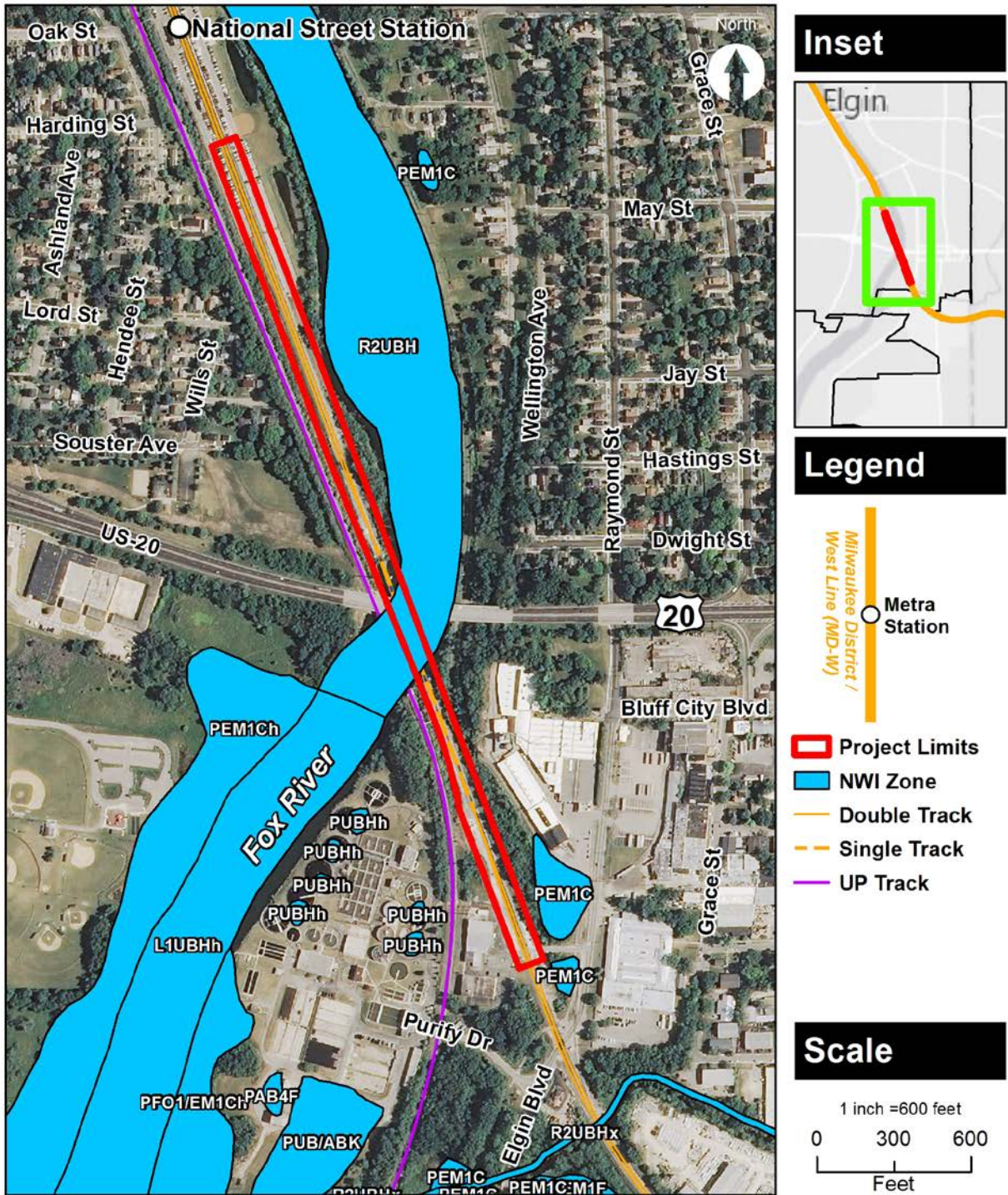


Figure 3-4: National Wetland Inventory (NWI) Map

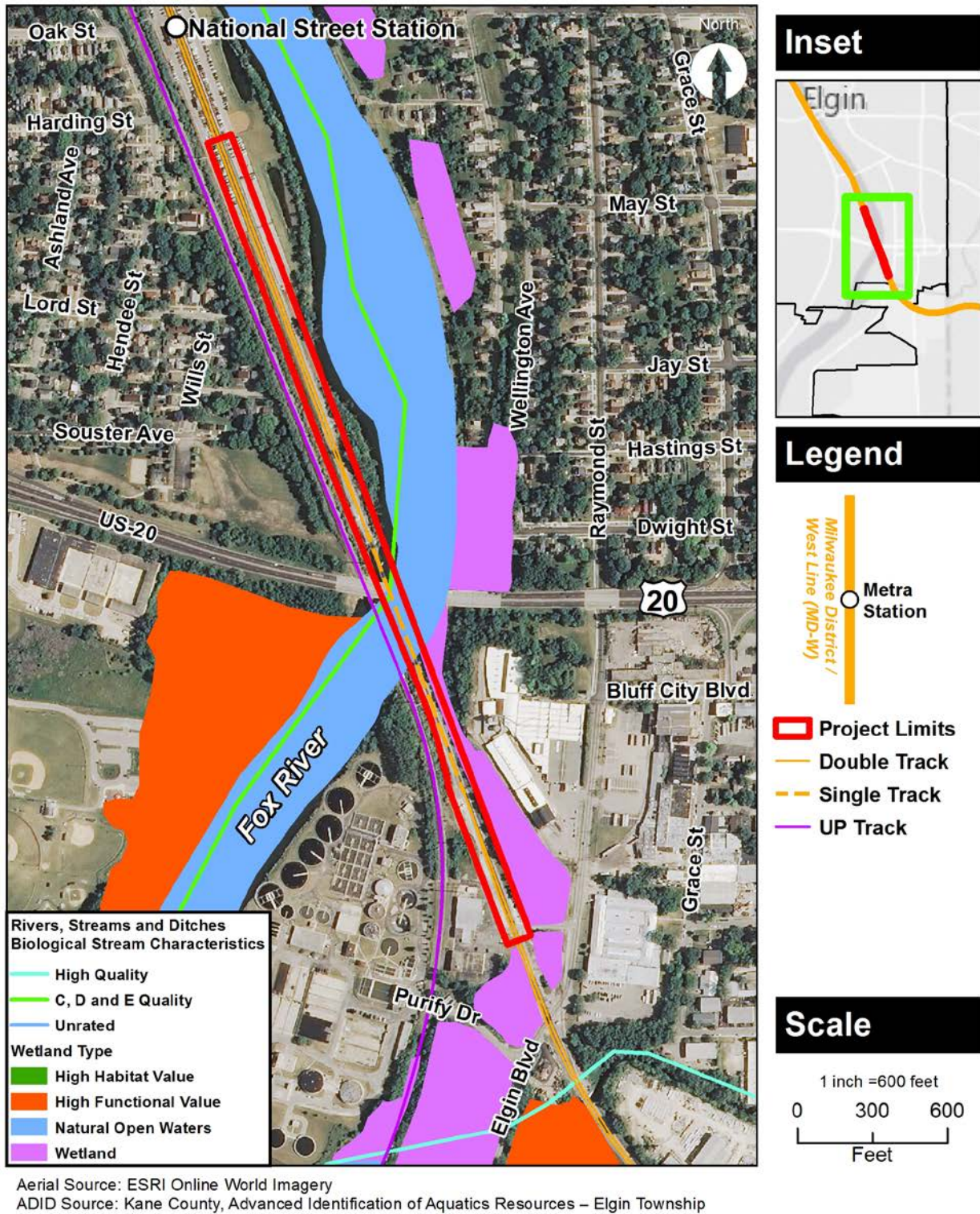


Figure 3-5: ADID (Advanced Identification) Map

3.4.2 Existing Conditions

The National Wetland Inventory (NWI) map (**Figure 3-4**) for the Elgin Quadrangle indicates one wetland adjacent to the proposed project limits. However, the Kane County Advanced Identification Program (ADID) map (**Figure 3-5**) indicates the presence of one wetland within and adjacent to the southeast section of the proposed project limits (Kane County Department of Environmental Management *et al.*, 2004). A field investigation was then conducted on August 25, 2010. This field investigation did not indicate the presence of any wetlands within the project limits. The field investigation supersedes the NWI and ADID maps. Therefore, no wetlands are present within the project limits. However, the Fox River, a WOUS is located within the project area.



Storm and sanitary sewers are not present at the bridge. Stormwater runoff from the bridge directly enters the Fox River and stormwater within the remainder of the project area flows via sheet-flow to the Fox River. The quality of the stormwater runoff is typical of that from railways in urban areas. Stormwater runoff typical of railroads within urban areas includes suspended solids, fuels, oils, and lubricants, metals from wear-and-tear processes and corrosion-resistant poles, and human activities and chemicals from maintenance activities such as herbicide (Tram Vo, *et al.* 2015).

Federal

The Fox River is listed as a navigable WOUS under the jurisdiction of the USACE¹⁶. Work within the Fox River is subject to the requirements of Section 404 of the CWA and Section 10 of the River and Harbors Act of 1899¹⁸.

The Fox River is not a Wild and Scenic River (Interagency Wild and Scenic Rivers Council, 2014) nor is it designated as an Outstanding Resource Waters by the Illinois Pollution Control Board. The Fox River from the Elgin Dam (located approximately 1.5 miles north of the bridge) northward to the West Dundee Dam is included on the Nationwide Rivers Inventory (NRI) due to its recreational opportunities. The bridge is not located within the NRI segment of the Fox River.



State

The Fox River is listed as a Public Body of Water under Title 17 Illinois Administrative Code, Part 3704. The Illinois Draft 2016 Integrated Water Quality Report/Section 303(d) List (IEPA, 2016) identifies the Fox River within the project area (IL_DT-18) as not supporting designated uses of Primary Contact Recreation, Aquatic

¹⁶ USACE, Undated. Navigable Waters of The United States within the Chicago District regulated under Section 10 of the Rivers and Harbors Act of 1899. <http://www.lrc.usace.army.mil/Missions/Regulatory/Navigable-Waters/>. Accessed 10/17/16.

Life, and Fish Consumption. Causes for the non-support finding for the designated uses includes fecal coliform, hexachlorobenzene, mercury, dissolved oxygen, Polychlorinated Biphenyls (PCBs), sedimentation/siltation, and Total Suspended Solids (TSS). The segment of the Fox River within the project area (IL_DT-18) does not have established TMDLs. According to the IDNR Biological Stream Characterization (BSC) the segment of the Fox River within the project area is rated as C for Integrity and Diversity. Stream segments with diversity and integrity ratings of A or B indicate stream segments of exceptional quality or uncommon resources.

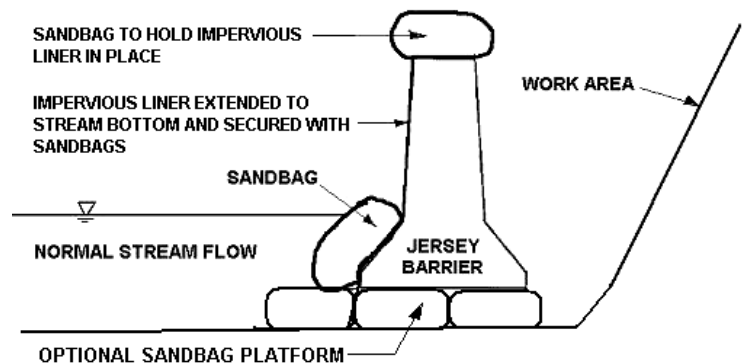
A consultation with the IDNR was conducted through EcoCAT on May 11, 2010 and on June 2, 2014. This process was conducted to obtain information on natural resources such as wetlands. The EcoCAT identified Bluff Spring Fen Illinois Natural Areas Inventory (INAI) Site and Bluff Spring Fen Nature Preserve in the instant May 11, 2010 review results. **Figure 3-6** shows the locations of fens.

3.4.3 Environmental Impacts

The following sections summarize the impacts to water resources for the No Build Alternative and Preferred Build Alternative.

No Build Alternative

The existing masonry piers, structural steel, and three western spans would require rehabilitation or replacement for the No Build condition. There would be temporary impacts to water quality related to this work. Cofferdams (see Example Cofferdam Construction Detail) and causeways may be required to complete the necessary repairs to the bridge to maintain the structure. Sediment within the Fox River is expected to be disturbed temporarily due to maintenance of the piers or through the construction of a



Example Cofferdam Construction Detail

As the Fox River is listed on the 303(d) list for causes of impairment including mercury and PCBs, the sediment would be tested prior to construction to assess whether mercury or PCBs are present. See Section 3.9 for a further discussion of possible hazardous materials.

Federal

A Section 404 and Section 10 permit from the USACE would be required for the work within the Fox River related to the rehabilitation of the bridge piers necessary to maintain the bridge under the No Build Alternative (33 U.S.C. §1344). Early coordination meetings with the USACE occurred on August 29, 2014 and on June 29, 2016. Minutes of these meetings are included in **Appendix C**. Continued coordination with the USACE would occur as the project progresses and would also include coordination through the Section 404 permitting process. It is anticipated this Project would meet the conditions of the USACE Chicago District's Regional Permit Program (RPP) for transportation projects and temporary construction activities. The Regional Permit Program allows for reviews of projects with lesser impacts to occur at the regional level as opposed to the national level. This somewhat streamlined process can allow for a review to be completed

more quickly. The Section 404 permit request would be submitted once design plans are advanced to 60 percent or 90 percent completion, which would occur after completion of the NEPA review process.

State

IDNR reviewed the Project information and concluded that adverse effects are unlikely, and consultation via the EcoCAT was terminated May 12, 2010. Additional consultation with the IDNR on August 12, 2011 and on June 2, 2014 did not result in a change in the identification or impact determination of wetlands (See **Appendix C**). No further consultations with the IDNR related to wetlands are anticipated. A general NPDES Permit for Storm Water Discharges from Construction Site Activities will be required as the construction area is expected to exceed one acre in size.



Aerial Source: ESRI Online Streets Imagery
 ADID Source: Kane County, Advanced Identification of Aquatics Resources – Elgin Township

Figure 3-6: Fen Map

Construction Impacts

The disturbance of sediment within the Fox River due to the rehabilitation of the bridge piers may contribute to the causes of impairment on the 303(d) list. Causes of impairment, including mercury and PCBs, may be present in sediment within the footprint of a proposed cofferdam and temporary causeway. A cofferdam would likely be implemented to improve and maintain the concrete encasements of the piers below the river water level. In addition, disturbance of the sediment may release sedimentation downstream resulting in an increase in suspended solids. Best Management Practices outlined in **Section 3.4.4**, including dewatering, silt curtain, and working in dry, low flow, or no flow conditions, would limit the potential for sediment to be disturbed and released downstream. After construction activities have been completed, these temporary water quality impacts would be expected to cease as no human activities would be disturbing the sediment. In addition, construction would not result in the disposal of hazardous, polluting, or toxic substances within the Fox River.

Permanent Impacts

Permanent impacts to water resources are not anticipated as stormwater runoff and the quality of stormwater runoff would not change.

Preferred Build Alternative

The Preferred Build Alternative would not greatly increase the amount of impervious land coverage or the amount of stormwater runoff entering the Fox River relative to the upstream drainage area. Even with the second track on the new bridge, additional stormwater runoff would be incrementally more than the existing condition. For comparison, the drainage area into the Fox River upstream of the project limits is approximately 1,508 square miles. The quality of the stormwater runoff would be typical of that from railways in urban areas (Tram Vo, et al. 2015) and would not have an impact on water quality to the Fox River as the relative quantity of stormwater runoff from the project is small compared to the total 1,508 square mile drainage area for the Fox River. The total area of the bridge, piers, and abutments is approximately 0.0008 square miles, which is insignificant compared to the total area of the Fox River upstream drainage area of 1,508 square miles. Environmental impacts for the Preferred Build Alternative are anticipated to be similar to the No Build Alternative as neither would have an impact on water quality from stormwater runoff and both involve in-stream work. The Preferred Build Alternative would also reduce the total number of bridge piers by two, lessening the bridge's footprint in the Fox River. The existing bridge has five piers and six spans, while the new bridge would have three piers and four spans.

Federal

A Section 404 and Section 10 permit from the USACE would be required for work within the Fox River related to the new bridge piers. Continued coordination with the USACE would occur as the Project progresses and would also include coordination through the Section 404 permitting process. Metra would be required to obtain a Section 404 permit from the USACE for temporary impacts to the Fox River. It is anticipated this Project would meet the conditions of the USACE Chicago District's Regional Permit Program (RPP) for transportation projects and temporary construction activities. The Section 404 permit request would be submitted once the design of construction documents are advanced to 60 percent or 90 percent completion, which would occur after completion of the NEPA process.

State

IDNR reviewed the Project information and concluded adverse effects are unlikely, and consultation via the EcoCAT was terminated May 12, 2010. Additional consultation with the IDNR on August 12, 2011 and on June 2, 2014 did not result in a change in the identification or impact determination of wetlands (See **Appendix C**). No additional consultation with the IDNR related to water resources is anticipated. A general NPDES Permit for Storm Water Discharges from Construction Site Activities would be required.

Construction Impacts

Construction impacts from the Preferred Build Alternative are anticipated to be similar to the No Build Alternative presented above as both would involve in stream work. The Preferred Build Alternative would involve placing three new piers and removing the five old piers from the Fox River, while the No Build Alternative would involve rehabilitating the five existing piers in place.

Permanent Impacts

Permanent impacts to water quality are not anticipated as the volume of stormwater runoff would not increase substantially compared to the volume of the Fox River, which has an upstream drainage area of approximately 1,508 square miles at the project limits, and the quality of stormwater runoff would not change. In addition, the Preferred Build Alternative would reduce the number of piers in the river by two. The total area of the bridge, piers, and abutments is approximately 0.53 acres, which is very small in comparison with the total area of the Fox River upstream drainage area.

3.4.4 Measures to Avoid or Minimize Harm

To minimize potential impacts to water resources, Best Management Practices (BMPs) would be implemented during removal and construction of the piers. BMPs would include the use of a causeway and cofferdam during construction of the Preferred Build Alternative. The causeway and cofferdam would isolate construction equipment from the Fox River. During construction, Metra would adhere to all requirements of the USACE Section 404 permit. The cofferdam would allow for work to occur in the dry. Sediment Erosion and Sediment Control (SESC) measures would be implemented during construction/maintenance and may include filter bags to filter sediment during dewatering of the cofferdam as well as silt fences and rock check dams. Sediment in the project area would be tested for mercury and PCBs prior to the start of construction. Section 3.9 has further details about the handling of potential hazardous materials that may be encountered during construction.

3.5 Flooding

This section discusses the analysis of how the No Build Alternative and Preferred Build Alternative may impact flooding within the project area. Please see the Water Resources (**Section 3.4**) for information related specifically to water quality, including wetlands.

3.5.1 Regulatory Framework/Methods

Federal Regulations

The Federal Emergency Management Agency (FEMA) delineates and publishes the boundaries of the floodplain and floodway, under Section 1360 of the National Flood Insurance Act of 1968 (42 U.S.C. §4101). The boundaries are published on the FEMA Flood Insurance Rate Maps (FIRM). The FEMA FIRMs are used for floodplain management and insurance purposes to describe the land area in terms of its risk of flooding (FEMA, 2016).

State Regulations

Fill within the floodway and floodplain within Kane County is regulated by the IDNR-Office of Water Resources (OWR) under 17 IL Administrative Code, Title 17, Chapter I, Subchapter h, part 3708, Floodway construction in Northeastern Illinois. The purpose of Part 3708 is to provide rules governing construction and filling in the regulatory floodway of rivers, lakes and streams of Cook, DuPage, Kane, Lake, McHenry and Will Counties. (17 IL, Chapter I, Subchapter h, Section 3708.10).

County Regulations

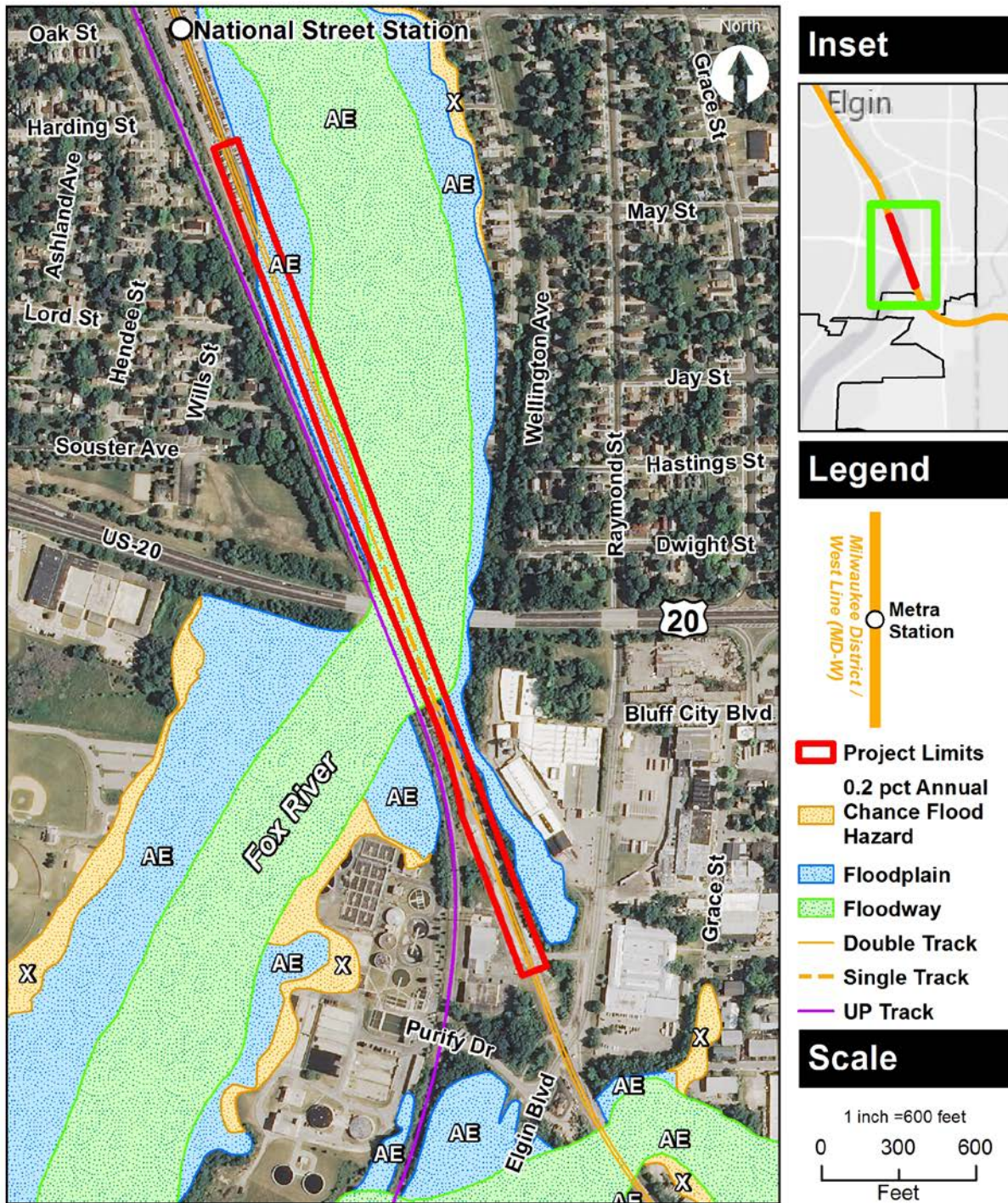
Fill within the floodway and floodplain within Kane County is regulated by the Kane County Water Resources Department under the Kane County Stormwater Management Ordinance.

Methodology

Floodplain data was reviewed (FEMA FIRM) and compared to the proposed project plans to determine potential impacts to floodplains and flooding.

3.5.2 Existing Conditions

The FEMA FIRM shows that the entire project limits approach on the west side of the Fox River, the Fox River Bridge crossing, and a portion of the project limits approach on the east side of the Fox River are in either the Zone AE floodway or Zone AE floodplain of the Fox River (FEMA, 2009). Zone AE floodway is defined as the floodway of the channel of a stream plus any adjacent floodplain areas that must be kept free of encroachment so that the one percent annual chance flood can be carried without substantial increases in flood heights. Zone AE floodplain is defined as the Special Flood Hazard Areas (SFHAs) subject to inundation by the one percent annual chance flood, base flood elevations determined. **Figure 3-7** shows the portion of the floodway/floodplain boundary map for the project area.



Aerial Source: ESRI Online World Imagery
Federal Insurance Rate Map Source:
Federal Emergency Management Agency, Q3 Digital Flood Data, Kane County

Figure 3-7: Flood Insurance Rate Map (FIRM) Map

3.5.3 Environmental Impacts

The following sections summarize the flooding impacts for the No Build Alternative and Preferred Build Alternative.

No Build Alternative

Construction Impacts

The rehabilitation of the existing bridge piers would require temporary placement of fill within the floodway for the temporary causeway. Metra would be required to obtain a permit from the Kane County Water Resources Department and the IDNR-Office of Water Resources (OWR) for fill within the floodway and floodplain of the Fox River. A Kane County Stormwater Management permit request and an IDNR-OWR permit request would be submitted after the completion of the NEPA process.

Permanent Impacts

The No Build Alternative would not require permanent fill within the floodway or floodplain.

Preferred Build Alternative

The Preferred Build Alternative would require placement of fill within the floodway and floodplain for construction of three new piers and abutments. A set of floodplain compensation calculations has been generated by the Project team (See **Appendix E**). These calculations take into account the difference between current floodway volume effects of the existing bridge compared to potential floodway volume effects of the proposed new bridge. Based on these calculations, compensatory storage will be provided to offset fill within the floodway, which will result in no increase in flooding after completion of the project.

Metra would be required to obtain a permit from the Kane County Water Resources Department and the IDNR-Office of Water Resources (OWR) for fill within the floodway and floodplain of the Fox River. A Kane County Stormwater Management permit request and an IDNR-OWR permit request would be submitted after the completion of the NEPA process.

Construction Impacts

The construction of the Preferred Build Alternative would require temporary placement of fill within the floodway for the temporary causeway.

Permanent Impacts

Approximately 4,392 cubic feet of fill would be placed in the floodway below the 10-year floodway elevation for construction of piers and abutments. Approximately 3,096 cubic feet of fill is proposed to be added between the 10-year and 100-year floodway for construction of piers and abutments.

3.5.4 Measures to Avoid or Minimize Harm

The use of compensatory storage, areas that offset any fill in the designated floodway as a result of the

Project, would ensure no changes to the overall floodplain or floodway, which means no changes would be required for the FIRM.

Compensatory storage for fill within the floodplain is required by the IDNR and Kane County Stormwater Management Ordinance.

Compensatory storage for floodway fill from the Preferred Build Alternative would be located on the west bank of the Fox River, adjacent to the existing Metra and Union Pacific Railroad (UPRR) bridges and at the east abutment of the bridge. A gravel access road utilized by both Metra and the UPRR currently terminates near the west bank of the river. This roadway may be shortened to provide the required storage capacity. **Appendix E** includes the compensatory storage plan and calculations. A total of 4,999 cubic feet of compensatory storage would be created below the 10-year floodway elevation, creating an excess of approximately 608 cubic feet of compensatory storage. A total of 3,419 cubic feet of compensatory storage would be created between the 10-year and 100-year floodway, creating an excess of 323 cubic feet of compensatory storage. Removal of the existing piers is included in the calculations as a reduction of fill in the Fox River.

3.6 Biological Resources

This section discusses the analysis of how the No-Build Alternative and Preferred Build Alternative may impact biological resources within the project area. Please see the Water Resources (**Section 3.4**) for information related specifically to water, including wetlands.

3.6.1 Regulatory Framework/Methods

Federal Regulations

The primary regulation concerning biological resources at the federal level is the Endangered Species Act [(ESA); 16 U.S.C. §§1531-1544]. The lead federal agency for implementing the ESA for the listed species within the project area is the US Fish & Wildlife Service (USFWS). The ESA requires federal agencies, in consultation with the USFWS via the Section 7 consultation process, to ensure that actions they authorize, fund, or carry out are not likely to jeopardize the continued existence of any listed species or result in the destruction or adverse modification of designated critical habitat of such species. USFWS has instituted a coordination policy regarding review of federally threatened and endangered species relative to Section 7 of the Endangered Species Act. The USFWS no longer conducts project by project reviews; rather, the applicant is required to conduct the assessment to determine if the project impacts federally-listed species¹⁷. To conduct the review, applicants use Section 7 Consultation guidance, provided on the USFWS website, to document all findings from investigations carried out in the area of the Project.

State Species

The Illinois Endangered Species Act (Illinois ESA; 520 ILCS 10) established the Illinois Endangered Species Protection Board to determine which plant and animal species are threatened or endangered in the state and to advise the Illinois Department of Natural Resources (IDNR) on means of conserving those species. Endangered species coordination is initiated through the IDNR with the submittal of the Project in the IDNRs

¹⁷ USFWS, 2016. Section 7 Consultation. <https://www.fws.gov/Midwest/Endangered/section7/section7.html>. Accessed 10/18/2016.

Ecological Compliance Assessment Tool¹⁸ (EcoCAT). This online system provides a preliminary assessment of whether any biological resources identified by the State are within the project area. If any resources are identified and further confirmed by field visits to the project area, the applicant is required to assess impacts to those resources (520 ILCS 10). The process for assessing impacts to threatened and endangered species in Illinois is completed through the Incidental Take Authorization (ITA) process (520 ILCS 10/5.5).

Methodology

Review of available online data (USFWS Section 7 online consultation process) was conducted to identify biological resources and the potential presence of threatened and endangered species in the project area. Coordination was also conducted with the IDNR via the EcoCAT system. Field assessments and shoreline surveys for mussels were conducted after the initial field investigation discovered a state threatened mussel under the Metra bridge in the Fox River.

3.6.2 Existing Conditions

Federal Species

The USFWS's Chicago Illinois Field Office web site¹⁹ was used to identify federally listed threatened and endangered species known within Kane County, Illinois. On September 1, 2010, the website identified the sheepsnose mussel (Endangered - *Plethobasus cyphus*) and the eastern prairie fringed orchid (Threatened - *Platanthaera leucophaea*) as known within Kane County, Illinois. Upon a subsequent visits to the website the sheepsnose mussel is no longer indicated as known within Kane County, Illinois (November 9, 2015 and March 19, 2016). On May 16, 2011 a NEPA project notification letter was sent to Louise Clemency of USFWS (see **Appendix C**) stating that the Project would not affect critical habitat or the eastern prairie fringed orchid as suitable habitat is not present.

Since 2011, a website consultation was conducted on November 9, 2015 to identify whether additional threatened and endangered species or critical habitat were identified. The Northern long-eared bat (Threatened – *Myotis septentrionalis*) was identified as known within Kane County. Suitable roosting habitat for the Northern long-eared bat is present within the project limits.

State Species

A consultation with the IDNR was conducted through EcoCAT on May 11, 2010 to determine if any state-listed species were present within the project area. The EcoCAT identified the Black-crowned night heron (Endangered - *Nycticorax nycticorax*), Elfin skimmer (Threatened - *Nannothemis bella*), and Osprey (Endangered - *Pandion haliaetus*) in the instant review results. IDNR reviewed this information and concluded adverse effects are unlikely due to the species' range and the lack of existing habitat within the project limits. Consultation with IDNR was terminated May 12, 2010. An updated EcoCAT submittal was submitted on August 12, 2011. The instant review response from the EcoCAT from August 12, 2011 (See **Appendix C**) shows the black-crowned night heron, the Osprey, and the spike mussel as potentially occurring within the project limits. An updated EcoCAT from June 2, 2014 showed the same three species.

A field visit was conducted August 25, 2010, and a spike mussel (State Threatened - *Elliptio dilatata*) was

¹⁸ EcoCAT is available at the following web site: <http://www.dnrecocat.state.il.us/ecopublic/>.

¹⁹ USFWS's Chicago Illinois Field Office web site is available at: <http://www.fws.gov/midwest/Chicago/>.

located along the east bank of the Fox River under the existing Metra bridge. No other live mussels were noted during the site investigation. A secondary field visit was conducted on September 23, 2010 to assess the potential for the Project to impact the habitat of the spike mussel. The assessment and shoreline/partial mussel survey, completed by Huff & Huff, Inc., revealed no live spike mussels in the river, but fresh dead shells were gathered at the site. This information was forwarded to the IDNR for further consultation on August 5, 2011.



Photographs of the Spike mussel found along the east bank of the Fox River under the existing Metra railroad bridge

The IDNR responded on August 17, 2011 concerning the presence of the spike mussel and the August 12, 2011 EcoCAT submittal (See **Appendix C**). The IDNR indicated that the consultation process would remain open as there may be potential adverse impacts to the spike mussel related to the proposed work within the Fox River.

3.6.3 Environmental Impacts

The following sections summarize the impacts to biological resources for the No Build Alternative and Preferred Build Alternative.

No Build Alternative

Federal Species

The No Build Alternative is not likely to adversely affect the Eastern prairie fringed orchid or the Northern long-eared bat (NLEB). Suitable habitat for the Eastern prairie fringed orchid is not present within or adjacent to the project area as documented in the Section 7 Consultation letter (see **Appendix C**). The Northern long-eared bat finding is based on a bridge inspection and commitment to remove trees, if needed, between August 1st and May 31st. The bridge inspection conducted on May 16, 2016, consistent with the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and Federal Transit Administration (FTA) Range-wide Programmatic Informal Consultation for Indiana Bat and Northern Long-eared Bat (USFWS, FTA, FRA, & FHWA 2016), found no evidence of bat activity (See **Appendix C**). The NLEB Project Submittal Form for FHWA, FRA, and FTA, was submitted to the USFWS on August 22, 2016. Additionally, on March 15, 2016, the USFWS Chicago office indicated that there are no known NLEB maternity roost trees or hibernacula within the six county Chicago metropolitan area. Tree trimming/removal may be required as result of the required minor rehabilitation or replacement of existing masonry piers, structural steel, and the three western spans of the existing bridge.

State Species

The No Build Alternative would result in temporary impacts from the required rehabilitation of existing masonry piers, structural steel, and three western spans of the existing bridge. These upgrades are required to modernize the existing bridge. Impacts may result from causeways or cofferdams for rehabilitation of bridge piers and removing and replacing bridge spans.

Construction Impacts

Construction for the rehabilitation of existing masonry piers would require a temporary causeway and cofferdam. Construction would occur within the existing Metra right-of-way and within the Fox River. Based on coordination with the IDNR regarding potential adverse impacts to the spike mussel, Metra would be required to request an Incidental Take Authorization (ITA) from the IDNR prior to the commencement of construction activities. The IDNR would require that the threatened and endangered species consultation remain open pending the completion of the ITA (See **Appendix B** for the March 28, 2014 meeting minutes).

The INDR indicated that the ITA does not have to be in place before the completion of the NEPA process, but has to be complete before construction commences. The ITA process would take approximately six months and would be required for the spike mussel. The IDNR suggested that the ITA cover all listed mussels in the Fox River in the event that other species are encountered before and during construction. Mitigation measures for the potential adverse impacts to the spike mussel would be detailed in the ITA and would most likely include the translocation of all live native mussels from the construction area. The survey and relocation of native mussels (including the spike mussel) within the project area prior to construction would result in no adverse impacts from construction activities on the spike mussel.

The ITA process is initiated through the development of a Conservation Plan specific to the proposed project and its potential impacts on the spike mussel. As part of the ITA process, the Project would require a public comment period to solicit comments concerning the Project and its potential impacts to the spike mussel. This public comment period is initiated through a formal Public Notice request to be filed in a regional newspaper such as the *Chicago Sun Times* and in local newspapers near the Project. Metra must sufficiently address any comments received from this public notice period prior to obtaining the ITA from the IDNR.

The proposed Conservation Plan would include mussel surveys prior to construction. The goal of the survey is to identify and capture all live native mussels and relocate them to suitable, similar habitat in the Fox River. The relocation area would be determined with assistance from the IDNR and would be located typically in areas upstream from the Project to protect them from potential impacts during construction. The survey and relocation of native mussels (including the spike mussel) within the project area prior to construction would result in no adverse impacts from construction activities on the spike mussel.

Permanent Impacts

Permanent impacts to threatened and endangered species are not anticipated. The survey for spike mussels prior to construction would relocate any spike mussels found within the construction area.

Preferred Build Alternative

Federal Species

Similar to the No Build Alternative, the Preferred Build Alternative is not likely to adversely affect the Eastern prairie fringed orchid or the Northern long-eared bat.

State Species

The Preferred Build Alternative would require in-stream work within the Fox River for removal of the existing bridge piers and construction of new bridge piers. As with the No Build Alternative, an ITA from the IDNR would be required for potential adverse impacts to the spike mussel from the removal and construction of the new bridge piers.

Construction Impacts

Construction of the Preferred Build Alternative would not result in adverse impacts to biological resources with the implementation of BMPs, the ITA, and development of a Conservation Plan for the spike mussel, as described in the Section 3.6.3, No Build section. Construction would primarily occur within the existing Metra right-of-way, on existing railroad property owned by the UPRR that would be acquired for the Project, or within the Fox River. The survey and relocation of native mussels (including the spike mussel) within the project area prior to construction would result in no adverse impacts from construction activities.

Permanent Impacts

Permanent impacts to threatened and endangered species are not anticipated. The survey for spike mussels prior to construction would relocate any spike mussels found within the construction area, as described in Section 3.6.3, No Build section.

3.6.4 Measures to Avoid or Minimize Harm

Federal Species

No impacts are anticipated, and accordingly there is no mitigation associated with biological resources under federal regulations.

State Species

To mitigate the potential impacts to the spike mussel from the Project, Metra would be required to implement the Conservation Plan developed as part of the ITA for the spike mussel. The Conservation Plan would require Metra to conduct a mussel survey in the Fox River prior to construction. Any spike mussels or live native mussels would be collected from the proposed work areas (including causeways and piers). Once these mussels are collected, they would be immediately relocated to suitable habitat upstream of the work. The mussel relocation would occur far enough from the Project so as not to be affected by any activities related to the bridge work. If other state threatened or endangered mussels are collected during the survey, Metra contractors would notify the IDNR immediately and take necessary steps to relocate these species along with the other native mussels. It is unlikely that other state listed mussels would be found near the

bridge based on early coordination on the Project with the IDNR in which no listed mussels were identified in this area of the Fox River.

Upon completion of the mitigation/relocation, a summary of the mitigation activities would be forwarded to the IDNR. Metra would also commit to conducting annual monitoring of the trans-located mussels for up to one year after completion of the bridge project. The results of the monitoring would be forwarded to the IDNR.

3.7 Noise

This section describes the predicted noise impacts of the Project. Noise is "unwanted sound," generally measured in terms of loudness. The loudness, or magnitude, of noise determines its intensity and is measured in decibels (dB). The overall noise level from transit sources is described in A-weighted decibels [dB(A)]. The A-weighted decibel scale was developed to better approximate the sensitivity of human hearing. Because the decibel is based on a logarithmic scale, a 10-dB increase in noise level is generally perceived as a doubling of loudness, while a 3-dB increase in noise is just barely perceptible to the human ear (FTA, *Transit Noise and Vibration Impact Assessment*, 2006).

3.7.1 Regulatory Framework/Methods

Metra analyzed noise impacts from the Project in accordance with the FTA guidance manual, *Transit Noise and Vibration Impact Assessment* (FTA, 2006). The FTA guidance manual sets forth the basic concepts, methods, and procedures for evaluating the extent and severity of the noise impacts resulting from transit projects.

FTA thresholds for noise impacts depend on existing noise levels. Under the FTA guidance manual, as existing noise levels increase, the allowed increase in transit noise exposure decreases. The Project would upgrade an existing rail corridor where trains are currently generating noise. Because existing noise levels from Metra operations are relatively high, noise impacts may be caused by relatively small increases in noise or vibration exposure.

For this assessment, Metra first identified noise-sensitive receivers in the project area. The FTA *Transit Noise and Vibration Impact Assessment* guidance manual recommends a screening distance of 750 feet to delineate the study area for a commuter rail project in an area without intervening buildings. Therefore, this noise-sensitive receiver identification process used a distance of 750 feet. In addition, FTA defines three different land use categories for identifying noise-sensitive receivers:

- Category 1 - Tracts of land set aside for serenity and quiet, such as outdoor amphitheaters, concert pavilions, and historic landmarks.
- Category 2 - Buildings used for sleeping, including residences, hospitals, hotels, and other areas where nighttime sensitivity to noise is of utmost importance.
- Category 3 - Institutional land uses with primarily daytime and evening uses including schools, libraries, churches, theaters, museums, cemeteries, historical sites and parks, and certain recreational facilities used for study or meditation.

The identified noise-sensitive receivers were then grouped into clusters when the receivers were determined to be similar distances from the existing and proposed future tracks and where the Metra operating conditions, such as train speed, were determined to be similar.

The second step in the noise assessment was to determine existing noise conditions. Noise measurements were taken at each receptor site in the project area to establish the existing background noise conditions at the clusters of noise-sensitive receivers. The overall existing noise level was determined by adding the modeled train noise levels to the monitored background noise level. Metra then used these overall existing noise levels to determine the impact thresholds at each cluster of noise-sensitive receivers.

The third step in the noise assessment was to predict future noise levels and identify predicted noise impacts. Noise modeling, consisting of a spreadsheet-based computer model, using FTA general assessment procedures, was conducted to predict future levels at each cluster of noise-sensitive receivers. Field noise monitoring levels were input along with anticipated future conditions to predict future noise levels. By comparing existing and predicted noise levels, Metra determined locations where predicted noise increases would constitute an impact. The FTA noise criteria identify two categories of impacts: moderate and severe. A moderate impact occurs where the change in noise would be noticeable, but might not be sufficient to cause a strong, adverse community reaction. A severe impact would occur where noise levels occur above which a substantial percentage of the population would be highly annoyed by new noise.

The final step in a noise assessment was to recommend mitigation measures. As noted in the FTA guidance manual, mitigation measures should be considered when moderate impacts are predicted and implemented when severe impacts are predicted unless there are compelling reasons why mitigation would not be feasible. Metra's analysis did not identify any impacts, so the analysis of feasible noise mitigation measures was not required.

3.7.2 Existing Conditions

There are three clusters of noise-sensitive receivers within 750 feet of the alignment, including two residences and one park. All three individual noise-sensitive receivers are identified and displayed in **Figure 3-8**.

The dominant noise source in the project area is train noise from the existing rail line. Metra trains operate during daytime and early daytime/nighttime hours while freight trains can operate 24 hours a day.

Metra conducted short-term (1-hour) noise measurements to document existing noise exposure at noise-sensitive receivers within 750 feet of the alignment. Short-term measurements were conducted at all three receptor sites in the project area. The measurement sites were chosen to represent different noise environments throughout the project area. The short-term measurements were used to estimate the existing noise levels at representative noise-sensitive receivers.

The estimated existing noise levels range from L_{dn} 58 dB(A) at the farthest noise-sensitive receivers to L_{dn} 70 dB(A) at the closest noise-sensitive receivers. **Table 3-2** provides a summary of the noise levels.

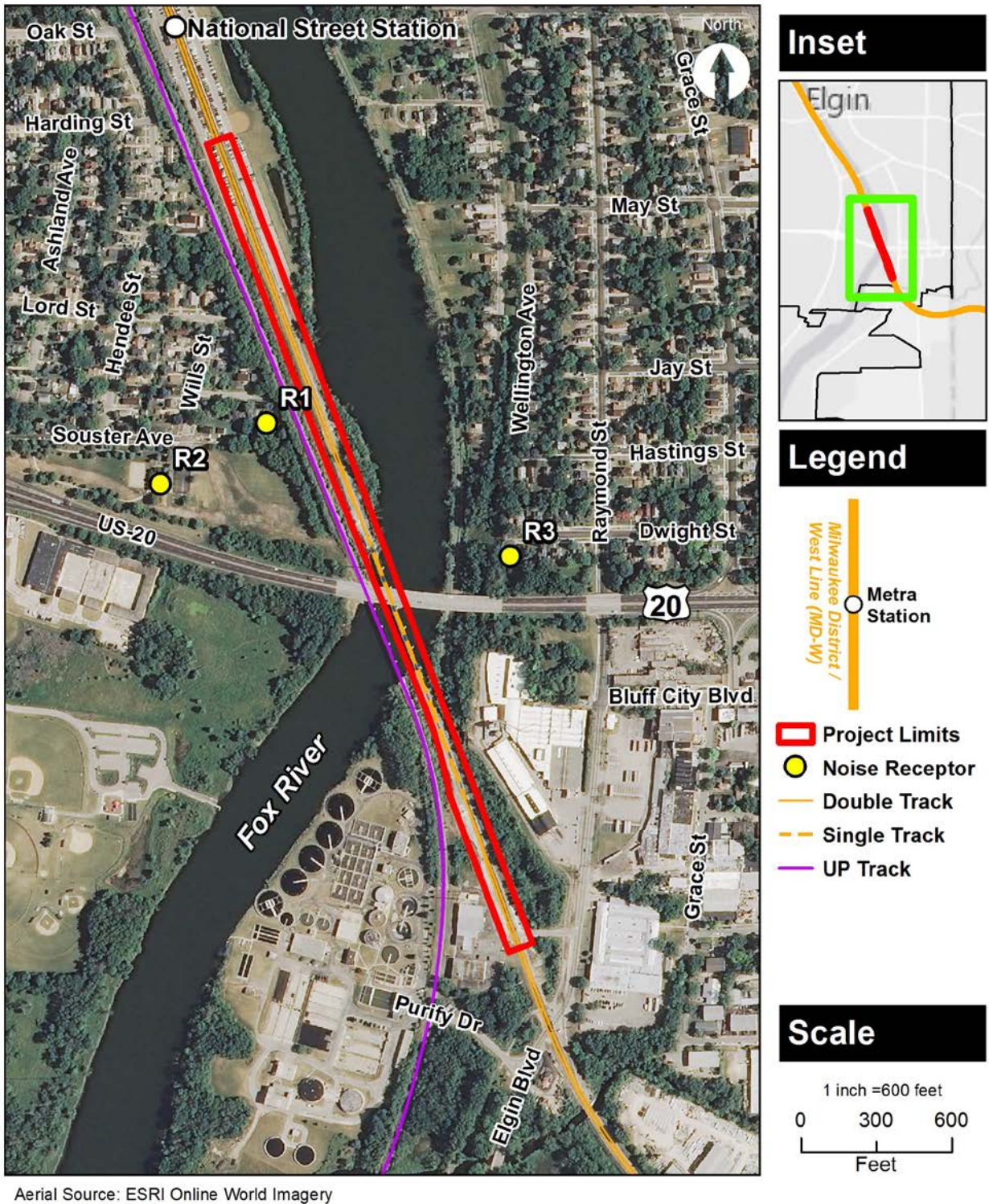


Figure 3-8: Noise Receptor Locations

3.7.3 Environmental Impacts

The following sections summarize the potential noise impacts of the No Build Alternative and Preferred Build Alternative.

No Build Alternative

Construction Impacts

Under the No Build Alternative, rehabilitation construction activities would still be required to maintain the existing structure. The No Build Alternative would result in minor temporary impacts on the surrounding neighborhoods due to construction activities. Temporary construction noise impacts would be due to demolition and construction, and construction vehicles. The construction activities would be limited to daytime hours where feasible, though night and/or weekend work may be needed during excavation, replacement of structural bridge spans, or other activities. If any planned work conflicts with the City of Elgin's noise ordinance, Metra would coordinate with the City to resolve the issue. Truck traffic would be primarily present along major roads near the project area and would use a defined access path to reach the project limits, likely along the existing right-of-way.

Permanent Impacts

There is no predicted change in noise levels for the No Build Alternative. The noise levels for the No Build Alternative would not change over existing conditions because there would be no projected change to traffic or track configuration, and therefore no noise impact would be predicted. No noise reduction would occur as a result of the No Build Alternative.

Preferred Build Alternative

Construction Impacts

Temporary noise impacts are likely to occur due to construction activities. Temporary construction noise impacts would be due to demolition and construction, and construction vehicles. The construction activities would be limited to daytime hours where feasible, though night and/or weekend work may be needed during track cutover, piling, excavation, deep foundation work, or other activities. If any planned work conflicts with the City of Elgin's noise ordinance, Metra would coordinate with the City to resolve the issue. Truck traffic would be primarily present along major roads near the project area and would use a defined access path to reach the project limits, likely along the existing right-of-way.

Permanent Impacts

Under the Preferred Build Alternative, no expansion of train services would occur. There were three noise-sensitive receiver clusters identified within 750 feet of the alignment. **Table 3-2** summarizes the findings of the general noise assessment completed for the Project. The Preferred Build Alternative noise impacts were evaluated for the receptor locations. The background noise level is based on monitoring at each location. The noise level from trains when the Project is built would be 70 dB(A) at R1, 62 dB(A) at R2, and 58 dB(A) at R3. These noise levels result in a projected overall build noise level (which includes noise from passenger and freight trains and other background noise) of 70 dB(A), 65 dB(A), and 61 dB(A), respectively. The projected

overall build noise levels do not change from the existing overall noise levels at any of the receptor locations. Consequently, there are no noise impacts associated with the proposed improvement.

Table 3-2: Existing and Predicted Noise Levels and Moderate and Severe Impacts at Noise-Sensitive Receiver Clusters

Receptor Location	Receptor Type	Noise Metric	Adjusted Background Noise, dB(A)	Existing Train Noise Level ¹ , dB(A)	Build Train Noise Level ¹ , dB(A)	Overall Existing Noise Level ² , dB(A)	Overall Build Noise Level ² , dB(A)	Overall Build Noise Increase over Existing Noise Level ² , dB(A)	Allowable Noise Level Increase (Mod./Sev.)	Impact Assessed
R1	Single-Family Residence	Ldn	50	70	70	70	70	0	1/3	No Impact
R2	Park	Leq	62	62	62	65	65	0	3/7	No Impact
R3	Single-Family Residence	Ldn	58	58	58	61	61	0	2/5	No Impact

1 Includes both freight train and passenger train noise.
2 Includes background noise, freight train noise, and passenger train noise.

3.7.4 Measures to Avoid or Minimize Harm

Mitigation measures for permanent increases in noise are considered when moderate impacts are predicted; noise mitigation must be implemented where severe impacts are predicted unless there are compelling reasons why mitigation measures are not feasible. As there are no impacts resulting from the Project, no mitigation is required.

For both the No Build and Preferred Build Alternatives, construction BMPs would be implemented to minimize the temporary construction noise impacts. These BMPs include conducting construction activities during daytime hours, where and when possible, coordinating with the City of Elgin on construction activities as they relate to local ordinances, and providing advance notification to the public of upcoming construction operations and schedules.

3.8 Vibration

This section describes the predicted vibration impacts of the Project. Ground-borne vibration can be caused by the vibration of a railroad structure, creating vibration waves that propagate through the soil and rock to the foundations of nearby buildings. The vibration of floors and walls may cause perceptible vibration, rattling of items such as windows or dishes on shelves, a rumble noise, or damage to buildings in extreme cases. Vibration is described in terms of velocity (Lv) and is measured in decibels (VdB), which is the root mean square vibration velocity relative to 1 microinch per second (FTA, *Transit Noise and Vibration Impact Assessment*, 2006).

3.8.1 Regulatory Framework/Methods

Metra analyzed vibration impacts from the Project in accordance with the *Transit Noise and Vibration Impact Assessment* guidance manual (FTA, 2006). The FTA guidance manual sets forth the basic concepts, methods, and procedures for evaluating the extent and severity of vibration impacts resulting from transit projects. The Project would upgrade an existing rail corridor that generates relatively high levels of existing vibration.

In conducting the analysis, Metra first identified vibration-sensitive receivers in the project area. FTA defines three land use categories to identify vibration-sensitive receivers, and defines screening distances for commuter railroad projects for each category:

- Category 1 - Buildings where vibration would interfere with operations (600 feet screening distance).
- Category 2 - Buildings used for sleeping, including residences, hospitals, hotels, and other areas where nighttime sensitivity to vibration is of utmost importance (200 feet screening distance).
- Category 3 - Institutional land uses with primarily daytime and evening uses including schools, libraries, churches, museums, cemeteries, historical sites, and certain recreational facilities used for study or meditation (200 feet screening distance).

After completing the screening process, Metra identified one vibration-sensitive receiver (receptor R1). Receptor R1 is a Category 2 receiver, and was identified within the FTA vibration screening distance (200 feet). The analysis process from this point forward is based on this Category 2 receiver (R1). The location of the vibration-sensitive receiver cluster is noted as R1 in **Figure 3-8**.

The FTA vibration criteria levels are defined in terms of human annoyance for the different vibration-sensitive receiver land use categories and unlike noise impacts, the criteria only contain one threshold for identifying impacts. In general, the vibration threshold of human perceptibility is approximately 65 VdB. The FTA vibration impact threshold for Category 2 land uses, including residences, is 72 VdB. Where existing vibration levels exceed the FTA impact threshold, guidance is to identify an impact only where there is more than a 3 VdB increase in vibration level.

The second step in the vibration assessment was to predict vibration levels and identify predicted vibration impacts at the vibration-sensitive receiver. By comparing existing and predicted vibration levels, Metra determined locations where predicted vibration levels would constitute an impact. The final step in a vibration assessment is to recommend mitigation measures. As provided in the FTA guidance manual for vibration impacts, mitigation measures would be developed in the following cases: (1) where existing vibration levels are lower than FTA thresholds and the future vibration levels would be above those thresholds, and (2) when the existing vibration is already higher than the FTA threshold, and the future vibration would be more than 3 VdB greater than the existing vibration. For predicted vibration impacts, the goal is to reduce predicted vibration levels to below the applicable FTA vibration impact threshold.

3.8.2 Existing Conditions

Vibration levels were modeled at the representative receptor (R1) in the project area to determine existing vibration levels at the vibration-sensitive receiver. Existing vibration levels were determined based on existing

train traffic data. The modeled vibration levels indicated the existing vibration levels exceed the FTA impact threshold of 75 VdB for Category 2 land uses (residential and other similar nighttime vibration-sensitive locations) at the representative receptor. See **Table 3-3**.

3.8.3 Environmental Impacts

The following sections summarize the potential vibration impacts of the No Build Alternative and Preferred Build Alternative.

No Build Alternative

Construction Impacts

Under the No Build Alternative, rehabilitation construction activities would still be required to maintain the existing structure. The No Build Alternative would result in minor temporary impacts on the surrounding neighborhoods due to construction activities. Temporary construction vibration impacts would be due to demolition and construction, and construction vehicles. The construction activities would be limited to daytime hours, where and when feasible, though night and/or weekend work may be needed during excavation, replacement of structural bridge spans, or other activities. Truck traffic would be primarily present along major roads near the project area and would use a defined access path to reach the project limits, likely along the existing right-of-way.

Permanent Impacts

There is no predicted change in vibration levels for the No Build Alternative and no vibration impact is predicted as there would be no changes in train volumes or track configuration.

Preferred Build Alternative

Construction Impacts

Temporary vibration impacts are likely to occur due to construction activities. Temporary construction vibration impacts would be due to demolition and construction, and construction vehicles. The construction activities would be limited to daytime hours, where and when feasible, though night and/or weekend work may be needed during track cutover, piling, excavation, deep foundation work, or other activities. If any planned work will conflict with the City of Elgin's local ordinance, Metra would coordinate with the City to resolve the issue. Truck traffic would be primarily present along major roads near the project area and would use a defined access path to reach the project limits, likely along the existing right-of-way.

Permanent Impacts

Permanent impacts from vibration levels are not expected for the Preferred Build Alternative. The only sensitive receiver identified within 200 feet of the alignment is predicted to have vibration levels that already exceed the FTA impact threshold, as presented in **Table 3-3**. However, the Project would not increase vibration levels. Therefore, this sensitive receptor is not considered to be impacted by the Project.

Table 3-3: Existing and Predicted Vibration Levels and Impacts at the Vibration-Sensitive Receiver Cluster

Vibration-Sensitive Receiver Cluster ID	Vibration-Sensitive Receiver Cluster Description	Distance to Nearest Mainline Track Structure Column (feet)	Existing Lv (VdB)	Proposed Lv (VdB)	FTA Impact Threshold ¹ (VdB)	Increase in Vibration Levels (VdB)	Impact Level
R1	Residence	75	88	88	75	0	No Impact

Lv = vibration velocity level; VdB = root mean square vibration velocity in decibels relative to 1 microinch per second
¹ Source: FTA 2006

3.8.4 Measures to Avoid or Minimize Harm

As there are no vibration impacts resulting from the Project, no mitigation is required.

Construction BMPs would be implemented to minimize the temporary construction vibration impacts. These BMPs include conducting construction activities during daytime hours, where possible, coordinating with the City of Elgin on construction activities as they relate to local ordinance, and providing advance notification to the public of upcoming construction operations and schedules.

3.9 Hazardous Materials

This section discusses the potential for encountering hazardous materials during project construction and implementation. Hazardous materials may include petroleum products, pesticides, organic compounds, heavy metals, asbestos-containing materials, lead paint, or other compounds that could harm human health or the environment. The nature and extent of contamination can vary widely. Early detection, evaluation, and determination of appropriate remediation of hazardous materials is essential.

3.9.1 Regulatory Framework/Methods

Federal and state laws regarding hazardous materials have been established for the protection of human health and the environment. At the federal level, the regulations include the Resource Conservation and Recovery Act (RCRA) (42 U.S.C. §6901, et seq., 1976); the Comprehensive Environmental Response, Compensation, and Liability Act (CERCLA) (42 U.S.C. §9601, et seq., 1980); the Superfund Amendments and Reauthorization Act (SARA) (42 U.S.C §9601, et seq., 1985); the Clean Air Act (42 U.S.C. §7401, et seq., 1970); the Toxic Substances Control Act (15 U.S.C. §2601, et seq., 1976); and the Federal Occupational Safety and Health Act (29 U.S.C. §651, et seq., 1970).

At the state level, regulations and programs include the Illinois Environmental Protection Act (415 ILCS 5, et seq.) and the Illinois Occupational Safety and Health Program (820 ILCS 219), with oversight by the Office of the State Fire Marshal.

Locally, the City of Elgin Police Department, City of Elgin Fire Department, and the City of Elgin Department of Public Health regulate and oversee issues related to hazardous materials.

A review of federal, state, and local regulatory databases was conducted during the summer of 2010 and updated in May 2016²⁰ to identify sites that currently or have historically handled, stored, transported, released, or disposed of hazardous or regulated materials, as these types of sites are potential sources of hazardous material contamination.

Specific sites within a quarter mile of the Project where hazardous materials are known or suspected to exist were evaluated for the potential for hazardous materials to be present. Each site was assigned a level of concern based on the following criteria:

- **High Concern** - Sites with known/probable soil, groundwater, or soil gas contamination that have not been remediated, or where remediation was incomplete or undocumented. Other considerations include the type and mobility of any contamination, distance to the Project, and groundwater impacts.
- **Moderate Concern** - Sites with known/potential soil, groundwater, or soil gas contamination and where remediation is in progress or was completed with restrictions in place, or contaminants do not appear to pose a concern for the Project. Sites may also be considered a Moderate Concern based on the type and intensity of former land use (e.g., chemical manufacturers, machine shops, gas stations), even if they did not otherwise have an environmental database listing.
- **Low Concern** - Sites where hazardous materials or petroleum products may have been or are stored, but where there is no known contamination associated with the site based on all available information. They may include hazardous material generator sites, sites with permitted air toxic emissions or sites with spills or leaks that were subsequently remediated and are no longer a concern.

Separation distance from the project limits is also considered when assessing sites. Separation distance is determined by measuring the distance from the project limits to the property boundary.

Polychlorinated biphenyls, lead-based paint, and asbestos-containing material are likely to occur in transformers and buildings constructed before 1978–1979 as manufacturing of PCBs was stopped in 1977²¹. The project area was evaluated for potential impacts associated with these hazardous materials by determining whether transformers and buildings potentially constructed before 1978–1979 were present.

3.9.2 Existing Conditions

The federal and state databases named above were searched in 2010 and again in May 2016 to identify potential sites of concern within a quarter mile of the project limits. Using the impact analysis criteria described above, sites were initially identified by FirstSearch Technology Corporation (2010) and were reviewed and classified as High, Moderate, or Low Concern based on their potential to act as a source of contamination to the Project. The sites near the project area were again reviewed in May 2016 to update potential database listings. The databases checked online included the RCRA, CERCLIS, SRP, LUST, and SPILLS

²⁰ A database search was conducted by FirstSearch Technology Corporation of 24 environmental databases in 2010 and the RCRA, CERCLIS, SRP, LUST, and SPILLS databases were searched in 2016. Detailed information on the results of the searches can be found in **Appendix F**.

²¹ Toxic Substances Portal – PCBs. 2015. Agency for Toxic Substances & Disease Registry. <https://www.atsdr.cdc.gov/phs/phs.asp?id=139&tid=26>. Accessed 10/17/16.

databases and are included with the FirstSearch Technology Corporation database review in **Appendix F**.

Nine sites were identified within a quarter mile of the project area and summarized in **Table 3-4**. The Metra railroad and Fox River are located within the project limits, with the next closest sites, Alphabet Shop Inc. and Elgin Corrugated Box, located adjacent to the project limits. Overall, the review identified six Moderate Concern sites, and three Low Concern. None of the sites was classified as High Concern.

The sites identified within a quarter mile of the project limits are listed in **Table 3-4**, below:

Table 3-4: Potential Hazardous Material Sites

Site Name	Address	Database	Distance	Status	Reason
Metra Railroad	NA	NA	Within limits	Moderate Concern	Railroad and railroad signal boxes
Fox River	NA	IEPA 303(d) list	Within limits	Moderate Concern	Potential presence of PCBs and mercury
Alphabet Shop Inc.	300 E. Elgin Ave, Elgin, Illinois	RCRA CESQG	Adjacent	Moderate Concern	RCRA CESQG of spent solvents and MEK
Elgin Sanitary District	Raymond St and Purify Dr., Elgin, Illinois	RCRA Non Gen	150 feet	Low Concern	RCRA non-generator of cadmium, downgradient location
Elgin Corrugated Box	824 Raymond Street, Elgin, Illinois	SRP	Adjacent	Moderate Concern	SRP site, proximity to project area
Fox Group II	363 Bluff City Boulevard, Elgin, Illinois	LUST, RCRA	350 feet	Moderate Concern	LUST incident, proximity to project area
Fox Group II	901 Raymond Street, Elgin, Illinois	LUST	250 feet	Moderate Concern	LUST incident, proximity to project area
IL Central Management Service Department of Vehicles	595 S. State Street, Elgin, Illinois	RCRA Non Gen	0.16 miles	Low Concern	Separation distance/ RCRA non-generator
Illinois Department of Transportation	595 S. State Street, Elgin, Illinois	RCRA Non Gen	0.16 miles	Low Concern	Separation distance/ RCRA non-generator

The Metra railroad, and associated signal boxes, is considered a Moderate Concern site due to potential for past herbicide and pesticide usage, chemicals associated with railroad tie preservation, and potential metals associated with railroad boxes.

The Fox River is listed as a Public Body of Water under Title 17 Illinois Administrative Code, Part 3704. Waters that do not, or are not anticipated to, meet applicable water quality standards are considered impaired and are cataloged in the 303(d) list, requiring state regulators to develop total maximum daily loads (TMDLs). TMDLs establish pollution reduction goals to improve the quality of impaired waters. The Illinois 2016 Integrated Water Quality Report/Section 303(d) List (IEPA, 2016) identifies the Fox River within the project area (IL_DT-18) as not supporting designated uses of Primary Contact Recreation, Aquatic Life, and Fish Consumption. Causes for the non-support finding for the designated uses includes fecal coliform, hexachlorobenzene, mercury, dissolved oxygen, Polychlorinated Biphenyls (PCBs), sedimentation/siltation, and Total Suspended Solids (TSS). Based upon the proximity to the project area and potential presence of hexachlorobenzene, PCBs and mercury, this site is considered a Moderate Concern.

The Alphabet Shop, Inc. was listed in the RCRA database as a conditionally exempt small quantity RCRA generator of spent solvents, methyl ethyl ketone, and ignitable waste. Due to chemical usage on site and close proximity to the project area, this site is considered a Moderate Concern.

LUST incidents occurred at the Fox Group II facilities at 363 Bluff City Boulevard and 901 Raymond Street. Both of these incidents have received No Further Remediation (NFR) letters; however, due to the close proximity to the project area (under 500 feet), these sites are considered Moderate Concern.

The Site Remediation Program (SRP) site at Elgin Corrugated Box has been closed and received an NFR letter. Due to the close proximity and presence of an NFR letter this site is considered Moderate Concern.

The remaining three sites all appeared on the RCRA database as non-generators. No materials were listed for the IL Central Management Service Department of Vehicles and the Illinois Department of Transportation. For the Elgin Sanitary District Site, the waste listed was cadmium. Based upon the nature of these listings and separation distance, these sites are considered Low Concern.

During the site visits on August 25, 2010 and May 16, 2016, the project limits were examined for evidence of any impact by hazardous materials. There was no evidence of dumping or ground staining except on the east side of the Fox River within the project limits. Discarded railroad ties were abandoned in the vegetated ditch between the Metra Milwaukee West Line tracks and the UPRR tracks. In addition, a small area (approximately 25 square feet) adjacent to the discarded railroad ties contained an estimated 10-gallon rusted container and oily refuse. The Project improvements are not expected to disturb the soil in this particular area.

3.9.3 Environmental Impacts

The following summarizes the potential impacts from hazardous materials for the No Build Alternative and Preferred Build Alternative. Construction impacts refer to the potential to encounter hazardous waste during the construction phase of the Project and permanent impacts refer to potential to introduce new sources of hazardous waste and/or spread existing sources of hazardous waste to new areas that would remain after construction of the Project is completed.

No Build Alternative

Six sites of Moderate Concern have been identified near the project area. As the No Build Alternative would still involve construction that may affect some of these sites, a potential for impacts related to hazardous materials is possible during construction. Soil sampling would need to be conducted prior to construction to determine if hazardous waste from these sites is present in the areas to be disturbed by construction. No permanent impacts related to hazardous materials would likely occur as part of the No Build Alternative.

Construction Impacts

Construction activities associated with the No Build Alternative include repair of spalled and damaged masonry stone on the existing piers, tuck pointing of masonry joints and pressure grouting the pier to assure internal masonry joints are solid. In addition, the existing structural steel would require rehabilitation to areas of the steel girder (or beam) where corrosion and holes in the steel are extensive and cross braced connections have failed or are near failure. In addition to maintenance activities, the three western spans located under US Route 20 would be replaced in the near future due to accelerated corrosion caused by salt spray from the highway traffic above.

These construction activities could encounter and/or generate hazardous materials such as paints, solvents, fuels, and hydraulic fluids that may be accidentally released during construction related to sites identified as having a “moderate” concern in **Table 3-4**. A “moderate” rating indicates a potential to encounter hazardous waste; however, it does not mean that hazardous waste is present. Soil sampling would be conducted prior to the start of construction to further assess the presence of hazardous waste.

Once soil sampling results have been received, safety precautions to avoid and minimize any construction-related impacts associated with the No Build Alternative, in adherence with federal, state, and local regulations, would be determined. In addition, a Construction Stormwater Pollution Control Plan, which describes methods to prevent or minimize stormwater runoff if the Project encounters contaminated soil or other hazardous materials, would be developed to minimize potential impacts.

Permanent Impacts

No permanent impacts related to hazardous waste are expected to occur under the No Build Alternative, as the potential for encountering hazardous materials is greatest during the construction phase. Any hazardous material encountered during the construction phase would be managed appropriately so as to not create a permanent impact.

Preferred Build Alternative

Six sites of Moderate Concern have been identified within the proposed project limits. Therefore, a potential for impacts related to hazardous materials is possible during construction. Soil sampling would be conducted prior to construction to determine if hazardous waste from these sites is present in the areas to be disturbed by construction. No permanent impacts related to hazardous materials would likely occur as part of the Preferred Build Alternative.

Construction Impacts

Prior to the beginning of construction, soil sampling would be conducted to further assess the presence of hazardous materials. The presence of contaminants of concern associated with the Moderate Concern sites, including petroleum products, solvents, methyl ethyl ketone, pesticides, herbicides, PCBs, metals, and chemicals associated with railroad tie preservation, would be determined by collecting soil samples in the nearest areas that would be disturbed by construction to the identified sites. In addition, sediment from the project area would be tested for PCBs and mercury prior to the start of construction. If sampling reveals the presence of hazardous materials, then appropriate measures would be taken to protect human health and the environment during construction activities. These measures could range from monitoring, to spoils management, to additional personal protective equipment for on-site personnel.

Construction activities associated with the Preferred Build Alternative include trenching for signal cables, building a new bridge, removal of the existing bridge, and extension of the new bridge piers to accommodate a second track once the old bridge has been removed. These construction activities could encounter and/or generate hazardous materials such as paints, solvents, fuels, and hydraulic fluids that may be accidentally released during construction in areas near the sites identified as “moderate” concern in **Table 3-4**. A Phase II site investigation would be conducted prior to the start of construction to further assess the presence of hazardous waste.

Once soil sampling results have been received, safety precautions to avoid and minimize any construction-related impacts associated with the Preferred Build Alternative, in adherence with federal, state, and local regulations, would be determined. In addition, a Construction Stormwater Pollution Control Plan, which describes methods to prevent or minimize stormwater runoff if the Project encounters contaminated soil or other hazardous materials, would be developed to minimize potential impacts.

Permanent Impacts

No permanent hazardous waste impacts are anticipated under the Preferred Build Alternative. Hazardous materials associated with the identified sites are most likely to be encountered during the construction phase of the Project. Any hazardous material encountered during the construction phase would be managed appropriately so as to not create a permanent impact.

3.9.4 Measures to Avoid or Minimize Harm

Federal, state, and local laws and regulations regarding hazardous materials would be followed before and during construction. While hazardous materials are not expected to be encountered, the following practices and plans would be implemented:

- Soil and sediment testing would be performed in the areas associated with the Metra railroad prior to the start of work to further investigate soil conditions and the potential presence of chemicals. If hazardous materials are identified within the project limits, then appropriate safety measures from ambient monitoring to spoils management and/or additional personal protective equipment for on-site personnel, would be taken to protect human health and the environment. The necessary safety measures would be determined once additional investigation of the area, including soils sampling, is completed.

- Metra contractors would follow all applicable laws and regulations concerning the proper certification and disposal of Clean Construction Demolition Debris (CCDD).
- Lead-based paint and hazardous material surveys of structures would be required before reconstruction or demolition of any property, including Metra-owned properties or structures, to identify any asbestos, lead-based paint particles, and hazardous materials, such as polychlorinated biphenyl or mercury-containing equipment. Any hazardous materials identified would be abated and disposed of in accordance with federal, state, and local regulations.
- As required by IEPA²² for sites exceeding 1 acre, Construction Stormwater Pollution Control Plans, which describe methods to prevent or minimize stormwater runoff if the Project encounters contaminated soil or other hazardous materials, would be developed.

Finally, during operation, Metra would adhere to all applicable federal, state, and local regulations, as well as existing system-wide hazardous material usage, storage, and disposal plans and procedures, further minimizing the potential for hazardous material impacts.

3.10 Environmental Justice

The United States Department of Transportation, in accordance with Executive Order 12898, is required to the greatest extent practicable and permitted by law, “to achieve environmental justice as part of its mission by identifying and addressing, as appropriate, disproportionately high and adverse human health or environmental effects, including interrelated social and economic effects, of its programs, policies, and activities on minority populations and low-income populations in the United States” (USDOT Order 5610.2(a), 2012). This section provides information on environmental justice (EJ) analysis and outreach conducted for this Project.

3.10.1 Regulatory Framework/Methods

Federal Regulatory Framework

Federal agencies are required to consider the potential for disproportionately high and adverse impacts on low-income and minority populations that could result from all programs, policies, and activities (Executive Order 12898). A disproportionate impact is one that would negatively affect low-income and minority populations (EJ populations) to a greater extent than non-EJ populations (Executive Order 12898; FTA Circular 4703.1). EJ populations were identified by comparing census block group data or census tract data to the demographic profile of the City of Elgin, unless there was a predetermined threshold set, such as with elderly populations.

Metra performed the EJ analysis in accordance with related federal laws and guidance including Title VI of the 1964 Civil Rights Act, Executive Order 12898, Executive Order 13166, and FTA Circulars 4703.1 and 4702.1B.

State of Illinois Regulatory Framework

²² Illinois Environmental Protection Agency. *General NPDES Permit of Storm Water Discharges From Construction Activities*. August 1, 2013; modified April 30, 2014.

The State of Illinois has its own laws governing EJ, specifically Law 097-0391 The Environmental Justice Act. This act states that “The principal of environmental justice requires that no segment of the population regardless of race, national origin, age, or income should disproportionately bear high or adverse effects of environmental pollution.” An additional analysis of elderly populations was conducted in compliance with this act.

Methodology

The EJ process and analysis for the Project was designed to accomplish the following:

- Avoid, minimize, or mitigate disproportionately high and adverse human health and environmental impacts, including social and economic impacts, on low-income and minority populations.
- Ensure the full and fair participation by all potentially affected communities in the transportation decision-making process.
- Prevent the denial of, reduction in, or substantial delay in the receipt of benefits by low-income and minority populations.

The terms “minority” and “low income” were defined in accordance with FTA Circular 4703.1. Minority populations include American Indian or Alaskan Native, Asian, Black or African American, Hispanic or Latino, and Native Hawaiian or Pacific Islander. Low income was defined as a person whose median household income is at or below the Department of Health and Human Services poverty guidelines.

Metra assessed the potential for direct and indirect or cumulative adverse impacts on EJ populations based on the following factors:

- Direct impacts would be permanent, result from implementation of the proposed project, and occur at the same time and place (40 CFR §1508.8). A direct impact distance of 500 feet was applied in determining whether EJ or non-EJ populations would experience disproportionately high and adverse environmental or health impacts. This distance was applied based on expected direct impacts from construction and implementation of this Project. This is the direct area around which construction activities would occur and where impacts due to construction would be most visible and noticeable for EJ and non-EJ populations alike.
- Indirect impacts are those caused by a project or plan, but which are separated from direct impacts by time and/or distance. Indirect impacts include induced growth and related environmental impacts, such as changes to land use patterns, population density or growth rates, and related impacts on air quality, water, and other natural systems. Cumulative impacts would be those that result from the incremental impact of the proposed project when added to other past, present, and reasonably foreseeable future actions, regardless of what agency or person undertakes such other actions (40 CFR §1508.7). The area assessed for potential indirect or cumulative impacts on EJ populations affected by the Preferred Build Alternative was an area within a half mile of the Project. This distance was applied because the potential mobility impacts or benefits of the proposed project and other planned projects are likely to be experienced by people who live, work, and/or recreate within a half mile of the project area, which is generally considered to be a walkable distance. **Section 3.11** of the EA provides additional information on indirect and

cumulative impacts.

Metra analyzed the 2014 American Community Survey 5-Year data for all census blocks within a half mile of the proposed Preferred Build Alternative location. Low-income populations were identified by comparing income levels and Department of Health and Human Services (DHHS) poverty thresholds. Low-income populations were identified where the median income of households is below the DHHS poverty guidelines. The combination of non-white races and Hispanic/Latino populations was used to determine and describe the minority population in the project area.

In addition to information about EJ populations, Metra collected information about elderly and disabled populations, which was available at the census tract level. These additional data layers were collected in accordance with the laws of the State of Illinois. No distinct elderly populations were identified. Disability statistics were compiled at the census tract level to include individuals with a sensory, physical, or mental disability or other condition that limits activities of daily living. Metra then compared these statistics to City of Elgin averages.

3.10.2 Existing Conditions

Federal Environmental Justice Populations

Figures 3-9 and 3-10 show by census block group EJ populations within the project area. The maps show several census blocks within a half mile of the project area that include low-income or minority populations.

Based on the DHHS poverty guidelines, 18.9 percent of the population within the analyzed block groups has an income below the poverty level. This amount is higher than the City of Elgin average of 14.3 percent (U.S. Census Bureau 2014)).

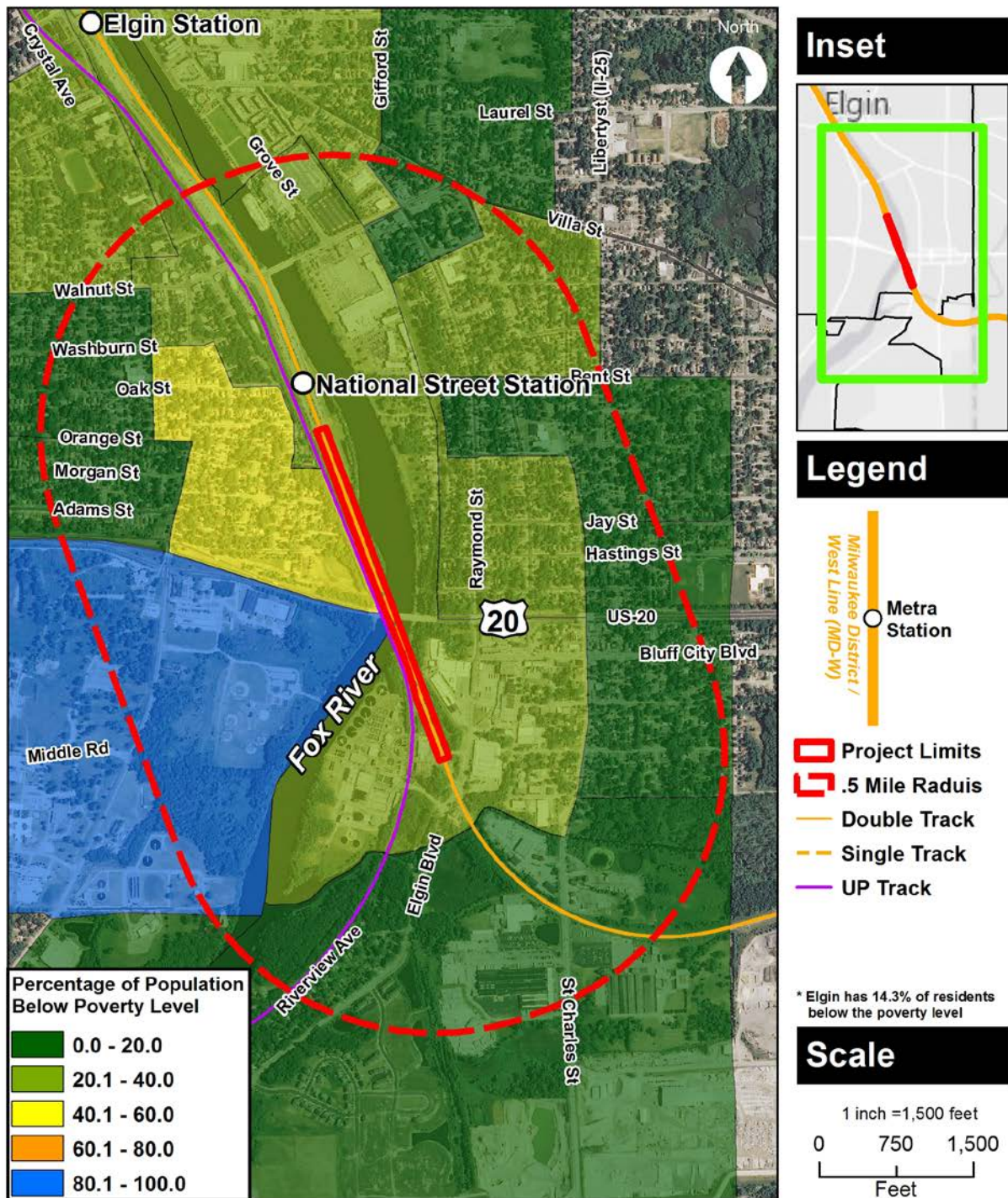
There are 17,248 people living within the block groups within a half mile of the project limits. The most prevalent race is white (67.0 percent). Hispanic or Latino populations can be of any race including white and they make up 48.3 percent of the total population. Of the total population living near the project area, minority persons, who include all non-white races and white Hispanics/Latinos, make up 61.2 percent (U.S. Census Bureau 2014), which is slightly higher than the City of Elgin average of 58.7 percent minority.

Title VI of the Civil Rights Act states that “No person in the United States is excluded from participation in, or denied the benefits of, or subjected to discrimination under any program or activity receiving Federal financial assistance on the basis of race, color, national origin, age, sex, disability, or religion.” Data was collected regarding disabled populations near the project area from the U.S. Census Bureau American Community Survey. People with disabilities near the project area constitute 8.2 percent of the project area population, which is lower than the citywide disabled population of 8.8 percent (U.S. Census Bureau 2014). However, two census tracts had higher percentages of disabled people than the City of Elgin average. Disabled people constitute 8.9 percent of census tract 8515 and 12.5 percent of census tract 8549.

State Environmental Justice Populations

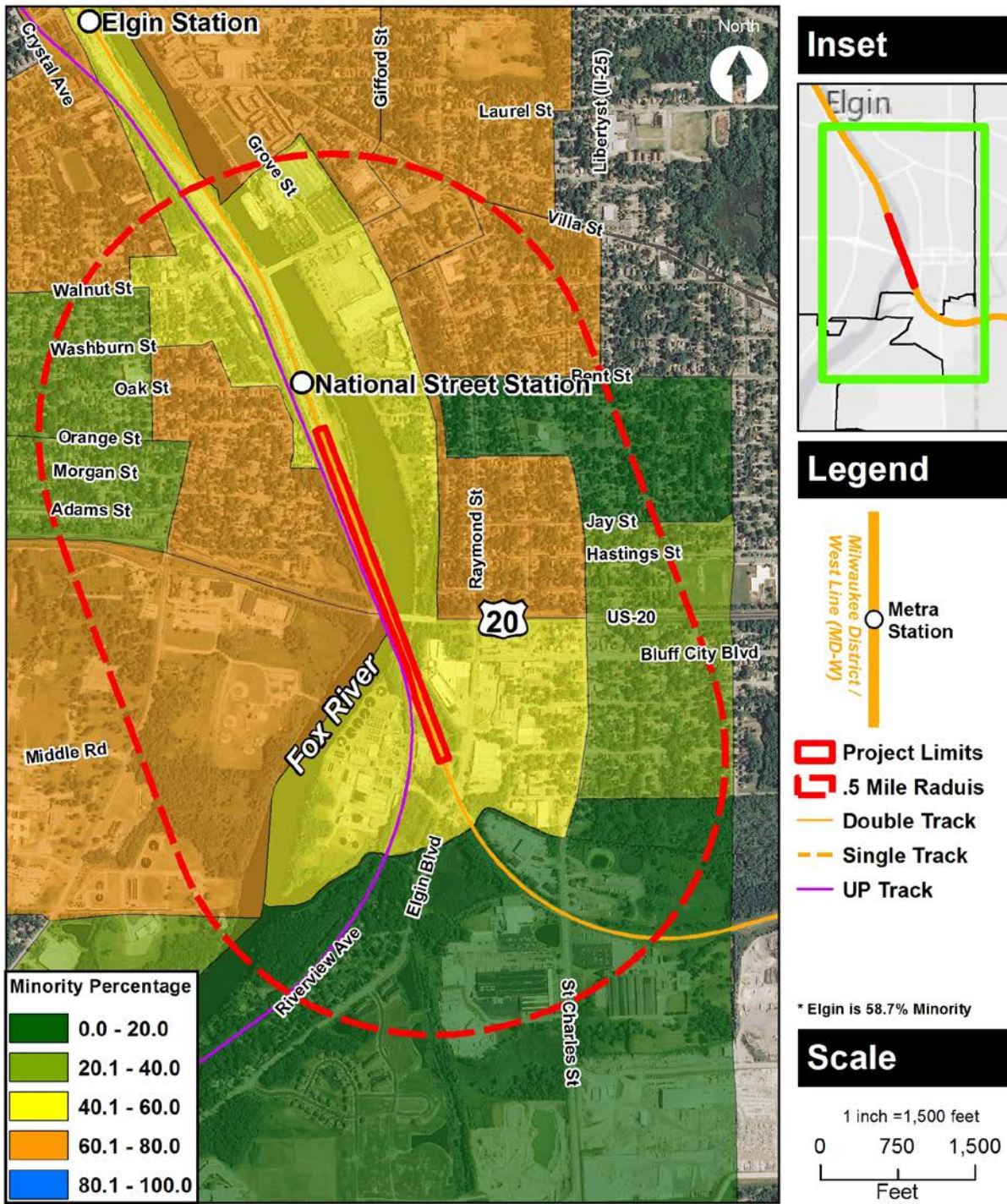
The State of Illinois includes age as a consideration in EJ analysis. Age group data was available on a Census tract basis. Approximately 6.8 percent of those living within the vicinity of the project area are elderly, which is lower than the City of Elgin elderly population of 9.0 percent. Census tract 8518.01 had an elderly

population of 9.22 percent, which is slightly higher than the City of Elgin average. As the elderly population in Census tract 8518.01 is only slightly higher than the City of Elgin average (within one percent), this population is not considered to represent a distinct EJ population.



Aerial Source: ESRI Online World Imagery
Population Source: United States Census Bureau's Topologically Integrated Geographic Encoding and Referencing Information

Figure 3-9: Low-Income Populations



Aerial Source: ESRI Online World Imagery
Population Source: United States Census Bureau's Topologically Integrated Geographic Encoding and Referencing Information

Figure 3-10: Minority Populations

3.10.3 Environmental Impacts

This section describes the potential for disproportionate impacts and unevenness of benefits in the project area's EJ communities. No permanent impacts are expected to result from the Project, and only temporary impacts related to construction activities are expected.

No Build Alternative

The No Build Alternative would have similar impacts to the Preferred Build Alternative involving the same EJ populations identified in **Section 3.10.2**. The No Build Alternative would still require construction activities to rehabilitate the existing bridge.

Construction Impacts

Since the No Build Alternative would still require construction activities to rehabilitate the existing bridge, noise and aesthetic impacts associated with construction would still be present. In addition, the existing bridge would remain single-tracked, which would maintain the presence of trains idling while waiting to cross. These trains would continue to impact the air quality and would remain a source of noise to the residents.

Construction would produce temporary noise and vibration impacts, but these would be mitigated. Some minor air quality impacts as a result of fugitive dust and/or construction vehicle emissions may also be experienced. Construction BMPs and careful construction scheduling would minimize these adverse impacts. Construction impacts would be similar throughout the project area and would not result in disproportionately high and adverse effects on minority or low-income populations. These impacts would be present under the No Build Alternative as construction activities are necessary to maintain the existing bridge.

No disproportionately high and adverse impacts due to construction are anticipated because impacts would be minor and temporary and would be mitigated. Construction would primarily occur within existing Metra right-of-way, which would limit neighborhood and community impacts.

Permanent Impacts

No permanent impacts are expected to occur under the No Build Alternative in the project area. As a result, no disproportionately high and adverse impacts, based upon the definition in FTA Circular 4703.1, would occur under the No Build Alternative.

Preferred Build Alternative

The Preferred Build Alternative would have no permanent impacts. No displacements would occur as a result of the Preferred Build Alternative. Based upon the analysis presented in **Sections 3.7, 3.8, and 3.12.2** there would be no permanent impacts associated with noise or vibration, and the Preferred Build Alternative would slightly improve air quality by reducing train idling times. Potential impacts were assessed by analyzing an area of direct impacts (within 500 feet) and an area of cumulative impacts (within a half mile).

Within the area of indirect or cumulative impacts (within a half mile of the project limits and outside the area of direct impacts), there are nine block groups that had higher than City of Elgin average of minority groups

and one of these block groups also had a higher than City of Elgin average incidence of poverty. No disproportionately high or adverse impacts are expected to occur.

Construction Impacts

The Preferred Build Alternative would result in temporary adverse construction impacts described below on neighborhoods surrounding the Project. No disproportionately high and adverse impacts due to construction are anticipated, because impacts would be temporary and would be mitigated. Construction would primarily occur within existing Metra right-of-way, which would limit neighborhood and community impacts.

Five block groups had areas of direct impacts (within 500 feet of the project limits). Of these five block groups, three had minority populations above the City of Elgin average and four block groups had higher percentage of people living in poverty than the City of Elgin average. Two of these block groups (Tract 8549 Block Group 1 and Tract 8518.01 Block group 2) do not have residences within 500 feet of the project limits. As a result, direct impacts would only be considered for the three block groups that have residences within 500 feet of the project limits. Direct impacts would primarily include construction noise. These impacts would be temporary as the Project would not lead to an overall increase in noise or vibration.

Residences within 500 feet for Census Tract 8515 Block Group 1 and Census Tract 8514 Block Group 6 are located east of the Fox River. These residences would likely be screened from construction impacts by US Route 20 and trees along the banks of the Fox River. As a result, no impacts are anticipated.

Residences along Hendee Street, Riley Street, Lord Street, Souster Avenue, Willis Street, and Robey Street are close to the northern project limits and the most likely to be affected by construction activities. The residences along and south of Lord Street are five to ten feet lower than the grade of the railroad, while residences north of Lord Street are approximately at grade. These residences are part of Tract 8516 Block Group 3. The block group had a large population (41.2 percent) below the poverty level and a minority population above the City of Elgin average. Direct impacts would primarily include construction noise. These impacts would be temporary as the Project would not lead to an overall increase in noise or vibration.

Construction would produce temporary noise and vibration impacts, but these would be mitigated. Some minor air quality impacts as a result of fugitive dust and/or construction vehicle emissions may also be experienced. Temporary noise impacts are likely to occur due to construction activities. The construction activities would be limited to daytime hours, where feasible, though night and/or weekend work may be needed during track cutover, piling, excavation, deep foundation work, or other activities. If any planned work conflicts with the City of Elgin's noise ordinance, Metra will coordinate with the City to resolve the issue. Truck traffic would be primarily present along major roads near the project area and would use a defined access path to reach the project limits, likely along the existing right-of-way. Construction impacts would be similar throughout the project area and would not result in disproportionately high and adverse effects on minority or low-income populations. These impacts would also be present under the No Build Alternative as construction activities are necessary to maintain the existing bridge structure.

Permanent Impacts

The Preferred Build Alternative would not create any permanent adverse impacts, as there would be no displacements, land use changes, or other impacts to the residential areas, such as an increase in noise. Therefore, the Preferred Build Alternative would not result in disproportionately high and adverse effects on

low-income or minority populations. The Project would actually benefit EJ populations and the regional population as a whole by reducing train idling and improving travel times along Metra's Milwaukee West Line.

3.10.4 Community Outreach

Metra conducted community outreach by coordinating with local elected officials. City officials from Elgin were fully supportive of the Project. In addition, the Project has received coverage in local area newspapers including the *Chicago Tribune* on October 26, 2015 and March 24, 2016 and in the *Daily Herald* on October 27, 2015. Metra issued a press release announcing the proposed improvements to the Milwaukee West Line Fox River Bridge (Metra Bridge Z-100) on March 23, 2016. Further community outreach will occur when the EA is issued for public review and comment.

3.11 Indirect and Cumulative

While the other sections of this EA provide analysis and findings on direct impacts of the Project, NEPA also requires the consideration of the potential indirect and cumulative impacts of federally funded projects, as discussed in this section.

3.11.1 Regulatory Framework/Methods

Indirect impacts, also known as secondary impacts, are defined under 40 CFR §1508.8. The impacts are caused by the Project or plan, but are separated from direct impacts by time and/or distance (yet still in the foreseeable future). Indirect impacts include induced growth and related environmental impacts, such as changes to land use patterns, population density or growth rates, and related impacts on air quality, water and other natural systems. Cumulative impacts are defined under 40 CFR §1508.7 as the combined result of the incremental direct and indirect impacts of a Project or plan, the impacts of past and present actions, and impacts of reasonably foreseeable future actions by others on resources of concern.

The boundary to determine the potential indirect impacts was based on all proposed elements of the Project, including construction limits and proposed property acquisitions (described in **Section 3.1**). For the analysis, findings from the environmental resource analyses were reviewed to properly evaluate the potential for indirect impacts on land use, transportation, and economic development plans and goals, as well as to identify notable or sensitive resources within the surrounding communities such as community facilities, historic resources, and other vulnerable or unique resources. A qualitative assessment of the potential for and impacts of induced growth that could result from this Project was then determined. The factors assessed relate to changes in growth and development expected as a result of the Project. Based on these factors, a determination was made on the potential and magnitude of impacts that could result from the Project and whether those impacts would be consistent with surrounding growth, trends, and goals within the project area.

To identify the potential for cumulative impacts areas, an area within a half mile of the project limits was evaluated. To perform the evaluation, Metra reviewed applicable current and future regional and local plans to look for projects or recommendations from the plans within the half-mile footprint.

The horizon year for assessing indirect and cumulative impacts is 2040, which represents the regional

transportation planning horizon. Construction of the Project is anticipated to begin in 2017 and the Project is currently anticipated to be operational in 2020.

Reasonably foreseeable projects include projects identified in *GO TO 2040*, the Transportation Improvement Program (TIP), and known private development and redevelopment projects in the project area.

3.11.2 Environmental Impacts

This section identifies and assesses the potential indirect and cumulative impacts of the Project.

No Build Alternative

The No Build Alternative would have similar impacts to the Preferred Build Alternative relative to indirect and cumulative impacts as discussed below. The No Build Alternative would still require construction activities to rehabilitate the existing bridge. In contrast to the Preferred Build Alternative, long term indirect impacts would include the continued expenditure of funds for future repairs to the existing bridge. In addition, trains would still continue to idle at the bridge waiting for clearance to cross. Noise and air quality would remain the same with no improvement due to the lack of a second track. The No Build alternative may slightly increase energy use over time through the need to idle trains at the single-track crossing.

GO TO 2040 includes two Milwaukee West Line specific projects in the fiscally unconstrained list: 1) track, signal, and other improvements to upgrade the line's core capacity and support ridership growth; and 2) an extension of the line from its current terminus in Elgin to Marengo in McHenry County²³. Implementation of the No Build Alternative, as opposed to the Preferred Build Alternative, could limit Metra's ability to fully implement either of these projects. Replacement and expansion of the existing bridge is one of the several pre-conditions needed for future core capacity improvements for the line extension outlined in the GO TO 2040 plan.

The Project is also consistent with proposed improvements to the Milwaukee West Line included in Metra's Strategic Planning process^{24 25}.

Construction Impacts

It is anticipated that no indirect impacts would occur for the construction activities necessary to rehabilitate the bridge in the No Build condition.

Permanent Impacts

The No Build Alternative would lack the benefits of the proposed project, including enhanced movement of passengers (mobility). Travel times would not improve, thereby limiting the mobility of passengers, especially those that rely upon public transportation.

²³ Descriptions of the Milwaukee West Line Specific projects included in the GO TO 2040 plan are available at the following web site: <http://www.cmap.illinois.gov/documents/10180/332742/Update+Major+Capital+Projects+FINAL.pdf/51a1943f-0c2d-4243-8d94-9232f4598566>.

²⁴ Information on Metra's Strategic Planning process is available at the following web site: metrarail.com/strategicplan.

²⁵ A map of the proposed Milwaukee West Line extension project is available on page 10 of the document available at the following web site: https://metrarail.com/sites/default/files/assets/about-metra/metra_open_house_1_boards_letter.pdf.

Preferred Build Alternative

Indirect Impacts

The area around the Project is urbanized and developed, with mature neighborhoods. The City of Elgin Comprehensive Plan shows no existing vacant/developable land in the project area. In addition, no new stations would be built as part of the Project. Due to these factors, no secondary development or induced growth is likely to be stimulated by the replacement of a single-track bridge with a double-track bridge.

The proposed replacement of the existing bridge has the potential to reduce delays, and decrease travel times. The decrease in travel times has the potential to draw more passengers and reduce roadway congestion. A reduction of vehicles would improve air quality with a decrease in vehicle emissions.

Due to the factors listed above, the Preferred Build Alternative is expected to have no indirect impacts with the exception of incremental beneficial impacts on air quality.

Cumulative Impacts

Past, present, and reasonably foreseeable future actions within the project area were considered in this analysis. The land uses and development in the project area and surrounding neighborhood have remained unchanged over the last 20 years according to aerial photos of the area. No major changes to land use in the area is expected by other future projects. In addition to the City of Elgin Comprehensive Plan showing no existing vacant/developable land in the project area, the growth management part of the Plan also does not show the project area as part of the immediate growth area or the pressured growth area for Elgin. The existing land use map in the City's plan shows the project area as a mix of railroad/utility, industrial, government buildings, parks and recreation, and residential. The future land use plan map also shows a mix of industrial, office/research, parks and open space, and residential.

IDOT is planning improvements to US Route 20 near the project area. Bridge replacement is planned for FY 2018 (TIP ID 09-11-0002) at the existing US Route 20 and IL Route 31 interchange to the west of the Project. Since this is an existing interchange in a developed area no changes to land use, no induced development, or cumulative impacts are expected from this Project.

Due to the land use in the project area remaining the same over the last 20 years and the only foreseeable future action is an improvement at an existing interchange in an already developed area no other cumulative impacts are expected to land use, transportation, or other resources in the project area.

3.12 Resources with Limited or No Impacts

A number of other environmental resources typically examined under NEPA were determined to present limited or no impacts from the proposed project. These resources include transportation, air quality, land use and economic development, navigable waterways and coastal zones, geology and soils, energy, safety and security, and visual and aesthetic conditions. The following sections briefly summarize the findings of the analyses.

3.12.1 Transportation

The No Build Alternative would improve the bridge, but would not alleviate the bottleneck that the single track bridge creates. Delays would continue permanently into the future. The grade crossing at the Alphabet Shop would also not be improved under the No Build Alternative.

Limited or no transportation impacts are anticipated from the Preferred Build Alternative. Commuter railroad service would be able to operate on the existing bridge while the new bridge is under construction. There may be train service delays for two weekends during construction. The existing bridge is a single-track structure preventing trains from crossing it in opposite directions at the same time. The bridge is the only single-track section on the Milwaukee West Line. Once the Project is completed, the current bottleneck would be eliminated, meaning trains in the opposite direction would no longer have to wait for the other train to pass; this is the main transportation benefit of the Project. Currently, 54 Metra commuter trains and eight Canadian Pacific Railroad freight trains use the bridge each day and would no longer have delays while moving across the river on the single-track bridge.

One existing at-grade crossing at Elgin Avenue would be replaced by the Project. This crossing provides access to a business called The Alphabet Shop. A temporary track crossing would be provided to ensure access would be maintained to The Alphabet Shop during construction. No station or parking lot modifications are proposed as part of the Project. There would be no impacts to pedestrians and bicyclists because no sidewalks or paths would be crossed by the Preferred Build Alternative.

3.12.2 Air Quality

The No Build Alternative would not provide the limited air quality benefits that are provided by implementing the Preferred Build Alternative as the single track condition would remain, causing train delays because of the bottleneck.

The Preferred Build Alternative could result in some adverse impacts on air quality during construction from construction equipment exhaust. Impacts during construction would be primarily associated with fugitive dust and emissions from on-road and non-road vehicles. The Project would be required to follow air quality guidelines in accordance with state and federal law. The Illinois Environmental Protection Agency (IEPA) has strict guidelines for controlling fugitive dust and diesel particulate emissions (Title 35 Illinois Administrative Code Subtitle B). As a result, these impacts, which are not substantial, would be minimized through implementation of appropriate construction BMPs. The Preferred Build Alternative would result in an overall incremental beneficial impact on air quality by improving train speeds, reducing train idling, and improving reliability of the transit system, which could attract new passengers who currently make trips in automobiles.

Coordination was initiated by Metra with the Chicago Metropolitan Agency for Planning (CMAP) in 2011 regarding the Project's inclusion the Transportation Improvement Program (TIP) and conformance to the State Implementation Plan (SIP) as part of the air quality analysis. States must develop a SIP if they have an area that is designated as "nonattainment" (designated areas) for air quality. The SIP is an air quality plan that explains how the nonattainment area(s) will meet the requirements of the Clean Air Act. CMAP reviews transportation projects to ensure they conform to the region's air quality plan. In a letter dated March 28, 2011 (see **Appendix C**), CMAP stated the Project is included in the TIP and the Project conforms to the SIP and the transportation-related elements of the 1990 Clean Air Act Amendments.

3.12.3 Land Use and Economic Development

The No Build Alternative is not expected to result in changes to land use or zoning in the project area. Construction related to the No Build Alternative would not displace any businesses, buildings, or residents. Construction for the No Build Alternative may require a temporary easement from the Union Pacific Railroad, which is adjacent to the project area.

The Preferred Build Alternative is not expected to result in major permanent impacts on economic development in the project area. No adverse changes in taxation policy or levels would occur as a result of the Project. The Preferred Build Alternative would not result in a permanent disruption of business activities, nor would it permanently affect regional construction costs. The Preferred Build Alternative is consistent with local and regional plans by promoting the use of transit and enhancing the efficiency of existing transit facilities.

The project area consists of a railroad corridor containing the existing Metra Milwaukee District Line and the adjacent Union Pacific Railroad. The project area, as well as immediately surrounding areas, are mostly Zoned CF – Community Facility. According to the City of Elgin Municipal Code Chapter 19.30, the Community Facility designation covers a wide range of uses, from railroad corridors to public utilities to churches and hospitals. Adjacent uses designated as Community Facilities include the water treatment plant to the south, Elgin Shores Forest Preserve to the southwest, government buildings to the west, and the Fox River Trail to the east. A small portion of southern end of the project area is Zoned CI – Commercial Industrial. The Commercial Industrial zoning designation covers a variety of municipal, retail and industrial uses, including railroad tracks.

Other adjacent uses include residential areas to the northwest and northeast, both designated RC2 - Residence Conservation 2, and a commercial/industrial area to the southeast in a CI – Commercial Industrial district. **Figure 3-11** shows the current zoning designations for parcels within a quarter mile of the project limits.

The Preferred Build Alternative is not expected to result in changes to land use or zoning in the project area. Construction of the Preferred Build Alternative would not displace any businesses, buildings, or residents. Construction would require a temporary easement of approximately 0.97 acres from the Union Pacific Railroad, which is adjacent to the project limits. Approximately 0.33 acres of land acquisition or a permanent easement would be acquired from the Union Pacific Railroad near the temporary easements. The easements would be limited to the unused land located between the Union Pacific Railroad and Metra Railroad tracks. The City of Elgin was consulted earlier in the Project and indicated that the Project does not require zoning changes, as the proposed Project alignment is consistent with the current railroad alignment and zoning regulations.

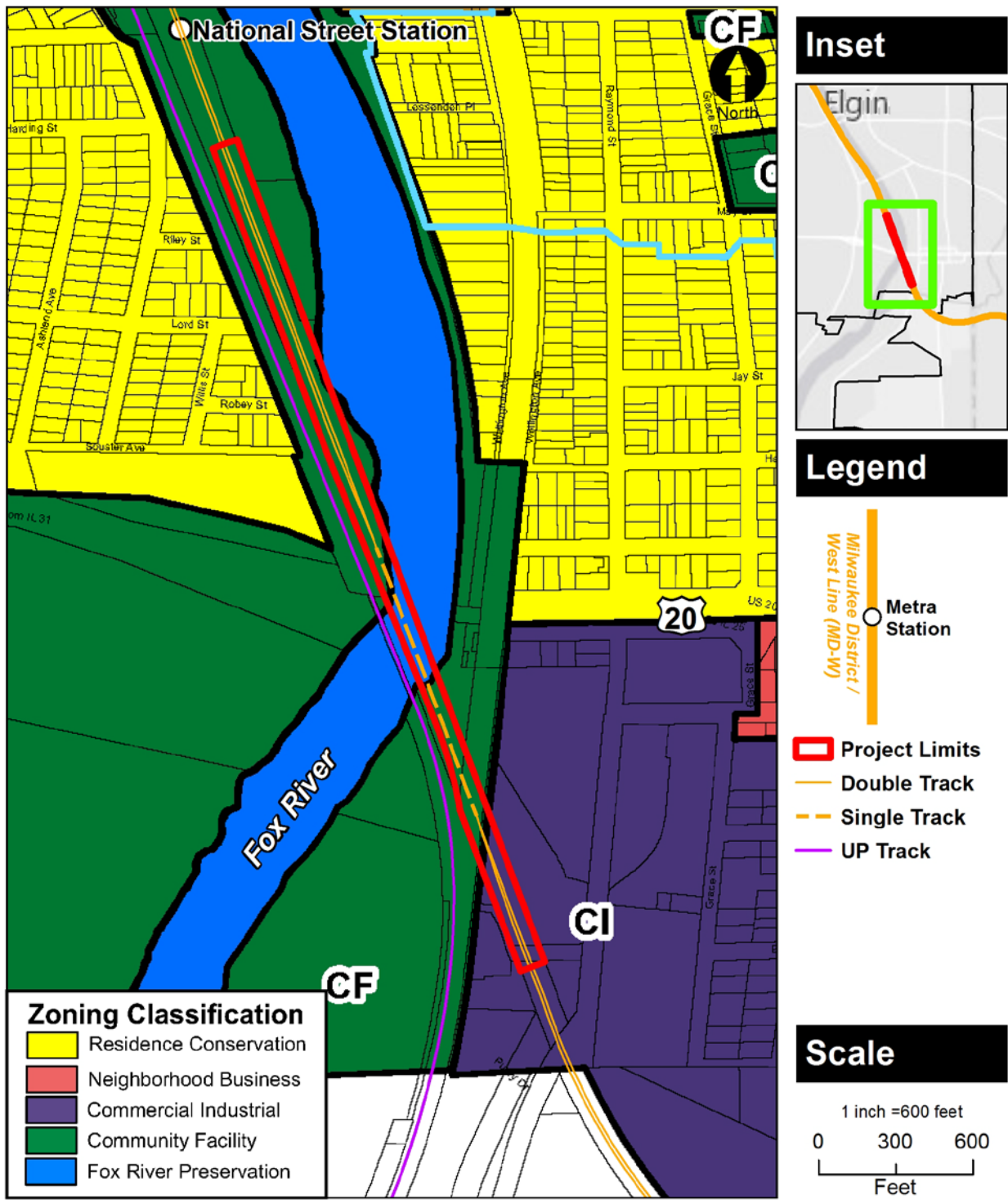
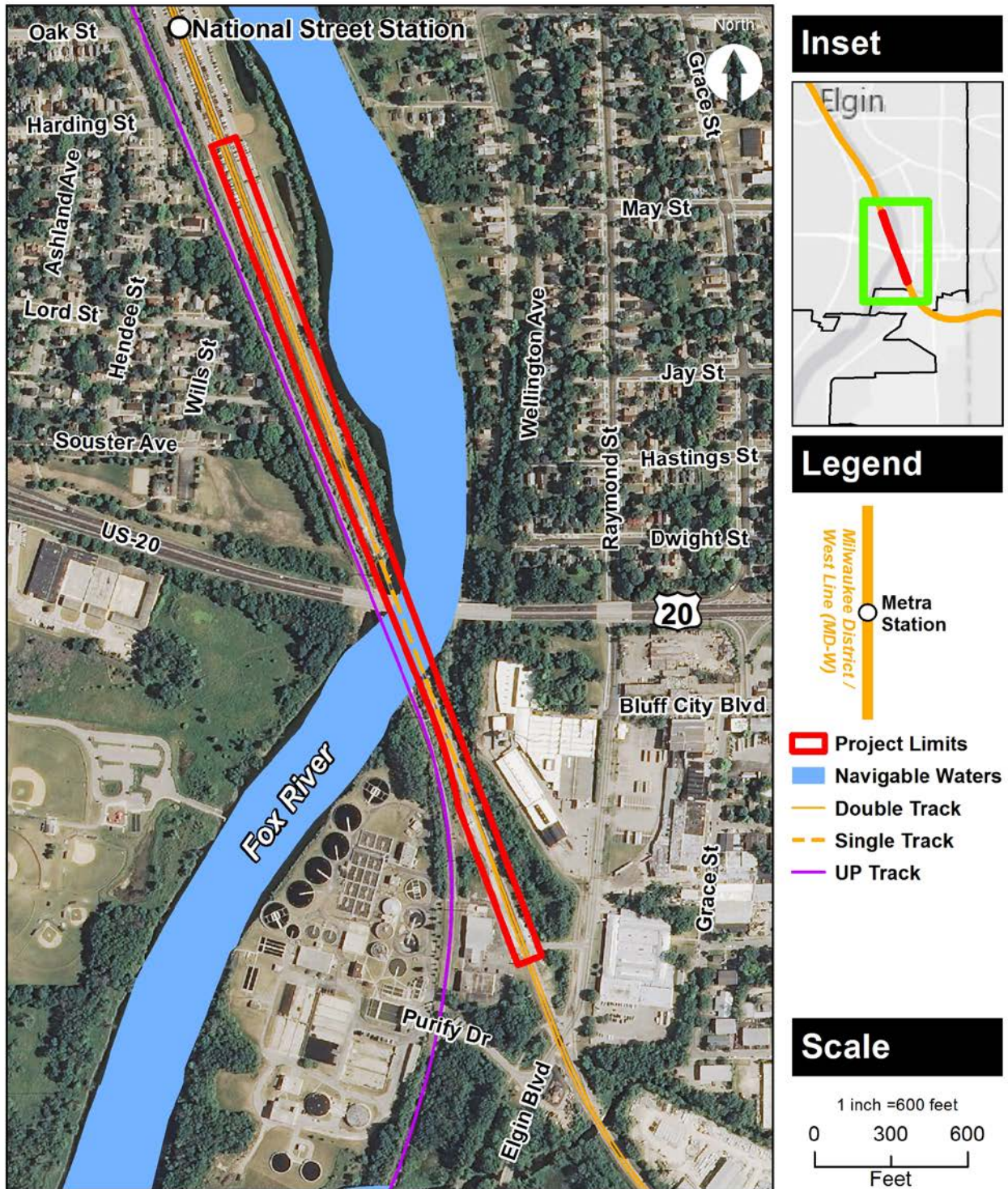


Figure 3-11: Current Zoning Designations

3.12.4 Navigable Waterways and Coastal Zones

The implementation of the No Build Alternative would not affect waterway navigation or a coastal zone management area from the existing condition. However, the five existing piers would remain in the river, compared to only three proposed piers proposed under the Preferred Build Alternative. Therefore, the No Build Alternative creates slightly more impacts than the Preferred Build Alternative.

The Preferred Build Alternative would not affect waterway navigation and it is not located adjacent to, nor does it affect, a coastal zone. The USACE designates the Fox River as navigable throughout. Navigable waterways are regulated by the USACE under Section 10 of the Rivers and Harbors Act of 1899 (33 U.S.C. §403). **Figure 3-12** shows navigable waters in the project area. The Preferred Build Alternative bridge over the Fox River would consist of three piers whereas the existing bridge consists of five piers. The reduction in piers would remove obstacles to navigation. Metra would request a Section 10 permit from the USACE as part of the Section 404 permit request (See **Section 3.4**). A Section 10 permit would be required due to temporary construction activities within a navigable waterway, though, as described above, there would be a permanent benefit due to the reduction in the number of piers.



Aerial Source: ESRI Online World Imagery

Figure 3-12: Navigable Waters

3.12.5 Geology and Soils

The No Build Alternative would not result in adverse impacts on geologic and soil resources. Soils in the project area are primarily loams and silt loams. Local topography is generally flat, with downward slopes towards the Fox River. Disposal of any material removed from site would follow the appropriate state and federal regulations. Soils that would be potentially impacted have been previously impacted by urban development along the railroad corridor and are not suitable for farming or agricultural activities as a result. Post-construction, soils would be stabilized to limit erosion into the Fox River.

The Preferred Build Alternative would not result in adverse impacts on geologic or soil resources. Soils in the project area are primarily loams and silt loams. Local topography is generally flat, with downward slopes towards the Fox River. Disposal of any material removed from site would follow the appropriate state and federal regulations. Soils that would be potentially impacted have been previously impacted by urban development along the railroad corridor and are not suitable for farming or agricultural activities as a result. Post-construction, soils would be stabilized to limit erosion into the Fox River.

3.12.6 Energy

The No Build Alternative would have more of an impact on energy use than the Preferred Build Alternative. The greater impact would be a result of the continuing need to idle trains waiting to cross the single track bridge. The energy consumption used during construction of the Preferred Build Alternative would eventually be offset by the improved efficiency of a double track bridge and the elimination of the existing bottleneck.

The Preferred Build Alternative would not have an adverse impact on energy consumption in Kane County. By improving energy-efficient commuter rail service, Metra service helps reduce overall community energy use in transportation. Energy consumption during construction would be offset by the long-term savings realized by more efficient track infrastructure, as the new bridge structure would be built according to current railroad design standards and would help maintain commuter rail as a viable transportation option. The new bridge would eliminate the single-track bottleneck, which would reduce delays resulting in better on-time commuter rail service, and use of a more energy-efficient mode of transportation than single occupancy vehicles. Lastly, double tracking would also allow faster trains to pass slower trains, improving service times.

3.12.7 Safety and Security

The No Build Alternative's required construction activity on the existing bridge would eventually improve safety and security from the existing condition. While the No Build Alternative would not replace all of the existing piers, abutments, and spans which were constructed in 1881, 1905, and 1926, respectively, continued future required improvements would maintain a safe and secure condition on the bridge and approaches.

No construction-related safety and security impacts are anticipated under the Preferred Build Alternative. The Preferred Build Alternative would also not result in any permanent negative impacts on safety and security, and is anticipated to result in safety and security benefits. The Preferred Build Alternative would replace the existing piers and abutments, constructed in 1881, and the spans which were constructed in 1905 and 1926, and which are in poor condition. The Preferred Build Alternative would modernize the system thereby reducing the already low risk of major incidents and providing safety benefits for Metra passengers and employees. The Project is being designed and would be operated consistent with federal, state, and local

safety and security policies and guidance.

3.12.8 Visual and Aesthetic Conditions

The visual character of the project area is a mix of land uses. The existing surrounding visual landscape is an urban area with the Fox River, three bridges over the Fox River, a park, a sewage treatment plant, utility lines, and light industrial uses. Within the project area, the existing Metra bridge generally extends north-south approximately 50 feet east and parallel to the UPRR railroad bridge. The US Route 20 bridge extends over the top of the Metra and UPRR railroad bridges. There are no historic districts or sites near the project area.

Under the No Build Alternative, the visual character of the project area would be similar to the existing conditions. Temporary impacts would result from routine maintenance and rehabilitation that would be required. These temporary impacts would include visual impacts from construction fencing and equipment during repairs. The No Build Alternative would also require the three western spans located under US Route 20 to be replaced in the near future due to accelerated corrosion caused by salt spray from the highway traffic above. Other work required under the No Build Alternative would include repair of spalled/damaged masonry stones, tuck pointing of masonry joints and pressure grouting the pier to assure internal masonry joints are solid, rehabilitation of sections of girder (or beams) where section loss is extensive and cross braced connections have failed or are near to failure, and installation of steel plates bolted to the top and bottom of the top flange where a crack has developed.

Under the Preferred Build Alternative, the visual character of the project area would be similar to the existing conditions. The new bridge would be built parallel to the existing bridge on the downstream side. The bridge would be between the existing Metra bridge and the existing UPRR bridge. Once the new bridge was constructed, the existing bridge would be removed. The view in the future would be similar to today with two parallel railroad bridges under the US Route 20 bridge. There would be no property displacements with the Preferred Build Alternative.

Construction of the Preferred Build Alternative and rehabilitation required by the No Build Alternative would result in impacts on the surrounding visual environment. Construction would primarily occur within the existing Metra right-of-way or on easements from the UPRR, which would minimize both visual impacts and neighborhood and community impacts during construction.

3.13 Section 4(f) Resources

Section 4(f) of the USDOT Act of 1966 is a federal law that established requirements for USDOT (including FTA) consideration of publicly owned parks/recreational areas that are accessible to the general public, publicly owned wildlife/waterfowl refuges, and publicly or privately owned historic sites of federal, state, or local significance in developing transportation projects. Section 4(f) prohibits use of these resources for transportation projects unless (1) it is proven that there is no feasible and prudent alternative to the use and the action includes all possible planning to minimize harm, or (2) the agency determines that the use of the property, including any measure(s) to minimize harm, will have a *de minimis* impact on the property.

This law, commonly known as Section 4(f), is now codified in 23 U.S.C. §303 and 23 U.S.C. §138, and is implemented by FTA through regulations at 23 CFR §774. Additional guidance on the implementation of Section 4(f) may be found in FHWA's Section 4(f) Policy Paper (USDOT, FHWA 2012). FTA has formally

adopted this guidance and this analysis was conducted consistent with the guidance.

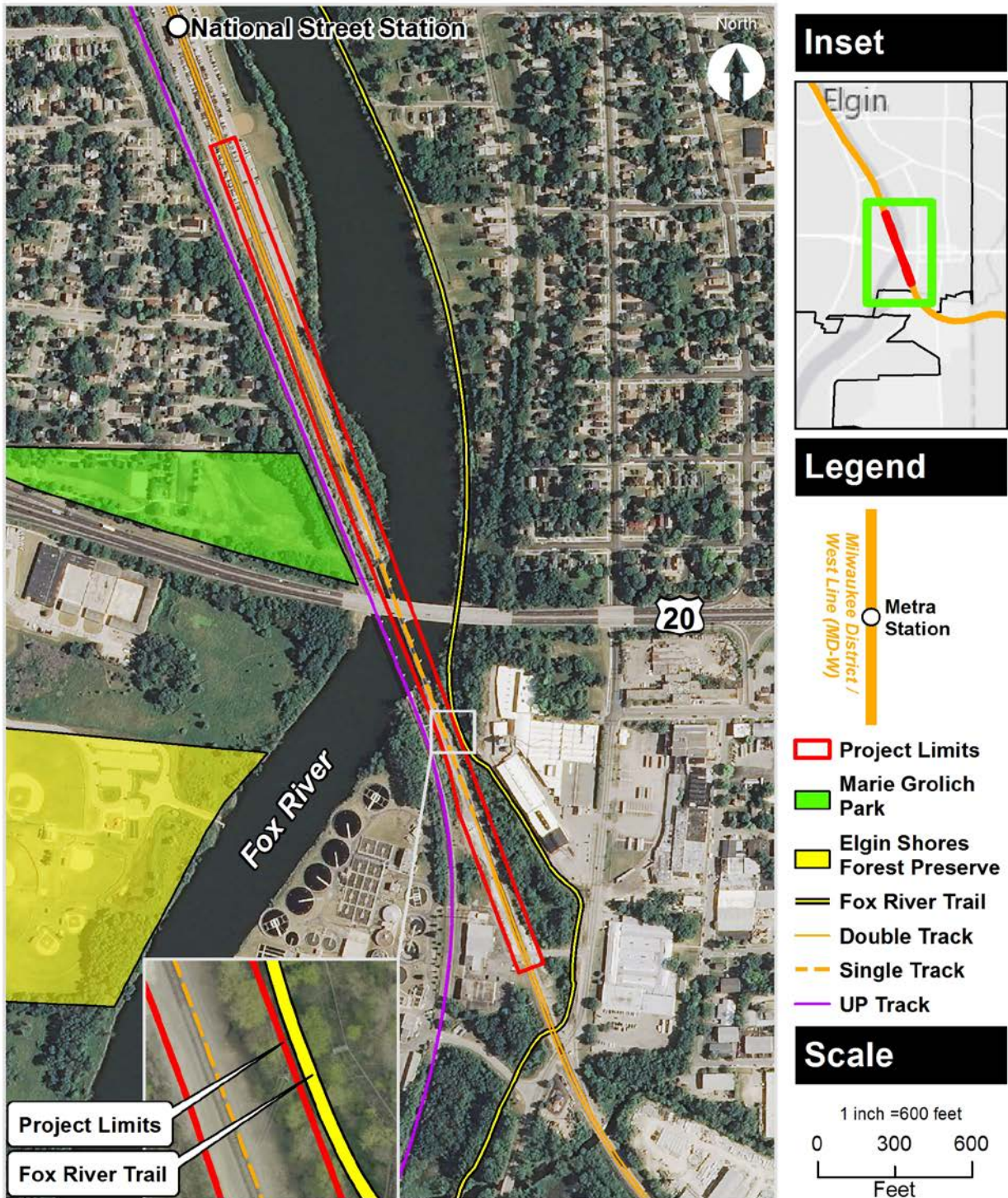
Based on the evaluation in this EA, no public parklands, recreational areas, historic sites, or wildlife and waterfowl refuges that are afforded protection by Section 4(f) are within the proposed project limits. The following discussion summarizes the closest recreational lands to the proposed project. **Figure 3-13** shows the location of nearby parks and recreational facilities.

Marie Grolich Park is located west of the UPRR tracks on the west side of the Fox River. Marie Grolich Park is owned and maintained by the City of Elgin, Parks and Recreation Department. There are limited recreational facilities at the park including a playground, practice fields, and a quarter-mile path. There is no work proposed west of the UPRR tracks and all work required for the new bridge would not affect the use of Marie Grolich Park. Indirect effects resulting from the Project would not affect the characteristics of this resource; consequently, there would be no Section 4(f) use of this resource.

The Fox River Trail is to the east of the Metra tracks, outside the Metra right-of-way, on the east side of the Fox River and is owned and maintained in this area by the Kane County Forest Preserve District. The trail is over 40 miles long in Kane County and extends along the Fox River from the City of Aurora to the Village of Algonquin. In the Elgin area near the Preferred Build Alternative, the trail is on the east side of the Fox River. There is no work proposed that would cross the trail and all work required for the bridge would not affect the use of the Fox River Trail. Indirect effects resulting from the Project would not affect the characteristics of this resource; consequently, there would be no Section 4(f) use of this resource.

Although these resources are near the project area, they are outside the permanent right-of-way and construction sites. The No Build Alternative and Preferred Build Alternative would not directly affect or incorporate land from these resources. The No Build Alternative and Preferred Build Alternative would not restrict access to these resources. The No Build Alternative and Preferred Build Alternative would not substantially impair or diminish the features or attributes of these resources. The No Build Alternative and Preferred Build Alternative would not result in a Section 4(f) use of these resources.

There are no wildlife or waterfowl refuges located on or adjacent to the proposed project site. A Section 4(f) evaluation of avoidance alternatives, least overall harm analysis, and all possible planning to minimize harm was not conducted because FTA finds that the No Build Alternative and Preferred Build Alternative would have no use of Section 4(f) resources.



Aerial Source: ESRI Online World Imagery

Figure 3-13: Parks

Chapter 4 Public and Agency Coordination

In 2010, Metra initiated planning for the replacement of the Milwaukee West Line Fox River Bridge (Metra Bridge Z-100) over the Fox River in the City of Elgin, Kane County, Illinois. Coordination was initiated with the Mayor’s office of Elgin to apprise Elgin of the potential Project and to seek support of the Project. In September 2010, the City of Elgin indicated their support of the Project. In 2011, with new elected officials in place at Elgin, Metra requested the support of the new administration with the City of Elgin. The City reiterated its support for the Project in August 2011.

No special meetings were conducted between Metra, elected officials and community groups due to the relatively small nature of the Project and the fact that impacts to local residents and businesses are not anticipated to occur with the bridge improvement project.

Metra issued a press release announcing the proposed improvements to the Milwaukee West Line Fox River Bridge (Metra Bridge Z-100) on March 23, 2016.

Metra will hold a public hearing as part of the EA process. The hearing will be conducted in an open house format, and will provide attendees with an opportunity to review the proposed project and provide input on project designs, costs, and environmental considerations. Project team members will be on-hand to explain the information presented on exhibit boards and to answer project-related questions. Attendees will be able to comment in writing during the hearing or submit their comments after the hearing by e-mail or U.S. mail. In addition, a court reporter will be present at the hearing to document oral comments.

FTA and Metra provided notice of the proposed bridge project to the federal, state, and local agencies involved in the project to date. FTA provided federal agencies and Native American tribes with project information letters in 2012. Metra provided state and local agencies with letters and informational materials at critical phases of the Project since its inception. Responses to the letters allowed FTA and Metra to confirm agency coordination and interest in the Project. **Table 4-1** provides a list of agencies contacted. **Appendix C** contains copies of correspondence.

To ensure proper development of required mitigation and commitments for this Project, Metra conducted agency coordination throughout the development of the EA. The correspondence provided an opportunity for early and ongoing agency coordination efforts. Correspondence relative to early coordination is located in **Appendix C**.

Table 4-1: Coordination with Agencies

Federal Agencies	State Agencies	Local Agencies
U.S. Fish & Wildlife Service	Illinois Department of Natural Resources	City of Elgin
U.S. Army Corps of Engineers	Illinois Historic Preservation Agency	Chicago Metropolitan Agency for Planning

4.1 Section 106 Coordination

The Section 106 process to identify and assess potential impacts to cultural and historic resources was carried out in coordination with the State Historic Preservation Officer of the Illinois Historic Preservation Agency, as detailed in **Section 3.3**. This included coordination between Metra and SHPO in 2010 and 2011. Additionally, the formal Section 106 process, including the formal consultation between FTA and SHPO, occurred in October 2015. This was concluded on October 30, 2015 when SHPO concurred with FTA's finding of no historic properties affected.

The Section 106 Coordination documentation is provided in **Appendix C**.

4.2 Tribal Coordination

FTA invited the following tribal organizations to participate in the Section 106 consultation process through correspondence dated August 17, 2012. Correspondence with the Tribal Nations is provided in **Appendix C**.

1. Correspondence, Mr. Harold Frank, Chairman, Forest County Potawatomi Community, August 17, 2012
2. Correspondence, Ms. Kelli Mosteller, Citizen Potawatomi Nation, August 17, 2012
3. Correspondence, Mr. Steve Ortiz, Chairperson, Prairie Band of Potawatomi Nation, August 17, 2012
4. Correspondence, Mr. Kenneth Meshigaud, Chairman, Hannahville Indian Community, August 17, 2012
5. Correspondence, from Ms. Melissa Cook, Tribal Historic Preservation Officer, Forest County Potawatomi Community, September 27, 2012
6. Correspondence from Metra to Forest County Potawatomi Community, November 9, 2012

The Forest County Potawatomi Community responded on September 27, 2012 requesting additional information. Metra responded on November 9, 2012 with additional details about the Project. The Forest County Potawatomi have not requested any further information. The other tribes did not respond to the consultation invitation.

4.3 Environmental Assessment Distribution and Public Comment Period

FTA will issue a Notice of Availability for this EA to provide the public an opportunity to review and comment on the EA. All comments received during the 30-day public comment period, and responses to those comments, will be incorporated into the final NEPA decision document. The EA will be available for review at the Gail Borden Public Library, Information Desk, 2nd Floor, 270 N. Grove Avenue, Elgin, IL 60120. A copy of the EA will be available on Metra's website at <https://metrarail.com/about-metra/reports-documents/project-studies/current-project-studies/z-100-ea> in pdf format and at Metra headquarters, 547 W. Jackson Boulevard, Chicago, IL 60661.

A public hearing will be scheduled to solicit comments from agencies and the public about findings presented in the EA. The hearing will be conducted in an open house format. The hearing will be advertised through display ads in regional and local newspapers, through Metra press releases, and flyers placed on Metra rail cars in the project area. Additional details concerning the hearing will also be posted on the Metra website. The hearing location will be near the project area, ADA-compliant, and accessible by public transit. Comments received during the hearing will be entered into the public record. A summary of the hearing and responses to substantive comments received, will be included in the final NEPA decision document. Written comments will be accepted at any time during the public comment period via U.S. mail to Metra, Grant Management & Accounting, 11th Floor, Attn: Milwaukee West Line Fox River Bridge Improvement Project (Metra Bridge Z-100), 547 W. Jackson Boulevard, Chicago, IL 60661. Comments will also be accepted at any time during the public comment period via email to: ProjectZ100NEPA@metrarr.com.

4.4 Next Steps

After review of the public comments received during the 30-day comment period and at the public hearing, FTA will issue a finding on the proposed project based on the significance of impacts identified during the NEPA process. FTA's finding will guide future planning and implementation of the Project.

Metra will continue to update and maintain a dedicated webpage to provide passengers and interested parties with information regarding work planned, scheduling, progress of the overall program, and other pertinent construction details. Information about the Project can be found at the following web address:

<https://metrarail.com/about-metra/reports-documents/project-studies/current-project-studies/z-100-ea>

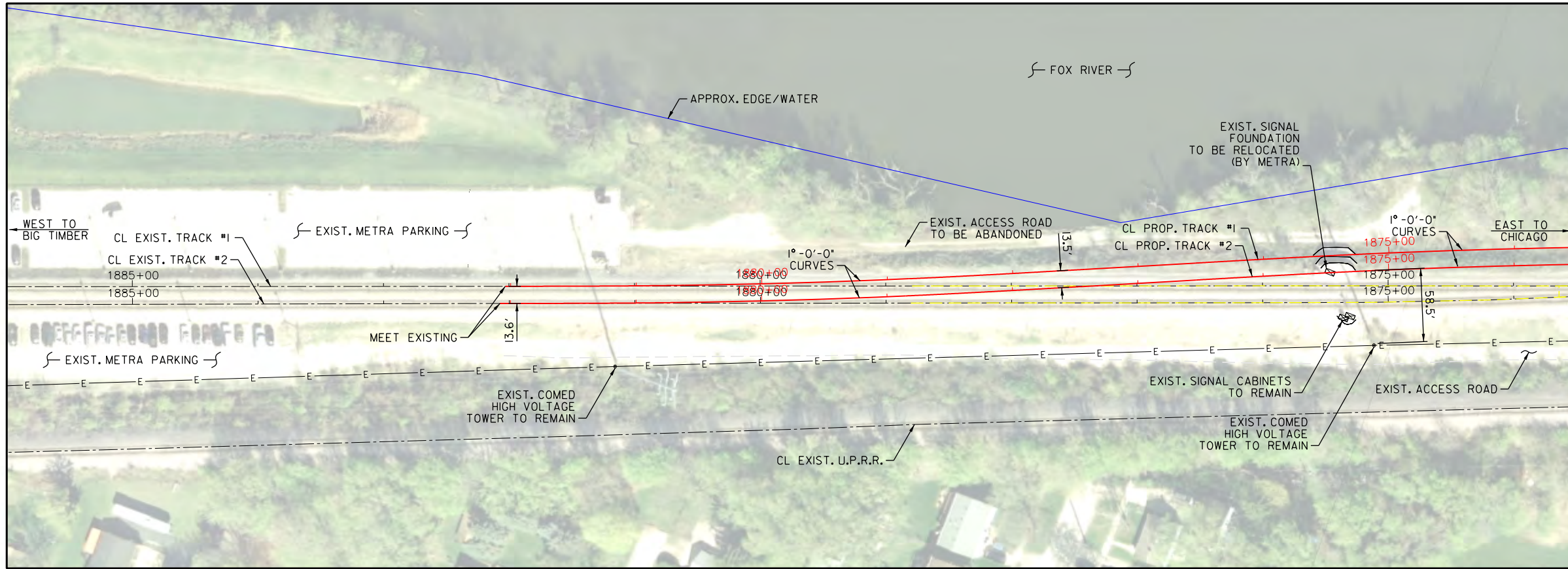
Efforts to minimize the impacts on riders and the surrounding community during construction, including temporary service delays to the Milwaukee West Line, would be scheduled to occur during weekends and off-peak periods when possible. Bus shuttle service during limited weekends would be provided, as needed, to ensure continual service for passengers.

Efforts would be undertaken through project development and construction to minimize disruption to communities and businesses during construction.

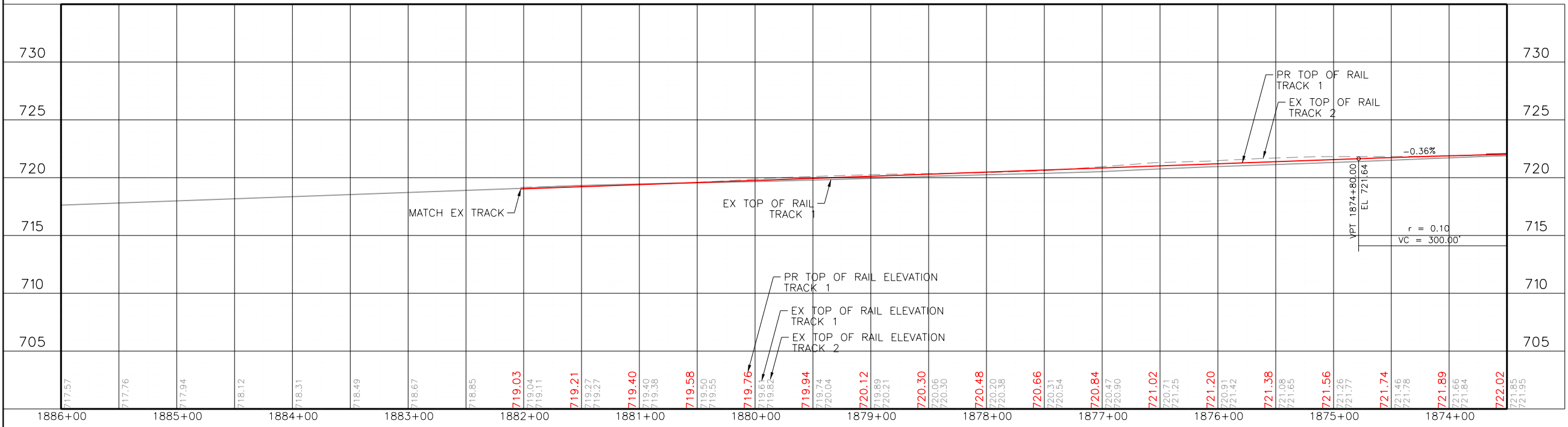
Appendix A Detailed Alternative Design Drawings

ALTERNATIVE 1A

New Double-Track Bridge on New Upstream Alignment



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---	EXISTING STRUCTURE
---	PROPOSED STRUCTURE
---	EDGE OF RIVER
---	OVERHEAD ELECTRIC



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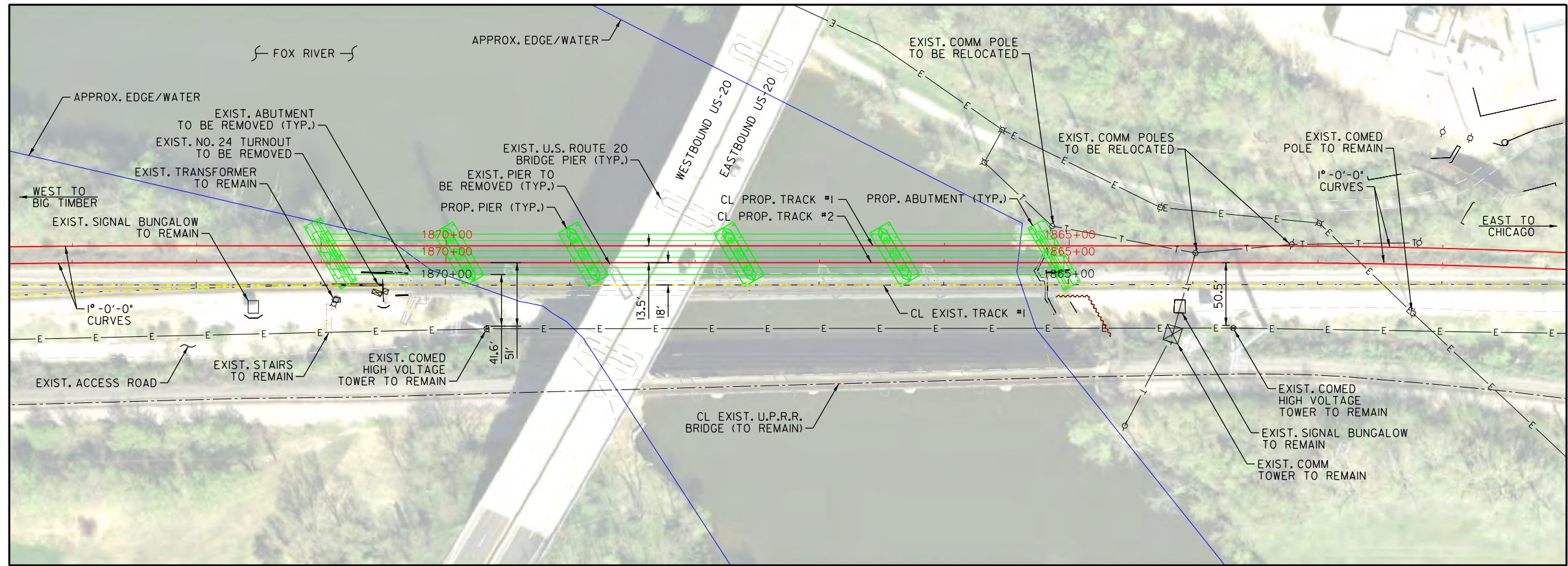
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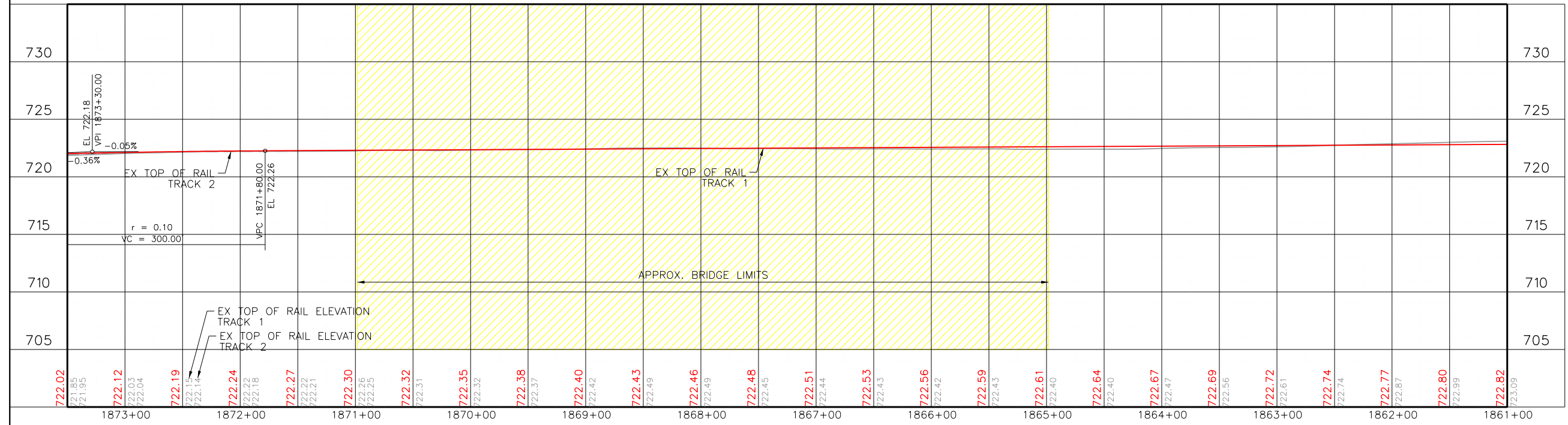
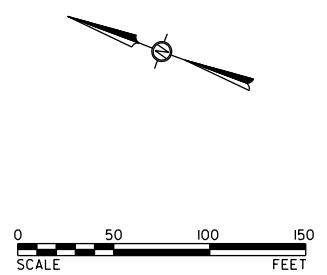
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**MILWAUKEE DISTRICT WEST
BRIDGE Z-100 OVER FOX RIVER**
KANE COUNTY - ELGIN, ILLINOIS

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ENVIRONMENTAL ANALYSIS EXHIBIT SET
ALTERNATIVE IA
NEW DOUBLE TRACK BRIDGE ON
NEW UPSTREAM ALIGNMENT

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SHEET NO.
AIA-1
1 OF 25



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--- (blue solid)	EDGE OF RIVER
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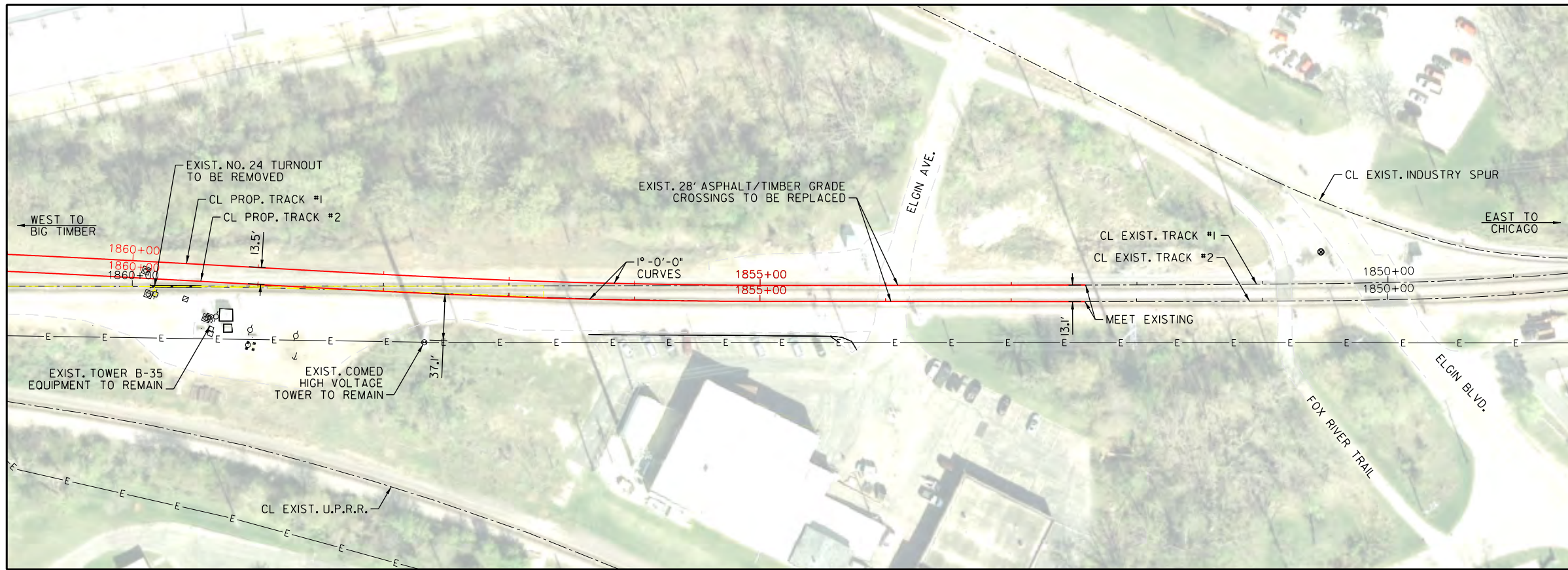
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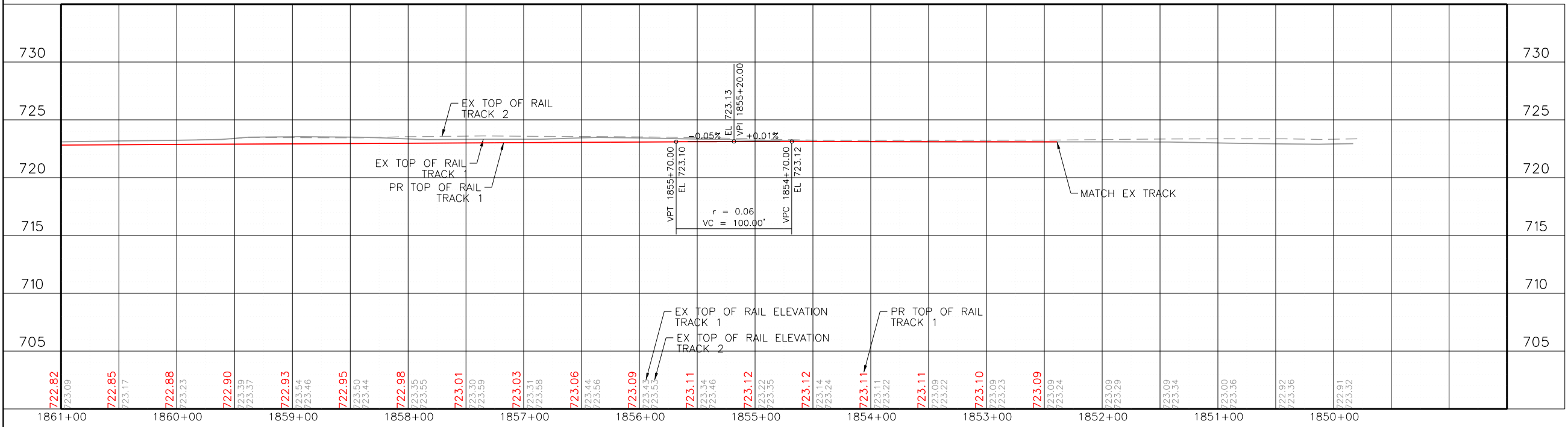
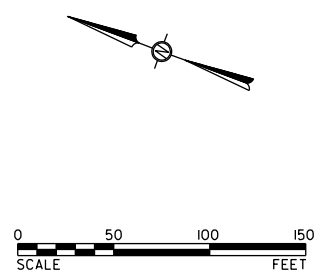
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---	OVERHEAD ELECTRIC



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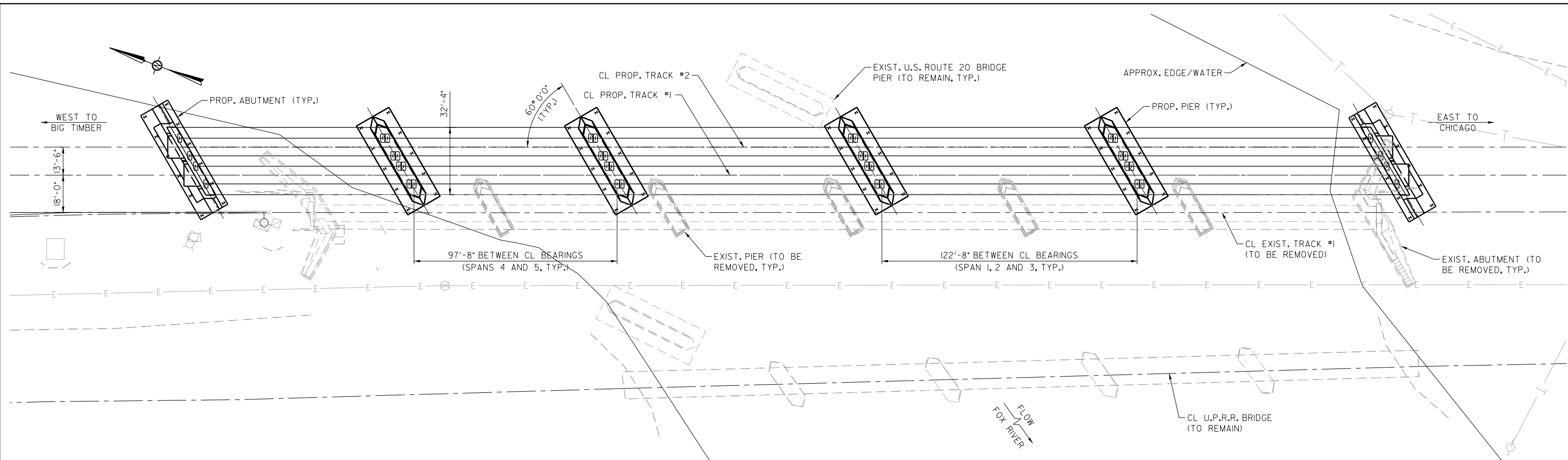
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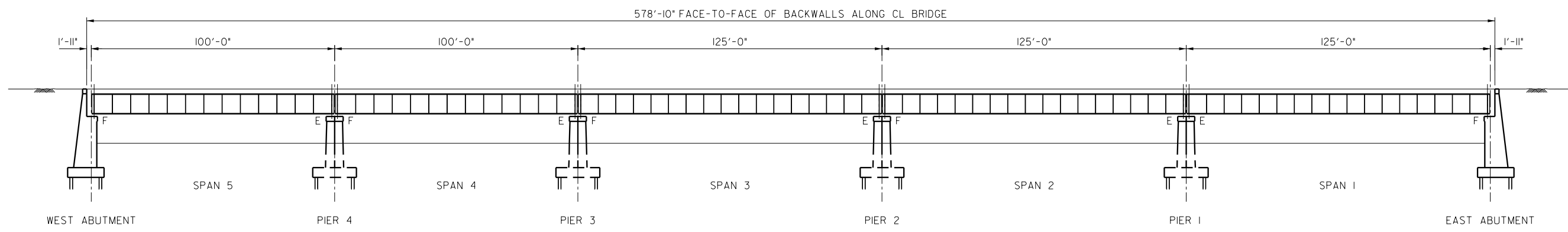
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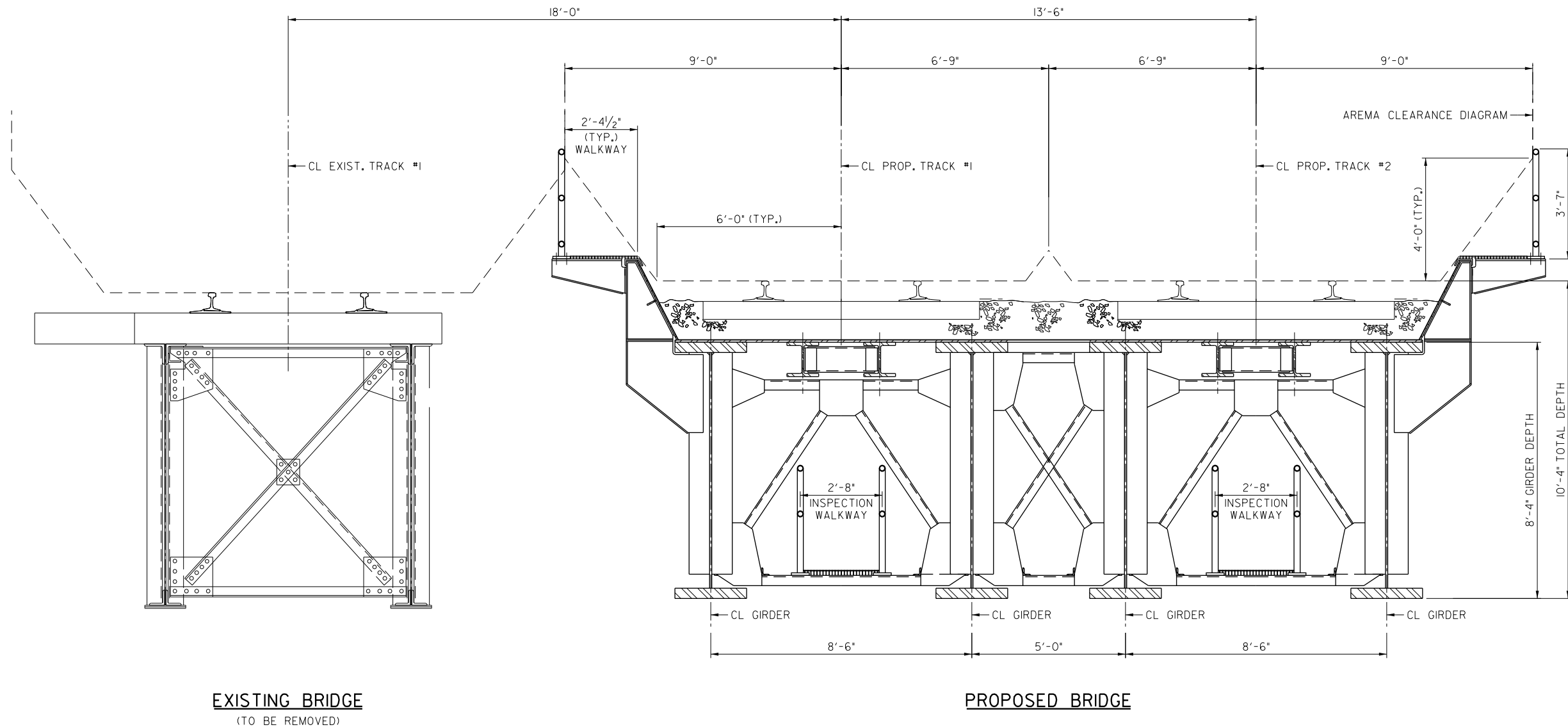


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TYPICAL CROSS SECTION
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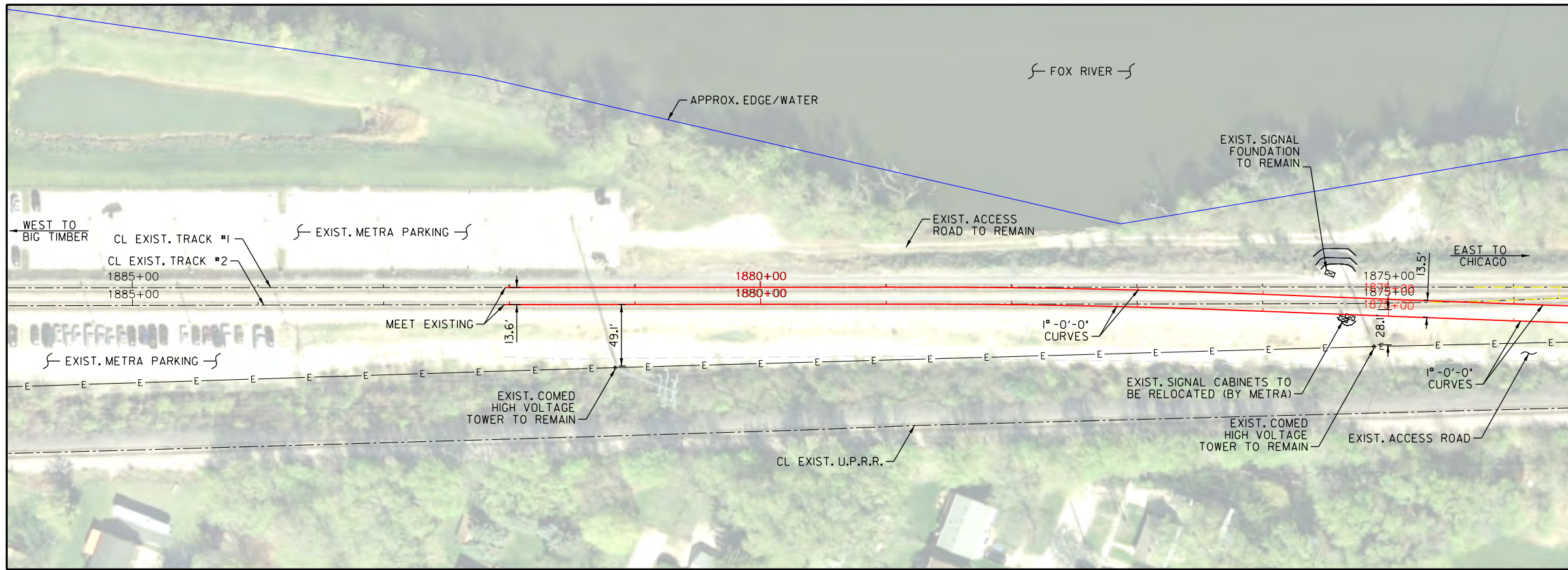
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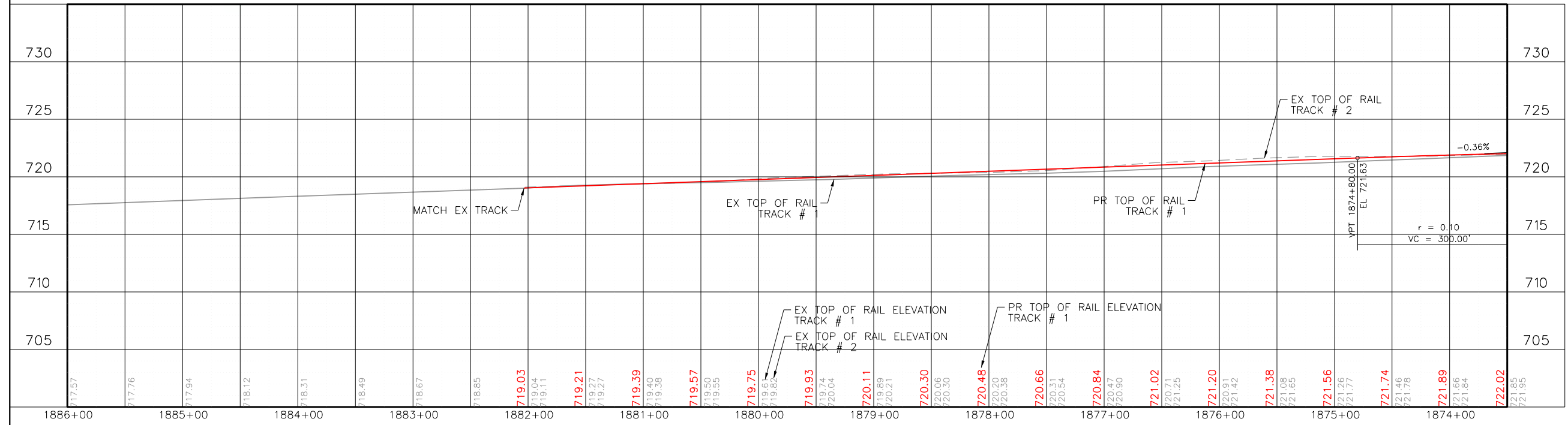
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ALTERNATIVE 1B

New Double-Track Bridge on New Downstream Alignment



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--- (dashed grey line)	EXISTING STRUCTURE
— (solid green line)	PROPOSED STRUCTURE
— (solid blue line)	EDGE OF RIVER
—E— (line with 'E' markers)	OVERHEAD ELECTRIC



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FAX: 847-605-9610

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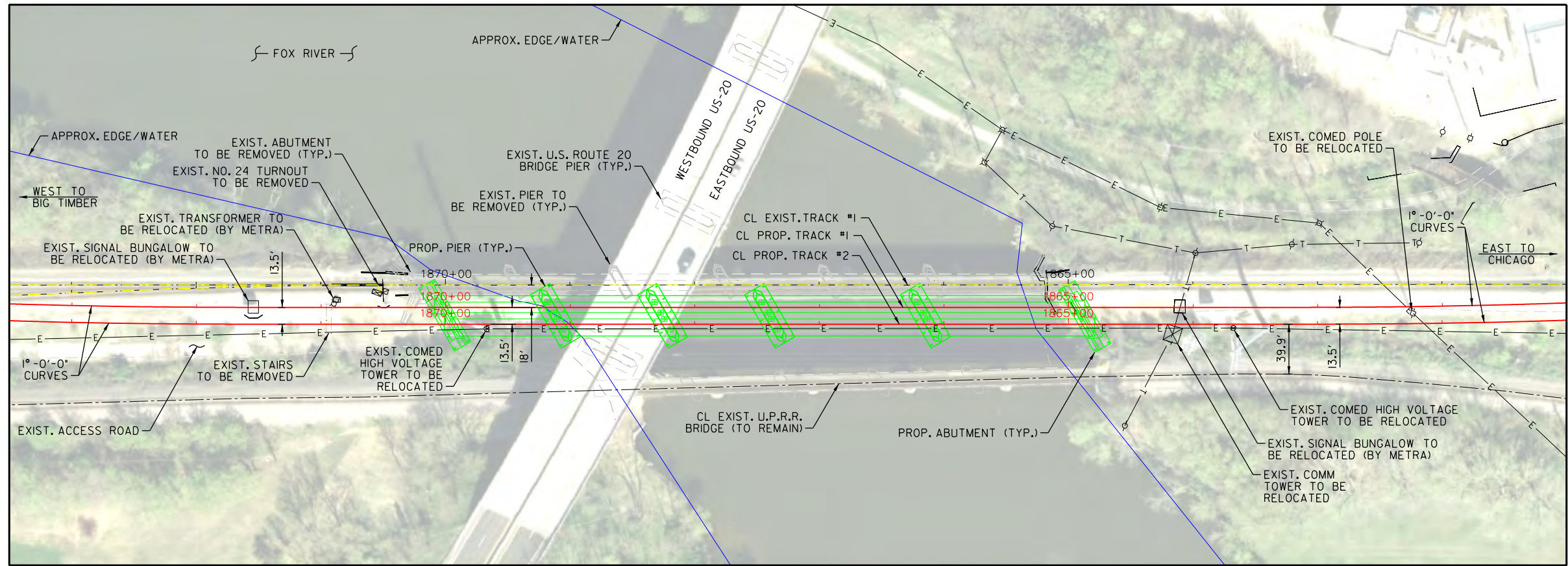
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KANE COUNTY - ELGIN, ILLINOIS

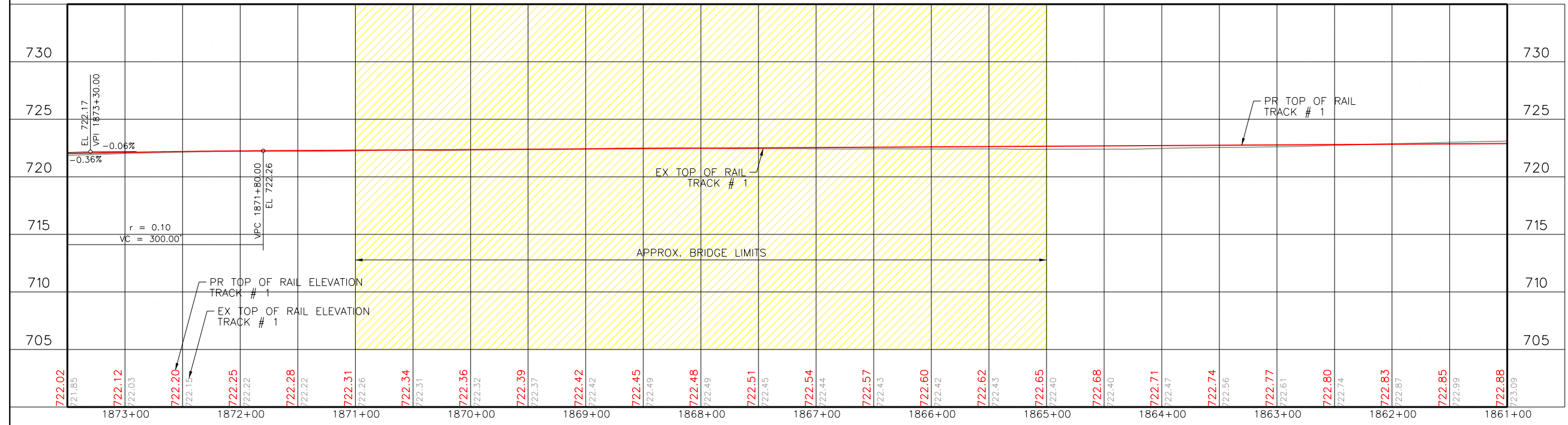
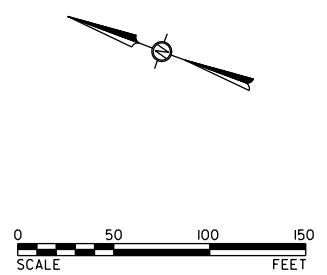
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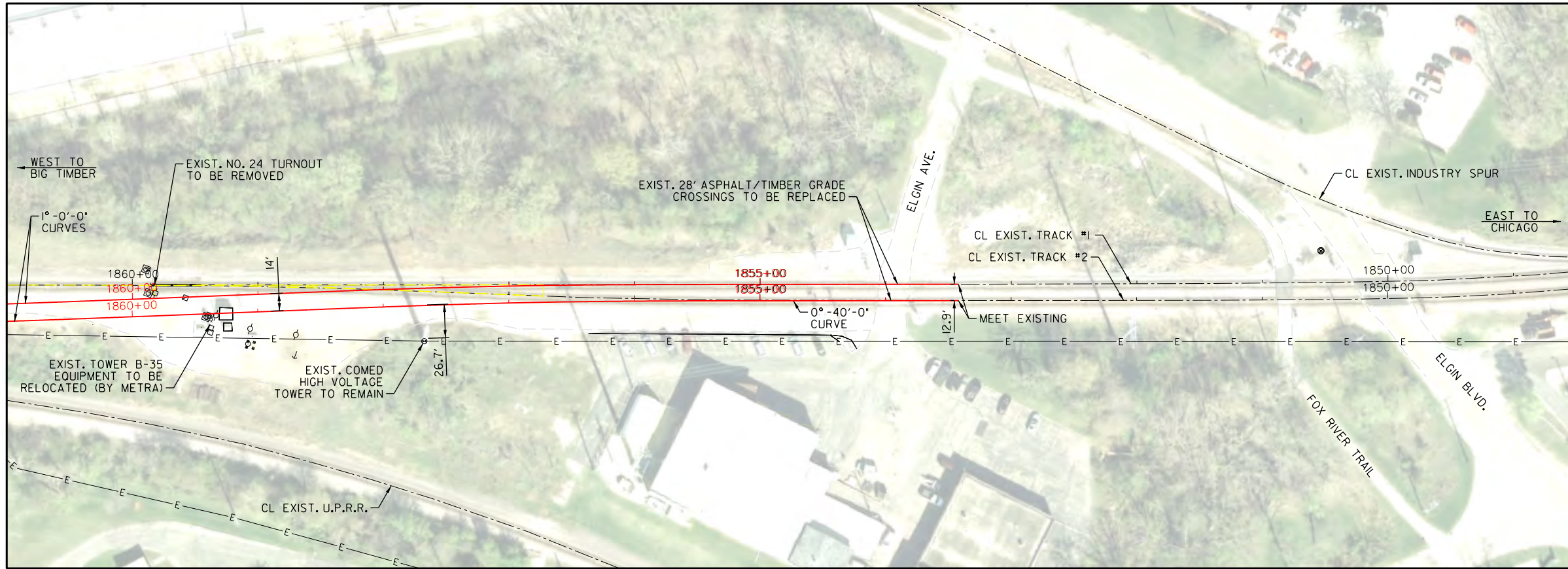
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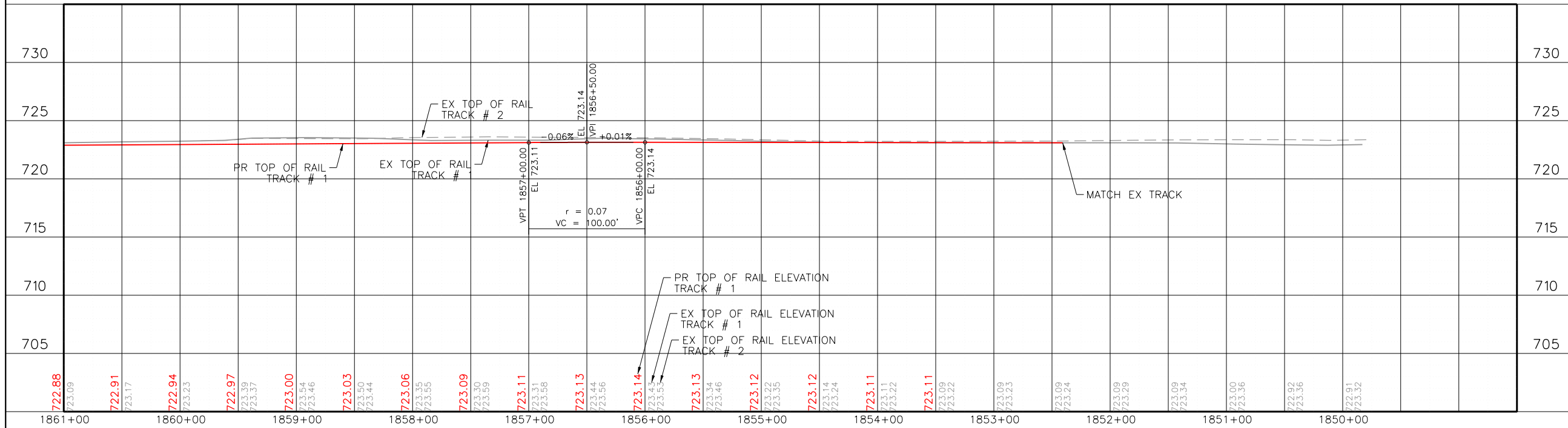
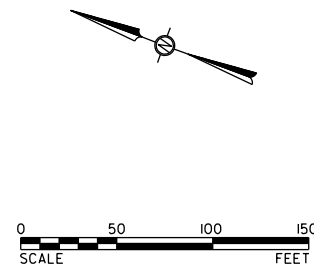
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— (solid blue line)	EDGE OF RIVER
—E— (line with 'E' markers)	OVERHEAD ELECTRIC



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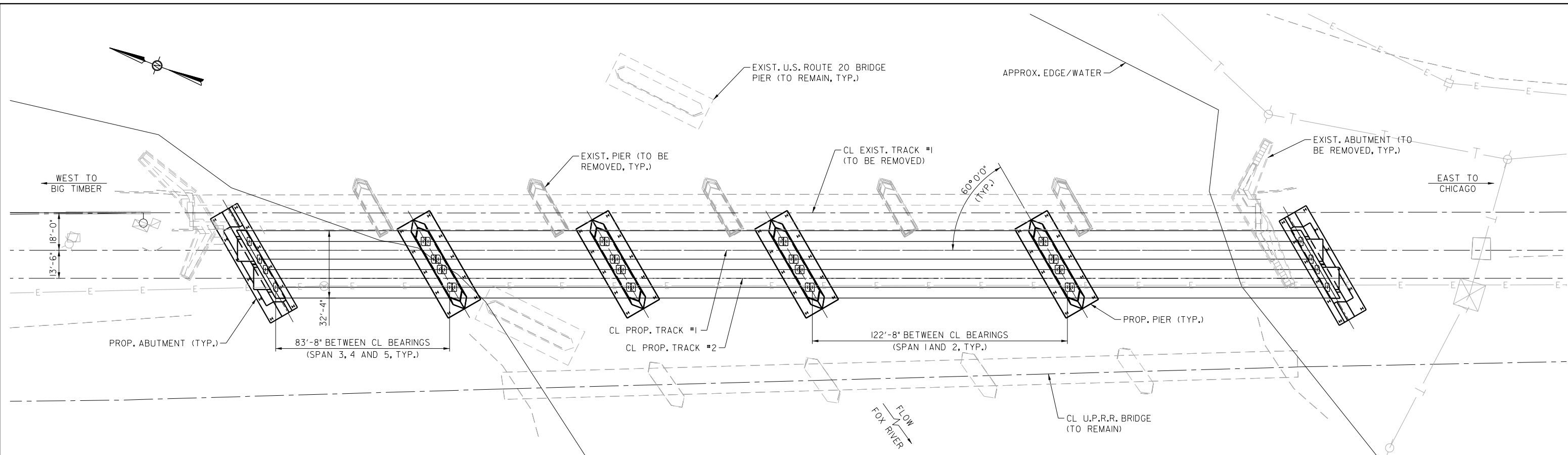
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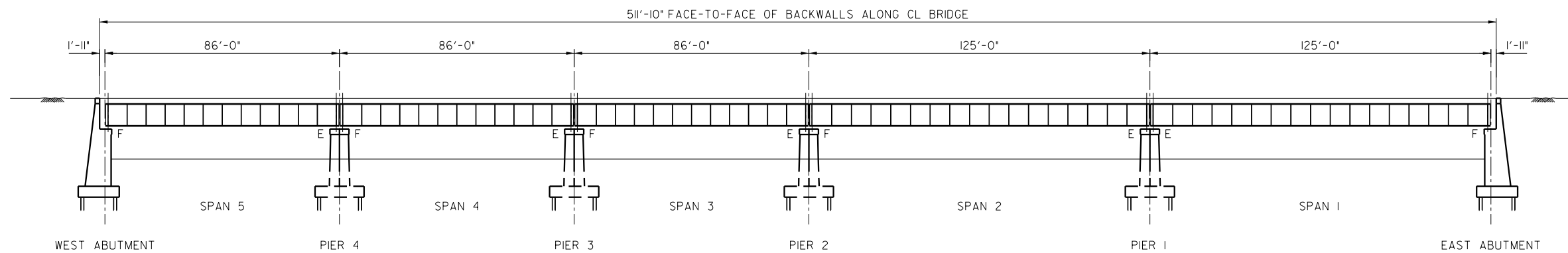
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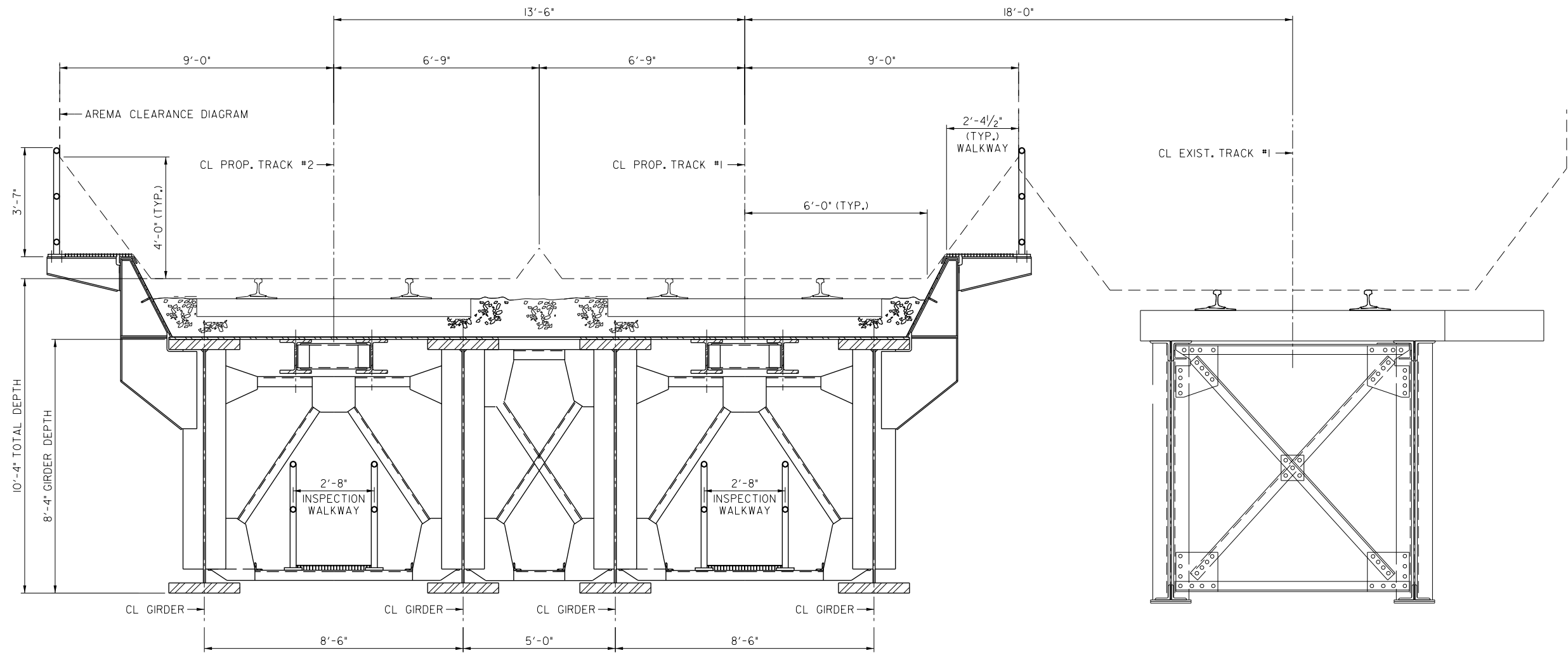
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9 OF 25



PROPOSED BRIDGE

EXISTING BRIDGE
(TO BE REMOVED)

TYPICAL CROSS SECTION

(LOOKING WEST UPSTATION)

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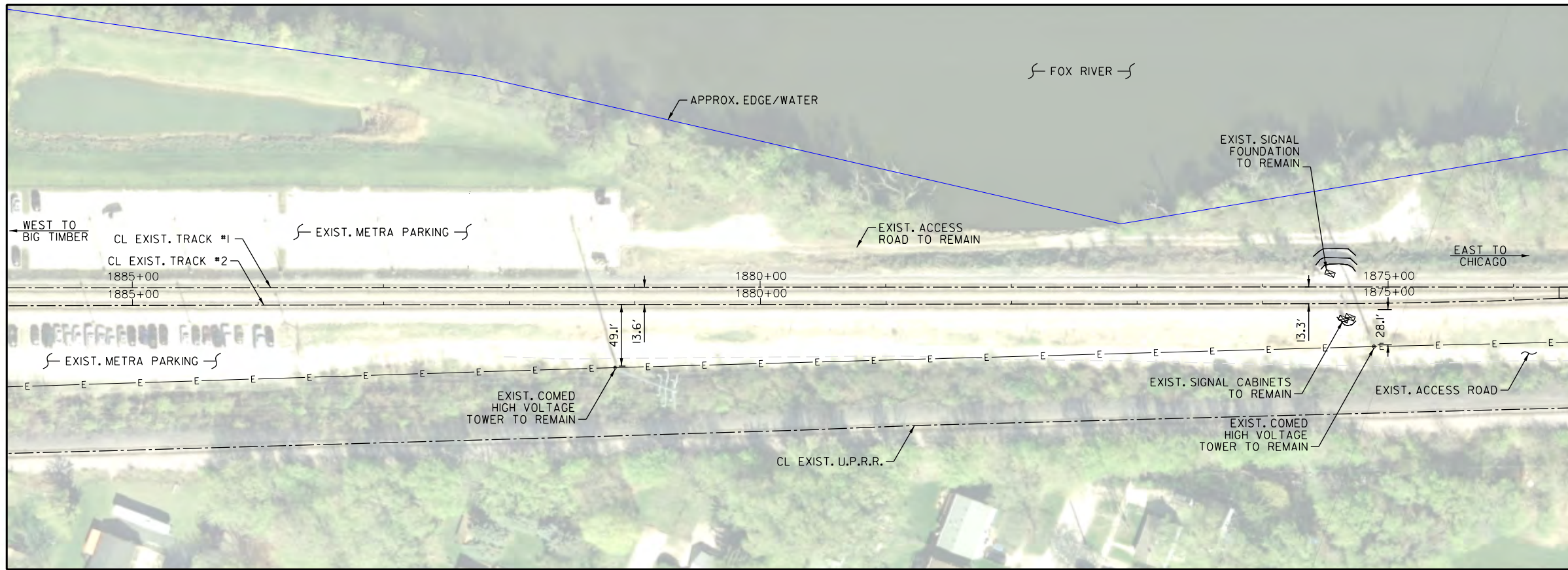
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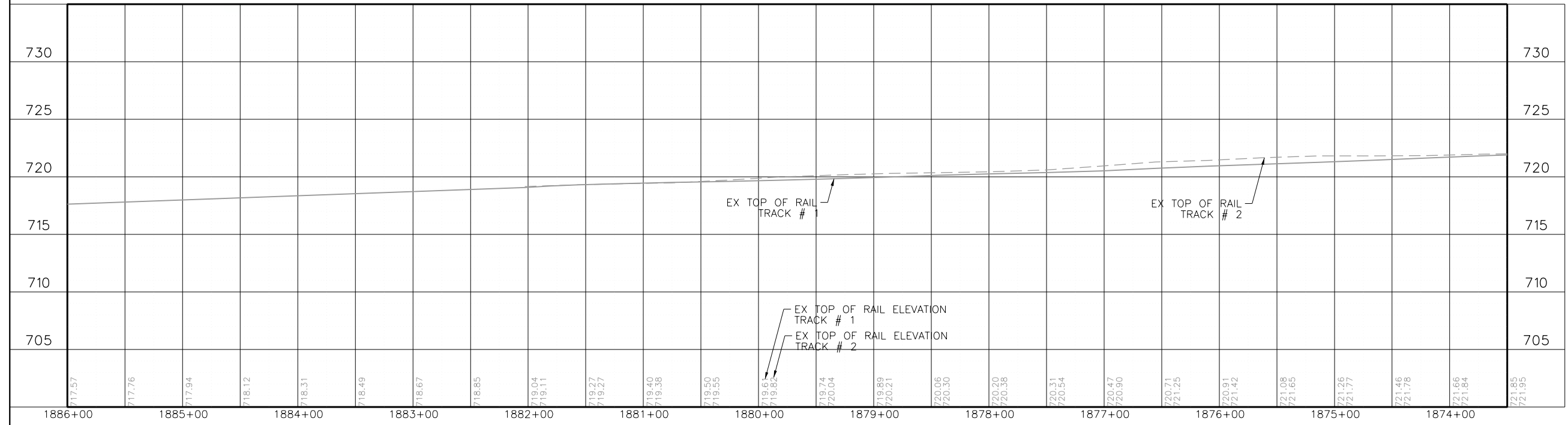
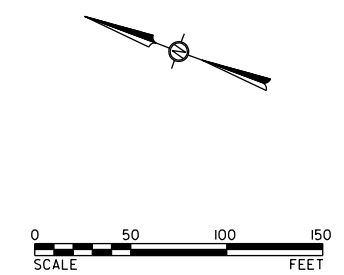
ALTERNATIVE 2

New Single-Track Bridge on the Existing Alignment



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---	CL PROPOSED TRACK
- - - -	CL TRACK REMOVAL
- - - -	EXISTING STRUCTURE
---	PROPOSED STRUCTURE
---	EDGE OF RIVER
—E—	OVERHEAD ELECTRIC

MATCH LINE
SEE DRAWING A2-2



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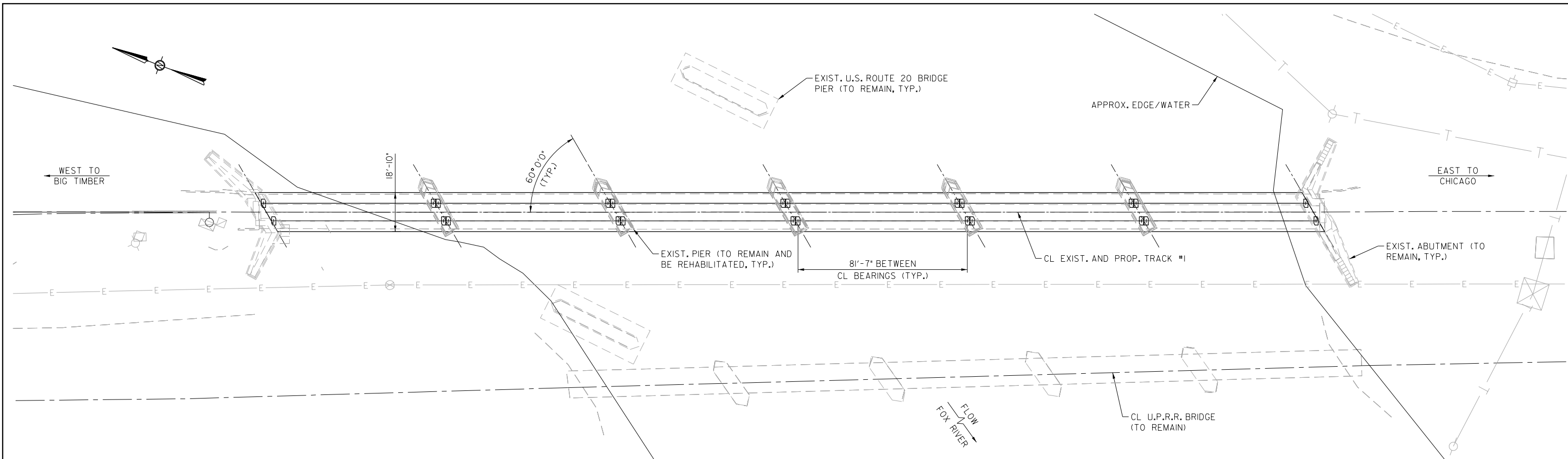
ENGINEERING DEPARTMENT
547 W. JACKSON BLVD.
CHICAGO, ILLINOIS 60601

PROJECT TITLE:
**MILWAUKEE DISTRICT WEST
BRIDGE Z-100 OVER FOX RIVER**
KANE COUNTY - ELGIN, ILLINOIS

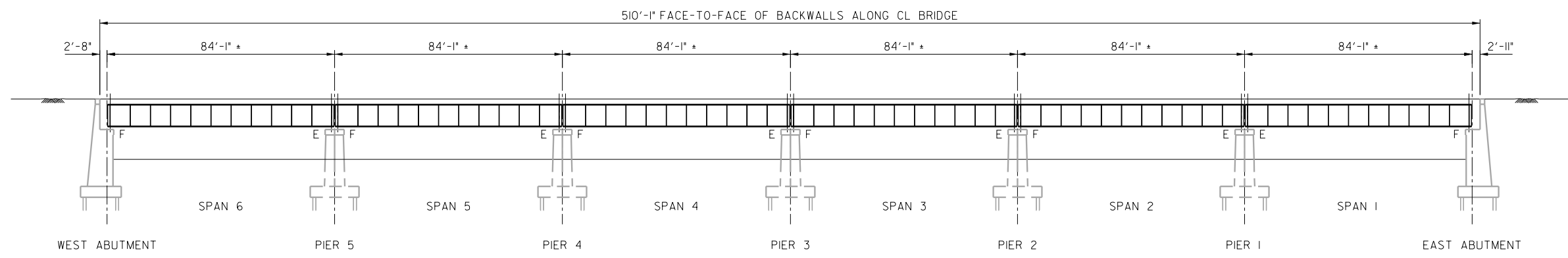
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ENVIRONMENTAL ANALYSIS EXHIBIT SET
ALTERNATIVE 2
NEW SINGLE TRACK BRIDGE
ON THE EXISTING ALIGNMENT

CADD FILE NAME:
0114-CT-I21.dgn
SHEET NO.
A2-1
II OF 25

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PLAN



ELEVATION

wjcollet11
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PRIMARY CONSULTANT

1475 E. WOODFIELD ROAD, SUITE 600
SCHAUMBURG, IL 60173
PHONE: 847-605-9600
FAX: 847-605-9610

SUB CONSULTANT

		DATE: 05-16-16		REVISIONS	
NO.	BY	DATE	DESCRIPTION		
	WJC	05-16-16	ISSUED FOR REVIEW		
	WJC				
	MDS				

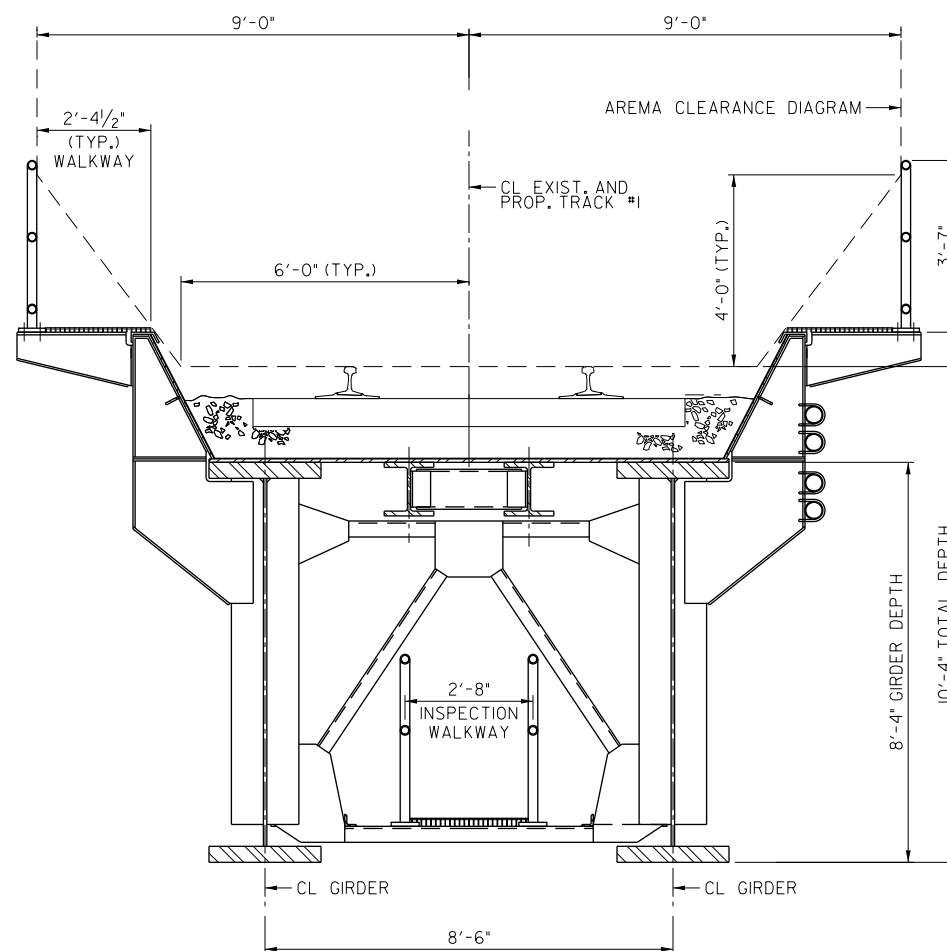
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ENGINEERING DEPARTMENT
547 W. JACKSON BLVD.
CHICAGO, ILLINOIS 60601

PROJECT TITLE:
**MILWAUKEE DISTRICT WEST
BRIDGE Z-100 OVER FOX RIVER**
KANE COUNTY - ELGIN, ILLINOIS

SHEET TITLE:
ENVIRONMENTAL ANALYSIS EXHIBIT SET
ALTERNATIVE 2
NEW SINGLE TRACK BRIDGE
ON THE EXISTING ALIGNMENT

CADD FILE NAME:
014-BR-201.dgn
SHEET NO.
A2-4
14 OF 25



TYPICAL CROSS SECTION

(LOOKING WEST UPSTATION)

wjcollett1
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PRIMARY CONSULTANT



1475 E. WOODFIELD ROAD, SUITE 600
 SCHAUMBURG, IL 60173
 PHONE: 847-605-9600
 FAX: 847-605-9610

SUB CONSULTANT

		DATE: 05-16-16		REVISIONS	
DGN BY:	NO.	BY	DATE	DESCRIPTION	
WJC			05-16-16	ISSUED FOR REVIEW	
CHK BY:					
METRA PROJ MGR:					
CONTRACT NO.:					
PROJ. NO.:					
SCALE:	4,000 ' / in.				



ENGINEERING DEPARTMENT
 547 W. JACKSON BLVD.
 CHICAGO, ILLINOIS 60601

PROJECT TITLE:
**MILWAUKEE DISTRICT WEST
 BRIDGE Z-100 OVER FOX RIVER**
 KANE COUNTY - ELGIN, ILLINOIS

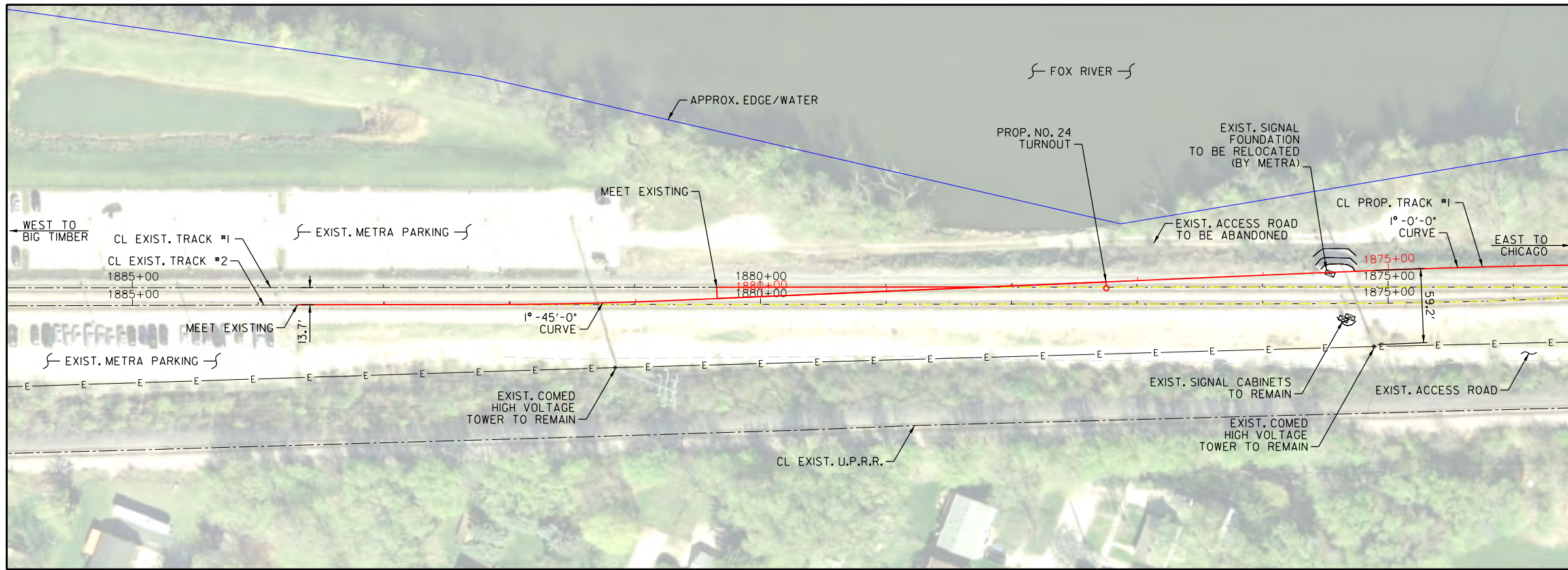
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 ALTERNATIVE 2
 NEW SINGLE TRACK BRIDGE
 ON THE EXISTING ALIGNMENT

CADD FILE NAME:
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SHEET NO.
A2-5

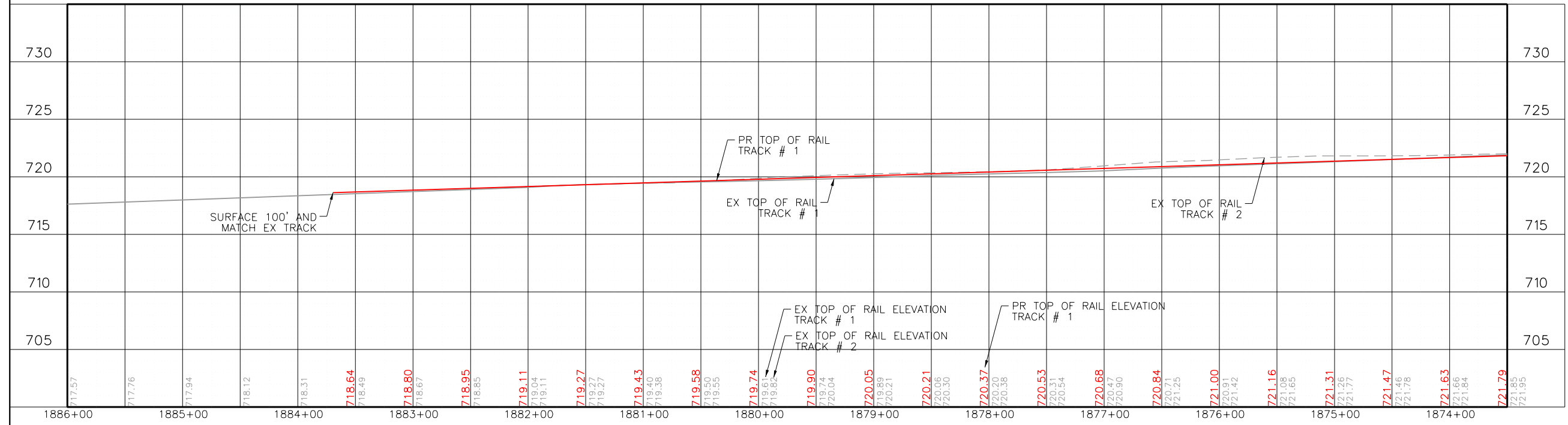
ALTERNATIVE 3

New Single-Track Bridge on an Upstream Alignment



LEGEND	
---	CL EXISTING TRACK
---	CL PROPOSED TRACK
---	CL TRACK REMOVAL
---	EXISTING STRUCTURE
---	PROPOSED STRUCTURE
---	EDGE OF RIVER
---	OVERHEAD ELECTRIC

MATCH LINE
SEE DRAWING A3-2



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PRIMARY CONSULTANT

1475 E. WOODFIELD ROAD, SUITE 600
SCHALMBURG, IL 60173
PHONE: 847-605-9600
FAX: 847-605-9610

SUB CONSULTANT

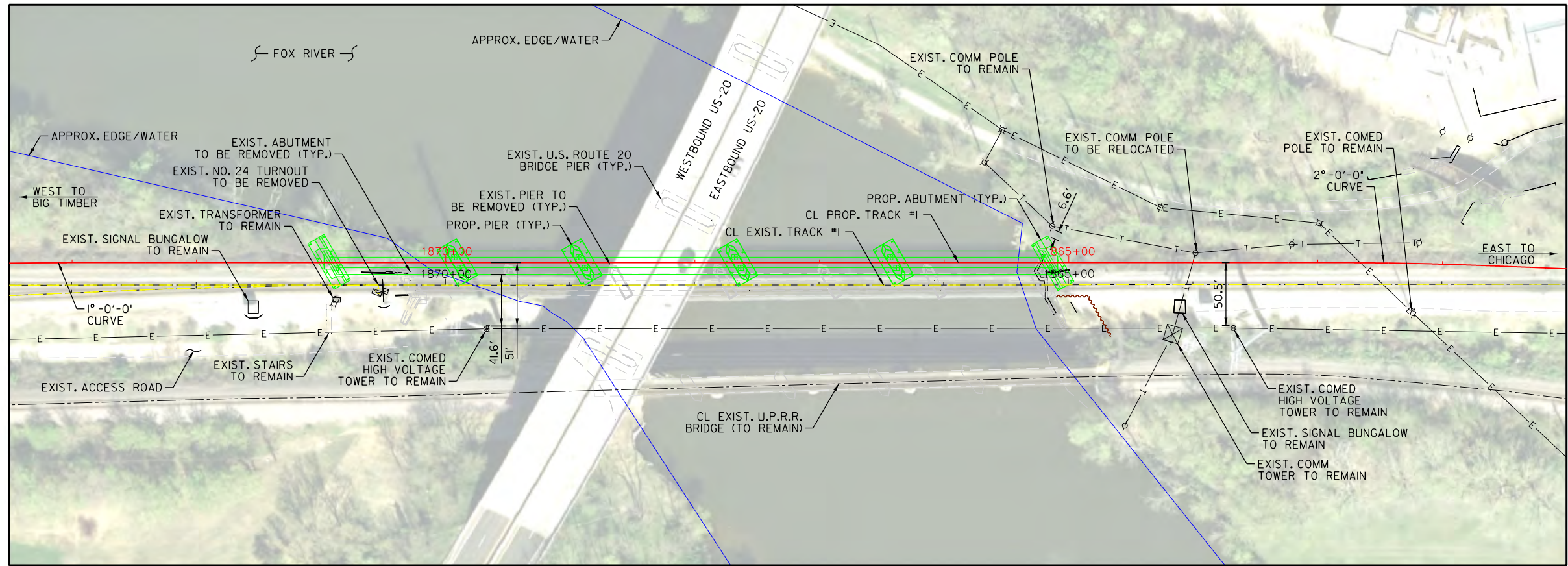
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DGN BY:	NO.	BY	DESCRIPTION
CIG			
CIG			ISSUED FOR REVIEW
BSM			
METRA PROJ MGR:			
CONTRACT NO.:			
PROJ. NO.:			
SCALE: 100,000' / in.			

ENGINEERING DEPARTMENT
547 W. JACKSON BLVD.
CHICAGO, ILLINOIS 60601

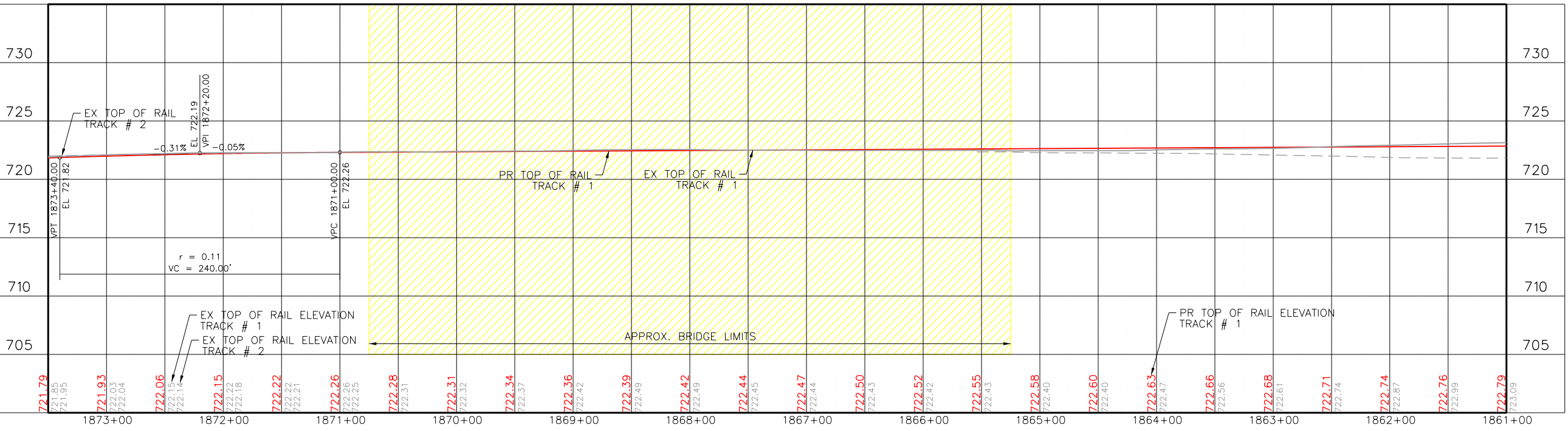
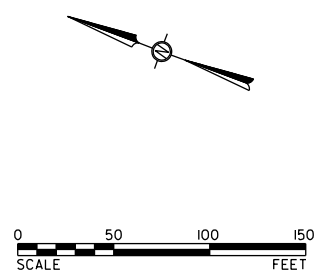
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**MILWAUKEE DISTRICT WEST
BRIDGE Z-100 OVER FOX RIVER**
KANE COUNTY - ELGIN, ILLINOIS

SHEET TITLE:
ENVIRONMENTAL ANALYSIS EXHIBIT SET
ALTERNATIVE 3
NEW SINGLE TRACK BRIDGE ON
NEW UPSTREAM ALIGNMENT

CADD FILE NAME:
0114-CT-131.dgn
SHEET NO.
A3-1
16 OF 25



LEGEND	
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--- (dashed yellow line)	CL TRACK REMOVAL
--- (dashed grey line)	EXISTING STRUCTURE
— (solid green line)	PROPOSED STRUCTURE
— (solid blue line)	EDGE OF RIVER
—E— (line with 'E' markers)	OVERHEAD ELECTRIC



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PRIMARY CONSULTANT

1475 E. WOODFIELD ROAD, SUITE 600
SCHAMBURG, IL 60173
PHONE: 847-605-9600
FAX: 847-605-9610

SUB CONSULTANT

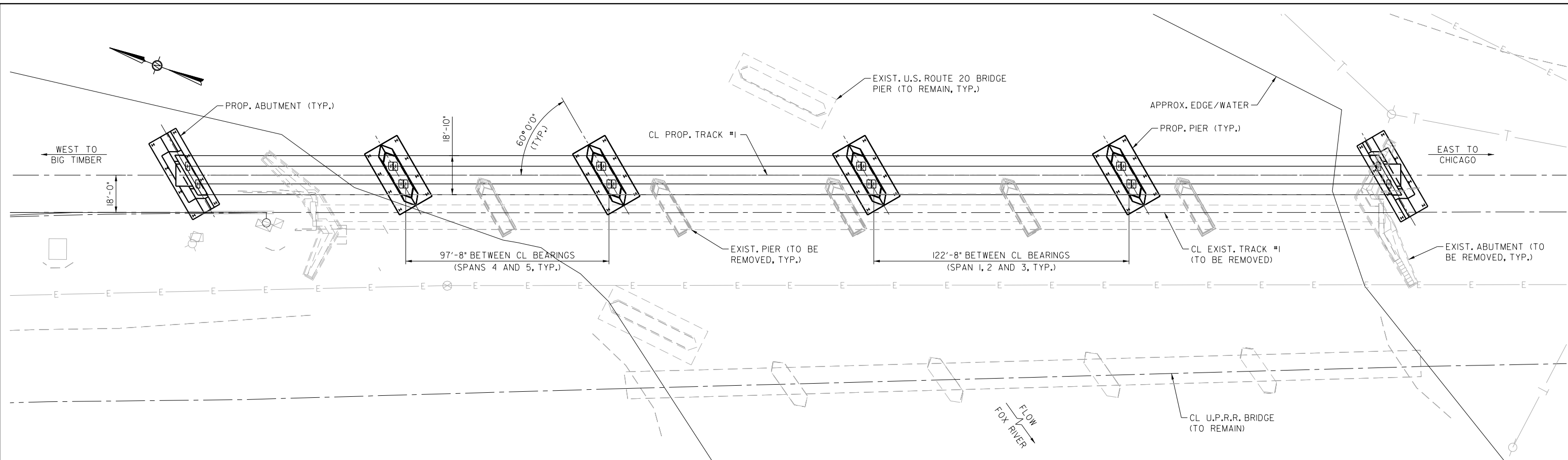
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DWN BY:			
CIG			ISSUED FOR REVIEW
CHK BY:			
BSM			
METRA PROJ MGR:			
CONTRACT NO.:			
PROJ. NO.:			
SCALE: 100,000' / in.			

ENGINEERING DEPARTMENT
547 W. JACKSON BLVD.
CHICAGO, ILLINOIS 60601

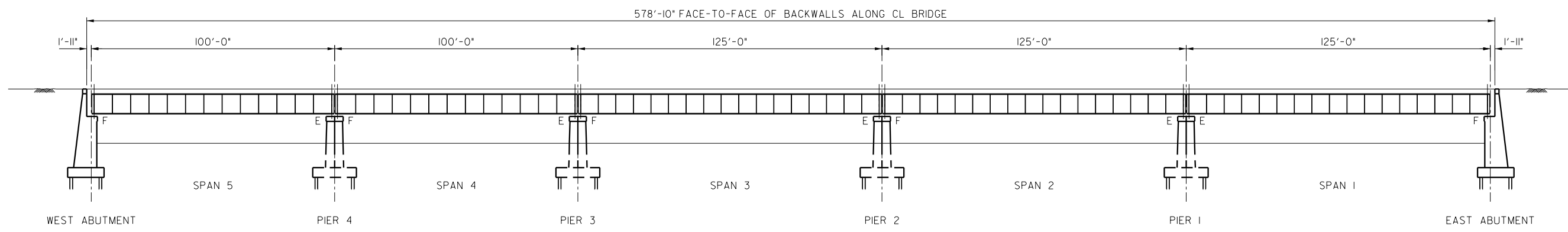
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**MILWAUKEE DISTRICT WEST
BRIDGE Z-100 OVER FOX RIVER**
KANE COUNTY - ELGIN, ILLINOIS

SHEET TITLE:
ENVIRONMENTAL ANALYSIS EXHIBIT SET
ALTERNATIVE 3
NEW SINGLE TRACK BRIDGE ON
NEW UPSTREAM ALIGNMENT

CADD FILE NAME:
0114-CT-132.dgn
SHEET NO.
A3-2
17 OF 25



PLAN



ELEVATION

wjcollett1
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PRIMARY CONSULTANT

1475 E. WOODFIELD ROAD, SUITE 600
 SCHAUMBURG, IL 60173
 PHONE: 847-605-9600
 FAX: 847-605-9610

SUB CONSULTANT

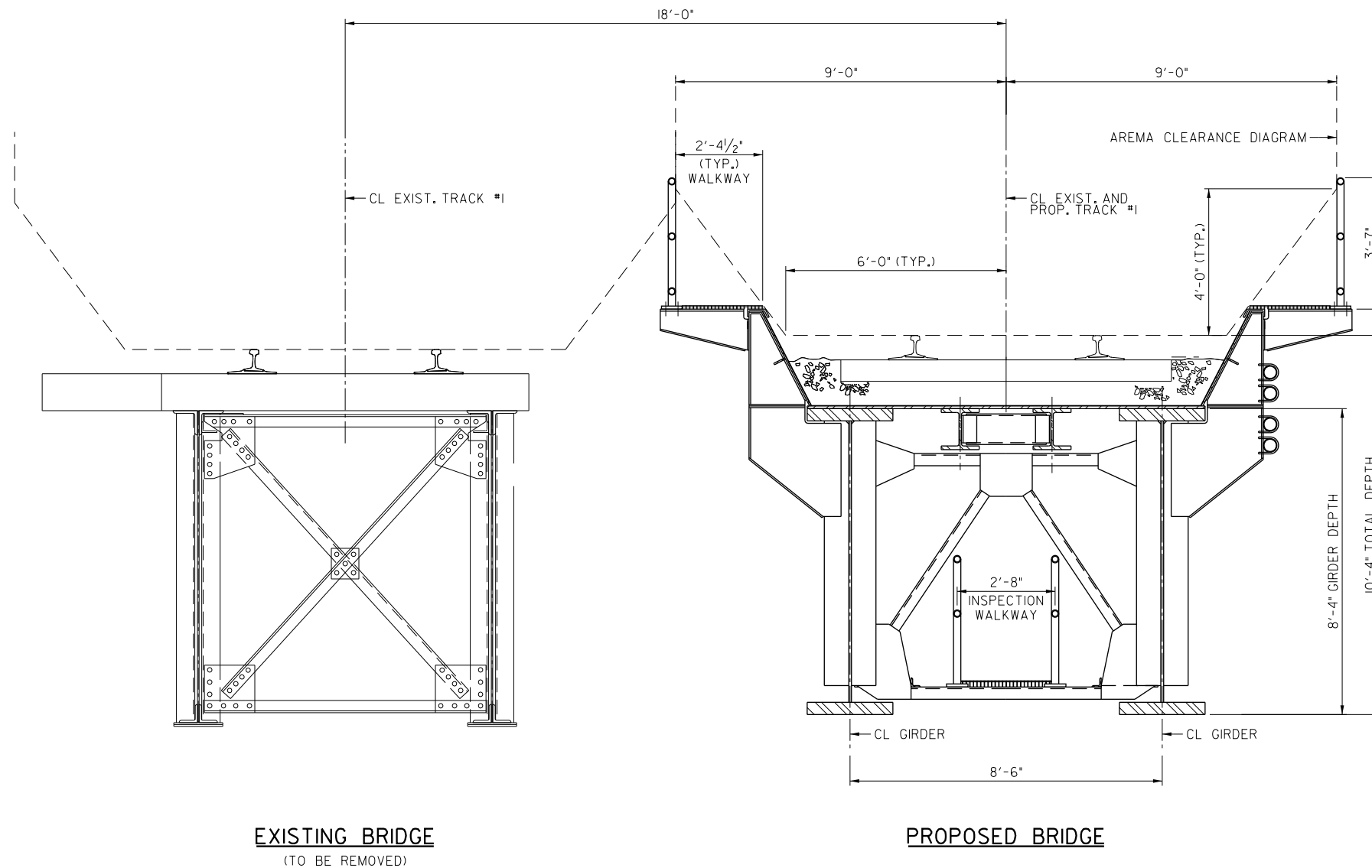
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DGN BY:	NO.	BY	DESCRIPTION
WJC			ISSUED FOR REVIEW
WJC			
MDS			
METRA PROJ MGR:			
CONTRACT NO.			
PROJ. NO.:			
SCALE: 46.667' / in.			

ENGINEERING DEPARTMENT
 547 W. JACKSON BLVD.
 CHICAGO, ILLINOIS 60601

PROJECT TITLE:
**MILWAUKEE DISTRICT WEST
 BRIDGE Z-100 OVER FOX RIVER**
 KANE COUNTY - ELGIN, ILLINOIS

SHEET TITLE:
 ENVIRONMENTAL ANALYSIS EXHIBIT SET
 ALTERNATIVE 3
 NEW SINGLE TRACK BRIDGE ON
 NEW UPSTREAM ALIGNMENT

CADD FILE NAME:
 014-BR-301.dgn
 SHEET NO.
A3-4
 19 OF 25



TYPICAL CROSS SECTION
(LOOKING WEST UPSTATION)

w/jcollett1
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PRIMARY CONSULTANT



1475 E. WOODFIELD ROAD, SUITE 600
SCHAUMBURG, IL 60173
PHONE: 847-605-9600
FAX: 847-605-9610

SUB CONSULTANT

		REVISIONS	
NO.	BY	DATE	DESCRIPTION
		05-16-16	ISSUED FOR REVIEW

DATE: 05-16-16
DGN BY: WJC
DWN BY: WJC
CHK BY: MDS
METRA PROJ MGR:
CONTRACT NO.
PROJ. NO.:
SCALE: 4,000 ' / in.



ENGINEERING DEPARTMENT
547 W. JACKSON BLVD.
CHICAGO, ILLINOIS 60601

PROJECT TITLE:
**MILWAUKEE DISTRICT WEST
BRIDGE Z-100 OVER FOX RIVER**
KANE COUNTY - ELGIN, ILLINOIS

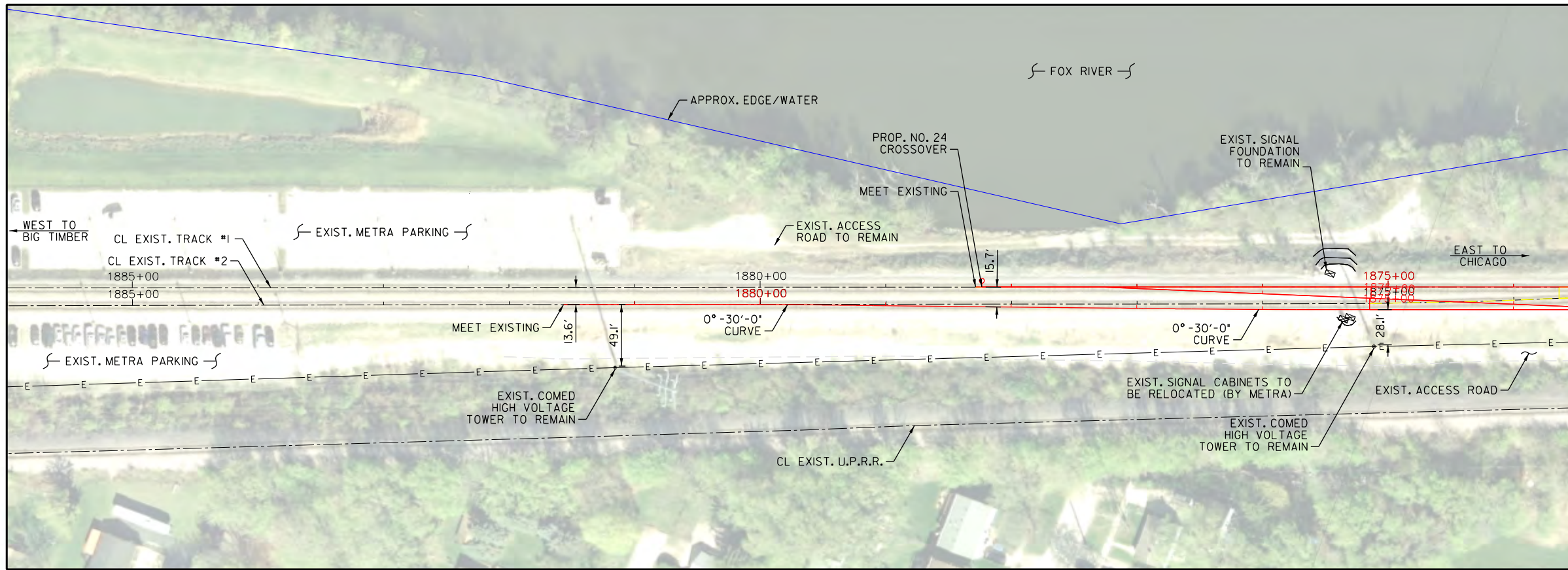
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ENVIRONMENTAL ANALYSIS EXHIBIT SET
ALTERNATIVE 3
NEW SINGLE TRACK BRIDGE ON
NEW UPSTREAM ALIGNMENT

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SHEET NO.
A3-5
20 OF 25

ALTERNATIVE 4

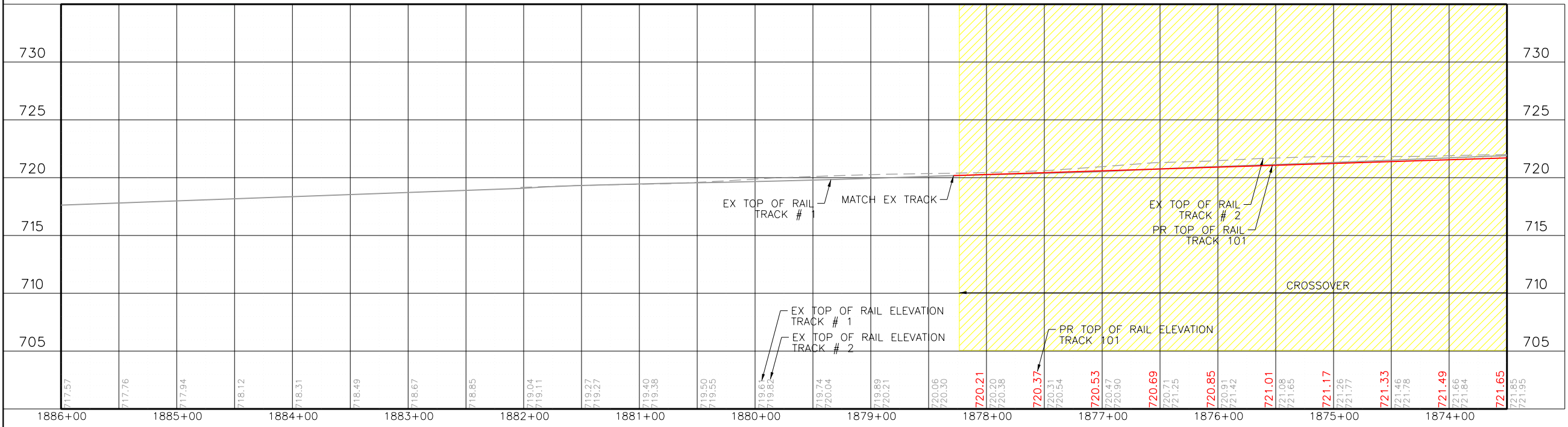
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(Preferred Build Alternative)



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--- (red dashed)	CL PROPOSED TRACK
--- (yellow dashed)	CL TRACK REMOVAL
--- (grey dashed)	EXISTING STRUCTURE
--- (green dashed)	PROPOSED STRUCTURE
--- (blue dashed)	EDGE OF RIVER
--- (black dashed with 'E')	OVERHEAD ELECTRIC

MATCH LINE
SEE DRAWING A4-2



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PRIMARY CONSULTANT

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SCHALMURG, IL 60173
PHONE: 847-605-9600
FAX: 847-605-9610

SUB CONSULTANT

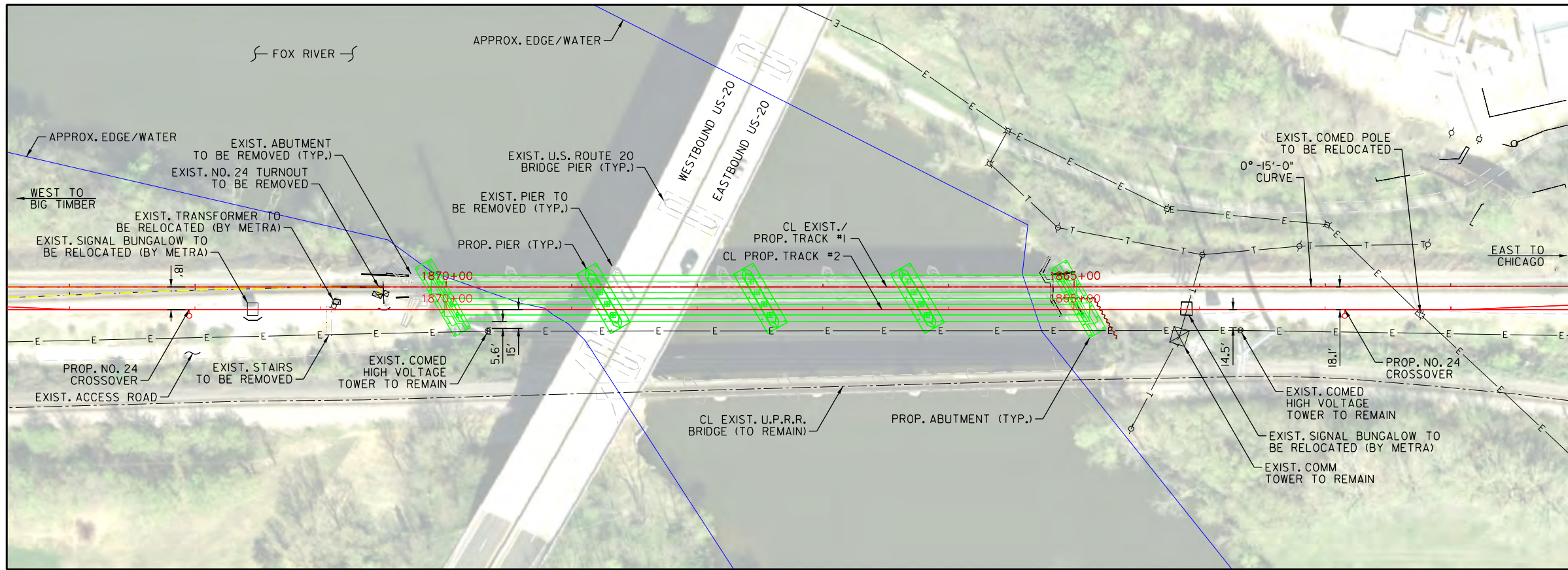
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DGN BY:	NO.	BY	DESCRIPTION
CIG			ISSUED FOR REVIEW
CHK BY:			
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SCALE: 100,000' / in.			

ENGINEERING DEPARTMENT
547 W. JACKSON BLVD.
CHICAGO, ILLINOIS 60601

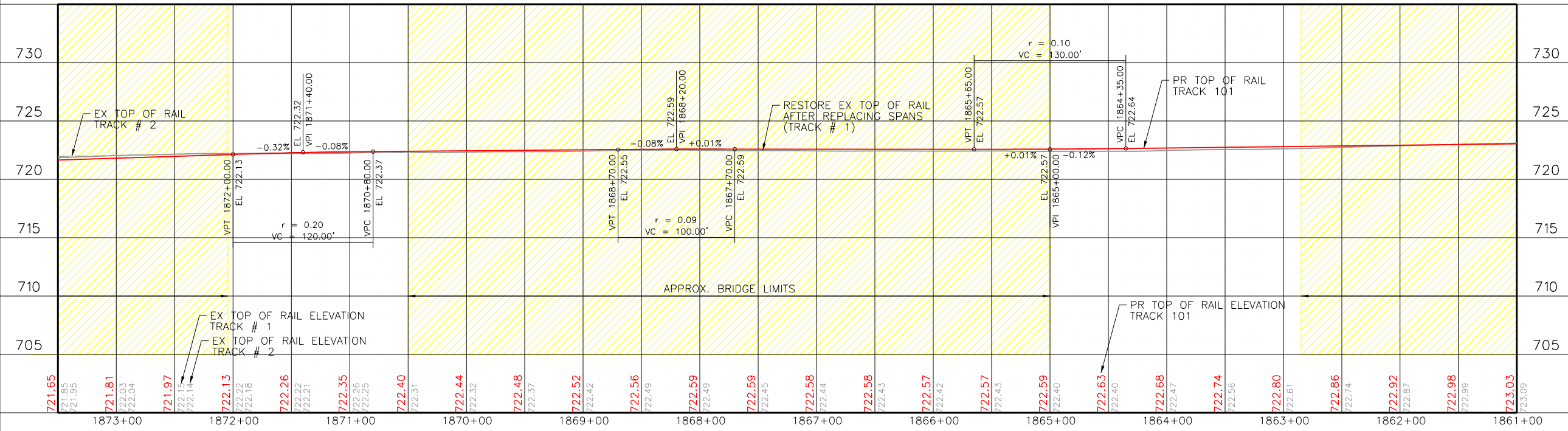
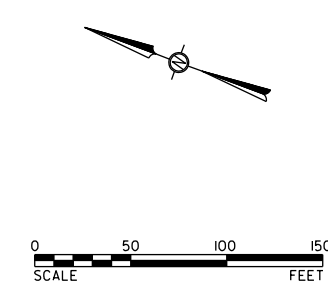
PROJECT TITLE:
**MILWAUKEE DISTRICT WEST
BRIDGE Z-100 OVER FOX RIVER**
KANE COUNTY - ELGIN, ILLINOIS

SHEET TITLE:
ENVIRONMENTAL ANALYSIS EXHIBIT SET
ALTERNATIVE 4
NEW DOUBLE TRACK BRIDGE ON
DOWNSTREAM AND EXISTING ALIGNMENT

CADD FILE NAME:
0114-CT-141.dgn
SHEET NO.
A4-1
21 OF 25



LEGEND	
--- (black dashed)	CL EXISTING TRACK
— (red solid)	CL PROPOSED TRACK
--- (yellow dashed)	CL TRACK REMOVAL
--- (grey dashed)	EXISTING STRUCTURE
— (green solid)	PROPOSED STRUCTURE
— (blue solid)	EDGE OF RIVER
—E— (black dashed)	OVERHEAD ELECTRIC



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PRIMARY CONSULTANT

1475 E. WOODFIELD ROAD, SUITE 600
SCHAMBURG, IL 60173
PHONE: 847-605-9600
FAX: 847-605-9610

SUB CONSULTANT

DATE	BY	DESCRIPTION
05-16-16	CIG	ISSUED FOR REVIEW

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	CIG	05-16-16	ISSUED FOR REVIEW

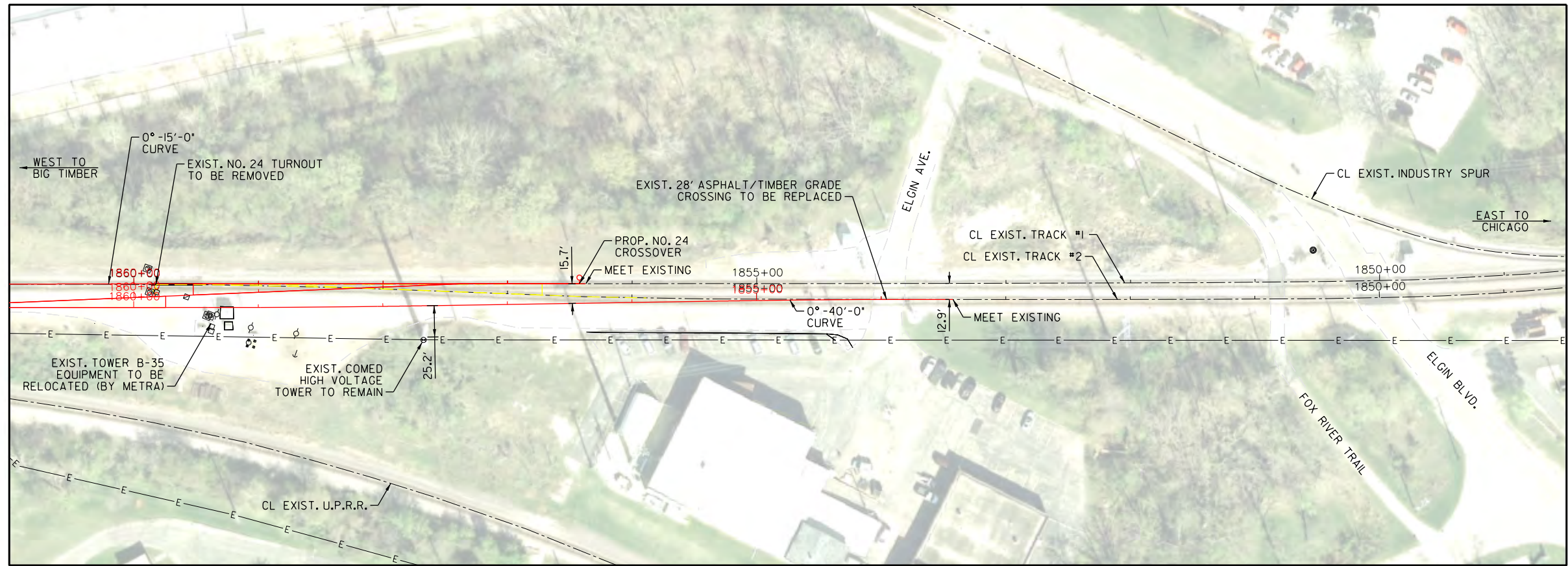
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DGN BY: CIG
DWN BY: CIG
CHK BY: BSM
METRA PROJ MGR:
CONTRACT NO.
PROJ. NO.:
SCALE: 100,000 ' / in.

ENGINEERING DEPARTMENT
547 W. JACKSON BLVD.
CHICAGO, ILLINOIS 60601

PROJECT TITLE:
**MILWAUKEE DISTRICT WEST
BRIDGE Z-100 OVER FOX RIVER**
KANE COUNTY - ELGIN, ILLINOIS

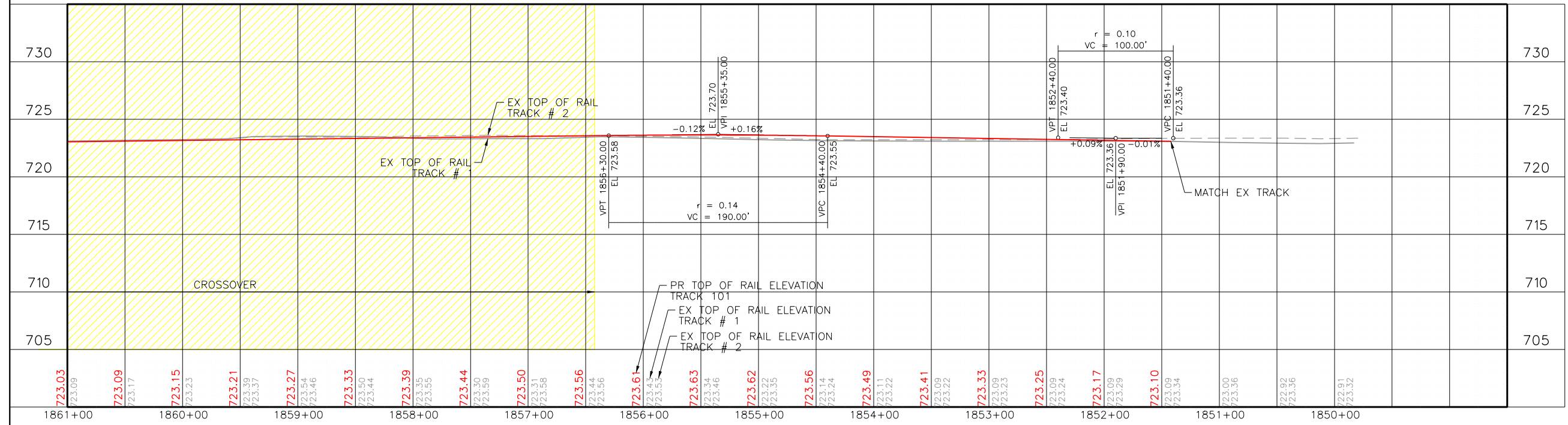
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ENVIRONMENTAL ANALYSIS EXHIBIT SET
ALTERNATIVE 4
NEW DOUBLE TRACK BRIDGE ON
DOWNSTREAM AND EXISTING ALIGNMENT

CADD FILE NAME:
0114-CT-142.dgn
SHEET NO.
A4-2
22 OF 25



LEGEND	
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— (solid red line)	CL PROPOSED TRACK
--- (dashed yellow line)	CL TRACK REMOVAL
--- (dashed grey line)	EXISTING STRUCTURE
— (solid green line)	PROPOSED STRUCTURE
— (solid blue line)	EDGE OF RIVER
—E— (line with 'E' markers)	OVERHEAD ELECTRIC

MATCH LINE
SEE DRAWING A4-2



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PRIMARY CONSULTANT

1475 E. WOODFIELD ROAD, SUITE 600
SCHALMURG, IL 60173
PHONE: 847-605-9600
FAX: 847-605-9610

SUB CONSULTANT

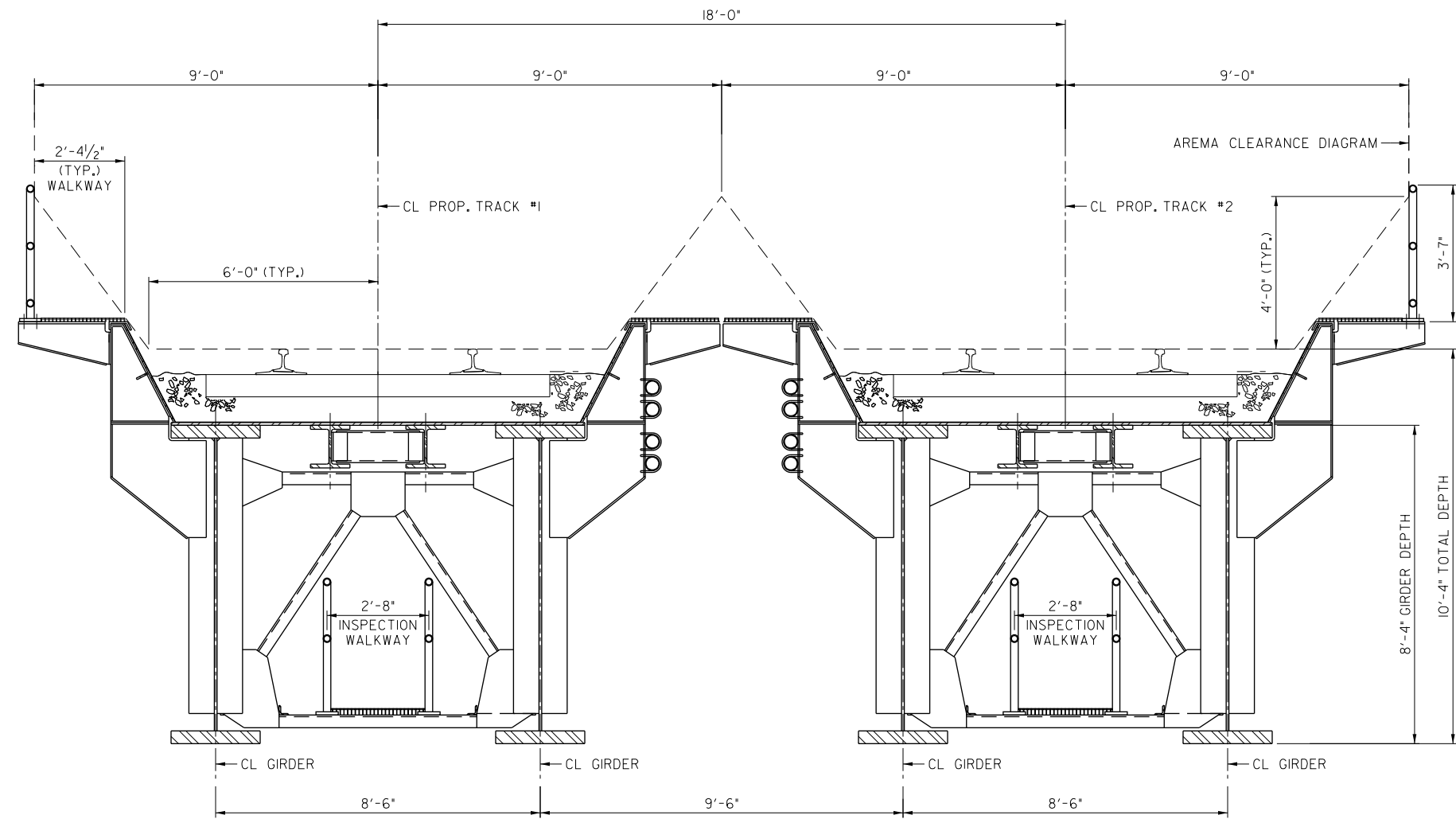
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CIG			
BSM			
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CONTRACT NO.:			
PROJ. NO.:			
SCALE: 100,000 ' / in.			

ENGINEERING DEPARTMENT
547 W. JACKSON BLVD.
CHICAGO, ILLINOIS 60601

PROJECT TITLE:
**MILWAUKEE DISTRICT WEST
BRIDGE Z-100 OVER FOX RIVER**
KANE COUNTY - ELGIN, ILLINOIS

SHEET TITLE:
ENVIRONMENTAL ANALYSIS EXHIBIT SET
ALTERNATIVE 4
NEW DOUBLE TRACK BRIDGE ON
DOWNSTREAM AND EXISTING ALIGNMENT

CADD FILE NAME:
0114-CT-143.dgn
SHEET NO.
A4-3
23 OF 25



TYPICAL CROSS SECTION
(LOOKING WEST UPSTATION)

w:\jcollett\1
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PRIMARY CONSULTANT



1475 E. WOODFIELD ROAD, SUITE 600
SCHAUMBURG, IL 60173
PHONE: 847-605-9600
FAX: 847-605-9610

SUB CONSULTANT

DATE: 05-16-16		REVISIONS	
NO.	BY	DATE	DESCRIPTION
	WJC	05-16-16	ISSUED FOR REVIEW
	MDS		



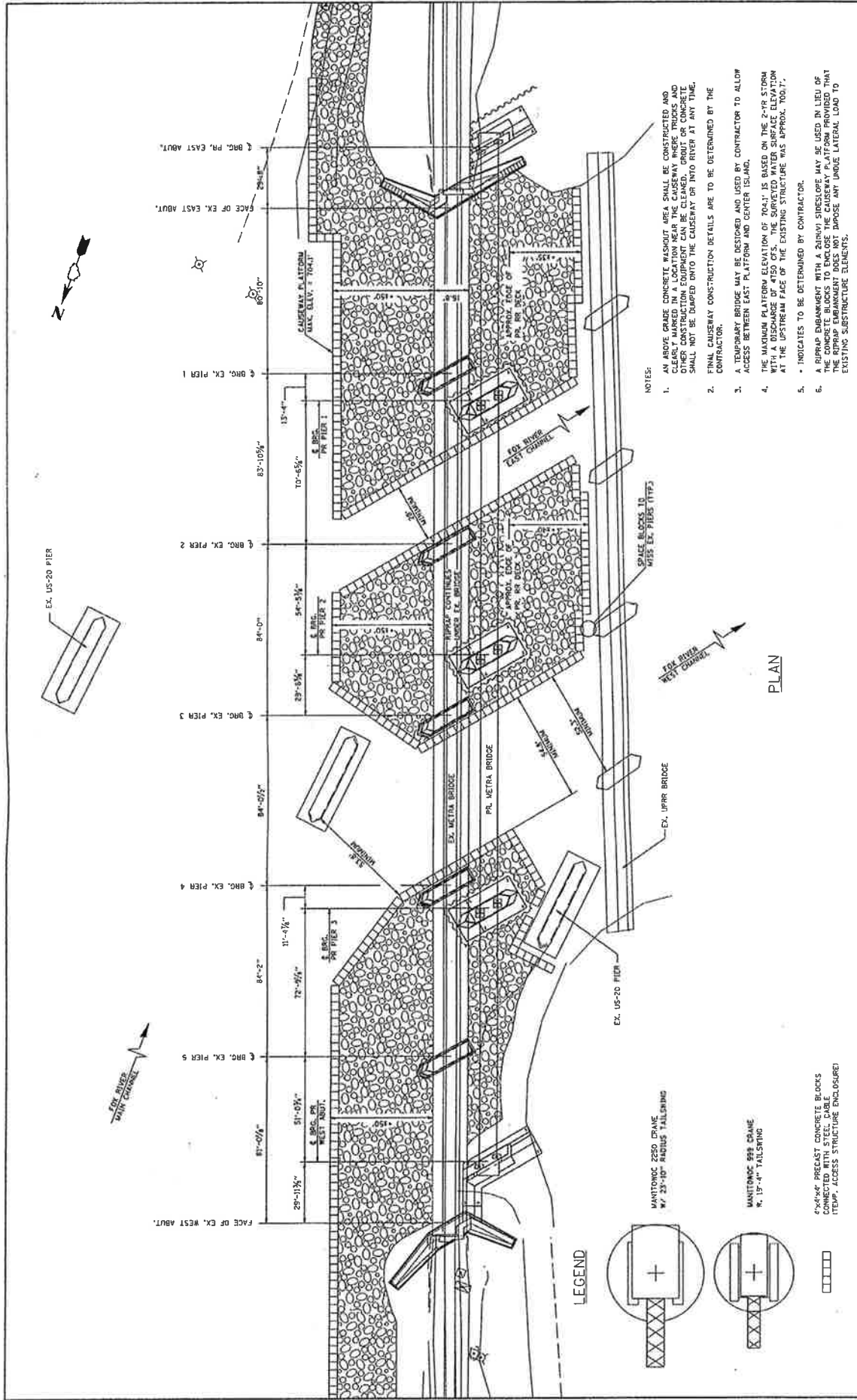
ENGINEERING DEPARTMENT
547 W. JACKSON BLVD.
CHICAGO, ILLINOIS 60601

PROJECT TITLE:
**MILWAUKEE DISTRICT WEST
BRIDGE Z-100 OVER FOX RIVER**
KANE COUNTY - ELGIN, ILLINOIS

SHEET TITLE:
ENVIRONMENTAL ANALYSIS EXHIBIT SET
ALTERNATIVE 4
NEW DOUBLE TRACK BRIDGE ON
DOWNSTREAM AND EXISTING ALIGNMENT

CADD FILE NAME:
0114-BR-402.dgn
SHEET NO.
A4-5
25 OF 25

Appendix B Temporary Causeway Plan



BRIDGE Z-100
OVER THE FOX RIVER
TEMPORARY CAUSEWAY PLAN
DATE: OCTOBER 15, 2000 SHEET NO. 11 OF 41

METRA - MILWAUKEE DISTRICT
WEST LINE
BRIDGE Z-100
ELGIN, ILLINOIS



DRAWN	SQL
DESIGN	SQL
CHECKED	F.M.L.
APPROVED	F.M.L.

NO.	DATE	REVISIONS
1	10/15/00	Draft 2001 Plan Submittal

LINE ENGINEERING LTD.
Consulting Engineers
Chicago, Illinois
210 W. Chicago
CITY 606-468
TEL 312-634-1400 FAX 312-634-1705
www.line.com 3/25/00 10:55:38 PM

- NOTES:**
1. AN ABOVE GRADE CONCRETE WASHOUT AREA SHALL BE CONSTRUCTED AND MAINTAINED THROUGHOUT CONSTRUCTION. ALL EXCESS CONCRETE AND OTHER CONSTRUCTION EQUIPMENT CAN BE CLEANED, BROUT OR CONCRETE SHALL NOT BE DUMPED ONTO THE CAUSEWAY OR INTO RIVER AT ANY TIME.
 2. FINAL CAUSEWAY CONSTRUCTION DETAILS ARE TO BE DETERMINED BY THE CONTRACTOR.
 3. A TEMPORARY BRIDGE MAY BE DESIGNED AND USED BY CONTRACTOR TO ALLOW ACCESS BETWEEN EAST PLATFORM AND CENTER ISLAND.
 4. THE MAXIMUM PLATFORM ELEVATION OF 704.1' IS BASED ON THE 2-YR STORM WITH A DISCHARGE OF 4150 CFS. THE SURVEYED WATER SURFACE ELEVATION AT THE UPSTREAM FACE OF THE EXISTING STRUCTURE WAS APPROX. 703.1'.
 5. * INDICATES TO BE DETERMINED BY CONTRACTOR.

6. A RIPRAP EMBANKMENT WITH A 2:1(S:H) SLOPES MAY BE USED IN LIEU OF THE CONCRETE BLOCKS TO ENCLOSE THE CAUSEWAY PLATFORM PROVIDED THAT THE RIPRAP EMBANKMENT DOES NOT IMPOSE ANY UNDOE LATERAL LOAD TO EXISTING SUBSTRUCTURE ELEMENTS.

Figure K

Appendix C Agency Coordination



Office of the Mayor

David J. Kaptain
MAYOR

150 DEXTER COURT
ELGIN, ILLINOIS 60120

847/931-5595
mayor@cityofelgin.org

January 26, 2017

Bruce M. Marcheschi
Chief Engineering Officer
Metra
547 W. Jackson Boulevard
Chicago, IL 60661

Dear Mr. Marcheschi:

To follow up on Elgin's previous letters of support between 2011 and 2015, the City of Elgin would like to express its continued support for Metra's MD-W Fox River Bridge Improvement Project.

At a meeting between City of Elgin staff, Metra staff, and the project's design team on January 20, 2017, the replacement of the existing single track bridge with a new double track bridge was discussed. As the century-old bridge is nearing the end of its useful life, its replacement is critical to passengers boarding at Elgin's three Metra stations, who depend on the bridge to connect them with employment, education, and recreational opportunities along the MD-W corridor, including downtown Chicago.

The MD-W Fox River Bridge Improvement Project supports Elgin's efforts to revitalize our downtown and improve mobility for our residents. In addition, the new bridge will have an improved appearance, and the reduced number of concrete piers will benefit recreational users of the Fox River. Elgin agrees to host the public meeting for the NEPA process and we look forward to the construction of this new bridge and the project's benefits.

Please contact me if you have questions or need additional information.

Sincerely,

David J. Kaptain
Mayor, City of Elgin



BUILDING AMERICA®

September 23, 2016

Mr. Bruce Marcheschi
Chief Engineering Officer
Metra
547 W. Jackson Blvd.

Re: Elgin Fox River Bridge #Z100

Dear Mr. Marcheschi:

Union Pacific (UP) is aware that Metra is currently planning to rehabilitate or replace *Metra's Elgin Fox River Bridge* on Metra's Milwaukee District West Line. As you know, *Metra's Elgin Fox River Bridge* is next to the *Union Pacific Elgin Fox River Bridge* on UP's West Chicago to Rockford Line on the Belvidere Subdivision.


Union Pacific appreciates your continued communication regarding plans for the bridge and will continue to work with Metra through UP's standard review and approval procedures as plans progress.

Union Pacific understands that Metra's project may require a temporary construction easement on a small amount of UP property. In addition, UP understands it is possible that a permanent easement for a small amount of land may also be required.

Union Pacific is willing to discuss these temporary and permanent land easements with Metra as they relate to UP's standards and approval procedures.

If you have any questions, please don't hesitate to contact me.

Sincerely,



Liisa Lawson Stark
Assistant Vice President – Public Affairs

CC: Mr. Glen Peters, Metra
Mr. Nate Morriss, Metra
Mr. Andrew Roth, Metra

August 22, 2016

Mr. Shawn Cirton
U.S. Fish and Wildlife Service
250 S Grove Ave #103
Barrington, IL 60010

Re: **Federal Transit Administration (FTA)
Range-wide Programmatic Informal Consultation for Indiana Bat and
Northern Long-eared Bat
METRA Milwaukee District Westline
Elgin, Kane County, Illinois
42.020042°lat./ -88.274656°long.**

Dear Mr. Cirton:

Metra is proposing the replacement of the Milwaukee District Westline (MDW) Fox River Bridge (Z-100) with a completely new structure, expanded to accommodate two tracks and controlled by a modern, PTC-compatible signal system in the City of Elgin, Kane County, Illinois. Existing land use adjacent to the project area includes transportation (Union Pacific Railroad), recreational, commercial, industrial, and undeveloped land. All work will be conducted within or immediately adjacent (less than 100 feet) to the existing track and Metra right-of-way. The project is located within the Fox River Watershed (HUC Code 07120007).

A Section 7 Consultation and endangered species review for the proposed project was completed by Huff & Huff, Inc. (H&H). On May 16, 2011 a NEPA project notification letter was sent to Louise Clemency of USFWS stating that the project will not affect critical habitat for the eastern prairie fringed orchid as suitable habitat is not present. Since 2011, a website consultation was conducted on November 9, 2015 to identify whether additional threatened and endangered species or critical habitat were identified. The Northern long-eared bat (Threatened – *Myotis septentrionalis*) was identified as known within Kane County. Suitable roosting habitat for the Northern long-eared bat is present within the project limits.

As this project is partially funded by the Federal Transit Administration (FTA), this project is subject to the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), FTA, and FWS informal programmatic consultation agreement for the NLEB. The scoping worksheet, project submittal form, and bridge inspection form is attached as required for consultation under the informal programmatic consultation agreement.

Mr. Shawn Cirton
Page 2 of 2
August 22, 2016

In summary, Huff & Huff, Inc. (H&H) has determined, for the Indiana Bat and Northern Long-eared Bat that the project will have no effect on the species based on the lack of suitable habitat and no evidence of bats on the bridge.

At this time Metra is requesting FWS review of the project and concurrence with the above finding within 14 days.

Sincerely,

A handwritten signature in blue ink, appearing to read "D. Simmons", with a long horizontal flourish extending to the right.

David F. Simmons
Director, Grant Administration

Enclosures

**Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and
Federal Transit Administration (FTA)
Range-wide Programmatic Informal Consultation for
Indiana Bat and Northern Long-eared Bat**

Project Submittal Form for FHWA, FRA, FTA, and
Transportation Agencies *Updated February 2016*

In order to use the programmatic informal consultation to fulfill Endangered Species Act consultation requirements, transportation agencies must use this submittal form to submit project-level information for all may affect, not likely to adversely affect (NLAA) determinations to the appropriate U.S. Fish and Wildlife Service (Service) field office prior to project commencement. For more information, see the Standard Operating Procedure for Site Specific Project(s) Submission in the User's Guide.

In submitting this form, the transportation agency ensures that the proposed project(s) adhere to the criteria of the range-wide programmatic informal BA. Upon submittal of this form, the appropriate Service field office may review the site-specific information provided and request additional information. If the applying transportation agency is not notified within 14 calendar days of emailing the Project Submittal Form to the Service field office, it may proceed under the range-wide programmatic informal consultation.

Further instructions on completing the submittal form can be found by hovering your cursor over each text box.

1. Date:

2. Lead Agency:

This refers to the Federal governmental lead action agency initiating consultation; select FHWA or FRA as appropriate

3. Requesting Agency:

a. Name:

b. Title:

c. Phone:

d. Email:

4. Consultation Code¹:

5. Project Name(s):

¹ Available through IPaC System Official Species List: <https://ecos.fws.gov/ipac/>

6. Project Description:

Please attach additional documentation or explanatory text if necessary

7. Other species from Official Species List:

No effect – project(s) are inside the range, but no suitable habitat – see additional information attached

May Affect – see additional information provided for those species (either attached or forthcoming)

8. For Ibat/NLEB, if Applicable, Explain Your No Effect Determination

No effect – project(s) are outside the species' range (submittal form complete)

No effect – project(s) are inside the range but no suitable summer habitat (submittal form complete)

No effect – project(s) are completely within existing road/rail surface and do not involve percussive or other activities that increase noise above existing traffic/background levels (submittal form complete)

No effect – project(s) includes maintenance, alteration, or demolition of bridge(s)/structure(s) and indicate(s) no signs of bats from results of a bridge/structure assessment (submittal form complete)

No effect – project(s) do not involve construction activities (e.g., bridge assessments, property inspections, development of planning and technical studies, property sales, property easements, and equipment purchases) (submittal form complete)

Otherwise, please continue below.

9. For Ibat/NLEB, if Applicable, Explain Your May Affect, Not Likely to Adversely Affect Determination (without implementation of AMMs)

NLAA – project(s) are inside the range but negative bat presence/absence (P/A) surveys (submittal form complete)

NLAA – project(s) conducted completely within existing road/rail surface and involve percussive activities (submittal form complete)

NLAA – project(s) are within areas that contain suitable forested habitat but do not remove or alter trees (e.g., landscaping rest areas, mowing, brush removal, sign or guiderail replacement, and stormwater management) (submittal form complete)

NLAA – project(s) of slash pile burning (submittal form complete)

NLAA – wetland or stream protection activities are associated with wetland mitigation and do not clear suitable habitat (submittal form complete)

Otherwise, please continue below.

For Ibat/NLEB, if applicable, continue to complete the submittal form to explain your may affect, not likely to adversely affect determination (**with implementation of AMMs**)

10. Affected Resource/Habitat Type

Trees

Bridge

Other Non-Tree Roosting Structure (e.g., building)

Other (please explain):

11. For Tree Removal Projects:

- a. Please verify that no documented roosts or foraging habitat will be impacted and that project is within 100 feet of existing road surface:
- b. Please verify that all tree removal will occur during the inactive season²:
- c. Timing of clearing:
- d. Amount of clearing:

² Coordinate with local Service field office for appropriate dates.

12. For Bridge/Structure Work Projects:

- a. Proposed work:
- b. Timing of work:
- c. Evidence of bat activity on bridge/structure:
- d. If applicable, verify that superstructure work will not bother roosting bats in any way:
- e. If applicable, verify that bridge/structure work will occur only in the winter months:

13. Please confirm the following:

Proposed project(s) adhere to the criteria of the range-wide programmatic informal BA (see Section 2.0).

All applicable AMMs will be implemented, including³:

Tree Removal AMM 1:

Tree Removal AMM 2:

Tree Removal AMM 3:

Tree Removal AMM 4:

Bridge AMM 1:

Bridge AMM 2:

Bridge AMM 3:

Bridge AMM 4:

Structure AMM 1:

Structure AMM 2:

Structure AMM 3:

Structure AMM 4:

Lighting AMM 1:

Lighting AMM 2:

³ See AMMs Fact Sheet (Appendix C) for more information on the following AMMs.

SCOPING WORKSHEET

INDIANA BAT AND NORTHERN LONG-EARED BAT RANGE-WIDE PROGRAMMATIC INFORMAL CONSULTATION

Complete the following steps to determine whether a project is within the scope of the range-wide programmatic informal consultation and to identify potential project effects on either the Indiana bat or Northern long-eared bat. The following information is needed to complete this form: project scope (including any construction methods to be used), project location, habitat characterization, completed survey results, and Avoidance and Minimization Measures (AMMs) to be included in the project.

STEP 1: PROGRAMMATIC SCOPE (Users Guide p. 3)

If answers to any of these questions are “yes”, the project is NOT covered by the range-wide programmatic informal consultation. Proceed no further in completing this worksheet. Separate consultation with the appropriate Service field office is necessary. If answers to all of the questions are “no”, proceed with Step 2 of this Worksheet.

	Yes	No
1. Will the project construct a new road corridor (new alignment, not minor realignments)?		X
2. Will project activities impact suitable forest habitat for bats > 100 feet from existing road/rail surfaces <i>at any time of year</i> (unless summer bat Presence/Probable Absence (P/A) surveys are negative)?		X
3. Will the project raise the road profile above the tree canopy within 1,000 feet of known summer habitat (based on documented roosts and/or captures)?		X
4. Is the project within 0.5 mile of hibernacula (including Indiana bat critical habitat) and 1) include construction activities extending outside the existing road/rail surface or 2) include construction activities wholly within the existing road/rail service but include percussive or other activities that increase noise above existing traffic/background levels?		X
5. Will the project clear suitable forest habitat at any distance from a road during the active season ¹ for bats (unless summer bat P/A surveys are negative)?		X
6. Will the project remove documented roosts or foraging areas/travel corridors (based on radio telemetry) <i>at any time of year</i> or remove trees within 0.25 miles of documented roosts <i>at any time of year</i> ?		X
7. Bridge Projects <i>at any time of year</i> : (a) Will the project remove a bridge with bat colonies known to be roosting under the bridge? (b) Will the project modify a bridge with bat colonies known to be roosting under the bridge so that it is no longer suitable for roosting?		X
8. Will bridge or structure maintenance activities likely disturb bats while bats are documented to be present?		X

STEP 2: POTENTIAL PROJECT EFFECTS

No Effect (NE) (User’s Guide p. 4)

If answers to any of the criteria below are “yes” the project will have “No Effect” on the Indiana bat and/or NLEB. Stop here. Document “no effect” on the Project Submittal Form (Appendix B of the User Guide) and retain for your files. No coordination with the Service is required. If answers to any of the criteria below are “no”, proceed with this Worksheet.

Check “NA” if the project will not involve the listed activity or condition.

	Yes	No	N/A
1. Is the project(s) outside the species range, based on USFWS IPaC database?		X	

¹ Coordinate with the local Service field office for active season dates.

2. Is the project inside the range and outside 0.5 mile of hibernacula, but no suitable summer habitat is present (e.g., high-density urban area or non-forested areas)?		X	
3. Are all project activities (anywhere, including within 0.5 miles of hibernacula) conducted completely within the existing road/rail surface and <u>do not involve</u> percussive or other activities that increase noise above existing traffic/background levels, such as blasting, use of pile drivers, rock drills, or hoe rams?		X	
4. Does the project involve maintenance, alteration, or demolition of bridge/structures and <u>the results of a bridge assessment indicate no signs of bats?</u>	X		
5. Does the project consist of non-construction activities (e.g., bridge assessment, property inspections, property sales, property easements, and equipment purchases)?		X	

May Affect (MA) (User’s Guide page 4)

If the answer to each of the criteria below is “true”, assume the presence of Indiana bat and/or NLEB. Proceed with this Worksheet.

	True	False
1. Project is in range of species, and		
2. Suitable habitat is present (for foraging, roosting, traveling, hibernating, swarming, nursing or other bat activities), and		
3. No bat surveys have been conducted or surveys are positive for presence of Indiana bat or NLEB.		

If the answers to any of the criteria below are “yes” the project “May Affect” the Indiana bat and/or NLEB. Proceed with Step 3 of this Worksheet.

Does the project action involve any of the following activities?	Yes	No	Unknown
1. Tree removal within suitable habitat			
2. Percussive activities that will increase noise above existing traffic/background levels (e.g., blasting, use of pile drivers, rock drills, or hoe rams)			
3. Increased lighting, either temporary or permanent (e.g., construction lighting or permanent lighting installation as part of project)			
4. Smoke/heat associated with burning brush piles			
5. Impacts to water bodies/wetlands where suitable bat habitat is present (e.g., piping a section of stream)			
6. Bridge or structure maintenance, repair or replacement at sites with bat activity			

STEP 3: AVOIDANCE AND MINIMIZATION MEASURES (User’s Guide page 5-6)

The next sets of questions will step through the process for determining whether a project “May Affect, but is Not Likely to Adversely Affect” the Indiana bat and/or NLEB. Avoidance and Minimization Measures (AMM’s) may be required.

May Affect, Not Likely to Adversely Affect (NLAA)

If answers to any of the questions below are “Yes”, the project “May Affect, but is Not Likely to Adversely Affect” the Indiana bat and/or NLEB, and IS covered by the range-wide programmatic informal consultation. AMM’s are not required for these activities. Document on the Project Submittal Form (Appendix B of the User Guide). If answers to any of these questions are “No” or “Unknown”, proceed with this worksheet.

Do any of the conditions below describe the project?	Yes	No	Unknown
1. Project is inside the range and in or near suitable habitat, but			

with negative bat P/A surveys. *If no bat surveys have been performed check "no" - presence of bats is to be assumed and AMM's will be required.			
2. Work activities will be conducted completely within the existing road/rail surface and <u>involve</u> percussive activities such as blasting and use of pile drivers, rock drills, or hoe rams.			
3. Work activities will take place in areas that contain suitable forested habitat, but no tree removal or habitat alteration will occur (e.g., landscaping rest areas, mowing, brush removal, sign or guardrail replacement, storm water management).			
4. No slash pile burning will occur.			
5. Wetland or stream protection activities associated with mitigation that do not clear suitable habitat.			

May Affect, Not Likely to Adversely Affect - AMMs Required

For the actions below, site-specific AMM(s) may be required to make the project NLAA for either bat species. If there is an applicable AMM, it MUST be implemented for the project to be eligible for use within the range-wide programmatic informal consultation. If an AMM listed below is not applicable (based on the type of action/effect), document why it is not applicable. For some projects, additional project-specific AMM(s) not listed below may be needed. If such additional AMM(s) are implemented, document them.

	Yes	No
TREE REMOVAL		
Will the project remove trees that are suitable maternity, roosting, foraging, or traveling habitat for Indiana Bat or NLEB? <i>If "No", proceed to next activity.</i>		
1. Will tree removal <i>at any time of year</i> occur <u>entirely</u> within 100 feet of existing road surface? (Note: If "no", this action is not covered under the range-wide programmatic Informal consultation. Proceed no further with worksheet. Separate consultation with the appropriate Service field office is necessary.)		
2. Will documented roosts or foraging habitat (based on radio telemetry) be removed <i>at any time of year</i> ? (Note: If "yes", this action is not covered under the range-wide programmatic informal consultation. Proceed no further with worksheet. Separate consultation with the appropriate Service field office is necessary.)		
3. Will trees be removed within 0.25 miles of documented roosts <i>at any time of year</i> ? (Note: If "yes", this action is not covered under the range-wide programmatic informal consultation. Proceed no further with worksheet. Separate consultation with the appropriate Service field office is necessary.)		
Unless current surveys document that the species are not present, all of the AMMs listed below will be applied, unless not relevant (e.g., no bridge work will occur). Indicate on the project submittal form which of the following tree removal AMMs will be implemented.		
TREE REMOVAL AMM 1: Modify all phases/aspects of project (e.g. temporary work areas, alignments) to avoid tree removal in excess of what is required to implement project safely. (Note: If this cannot be applied, project can still be MANLAA as long as removal is in winter and avoids known roosts.)		
TREE REMOVAL AMM 2: Apply time of year restrictions for tree removal when bats are not likely to be present.		
TREE REMOVAL AMM 3: Ensure tree removal is limited to that specified in project plans. Install bright orange flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits. Ensure that contractors understand the clearing limits and how they are marked in the field.		

TREE REMOVAL AMM 4: Avoid cutting down documented bat roosts that are still suitable for roosting or documented foraging habitat <i>at any time of year</i> . Avoid cutting down trees within 0.25 miles of documented roosts <i>at any time of year</i> . Ensure that suitable roosts remain on the landscape rather than focusing on general forest loss.		
--	--	--

*Note: "Trees" refers to trees that are suitable habitat for each species.

LIGHTING	Yes	No
1. Will the project involve the use of lighting during construction? <i>If "No", proceed to next activity.</i>		
2. Will the project action install permanent lighting? <i>If "No", proceed to next activity.</i>		
If the answer to either of above is "yes", indicate on the project submittal form which lighting AMM's will be implemented.		
LIGHTING AMM 1: Direct temporary lighting away from suitable habitat during construction.		
LIGHTING AMM 2: Use downward-facing, full cut-off lens lights, and direct lighting away from suitable habitat when installing new or replacing existing permanent lights.		

BRIDGE MAINTENANCE, ALTERATION OR REMOVAL	Yes	No
Does the project involve bridge maintenance, removal or other alteration? <i>If "No", proceed to next activity.</i>		
Unless current surveys or inspections document that the species are not present, the AMMs listed below will be applied, as appropriate. Indicate on the project submittal form which of the following AMMs will be implemented.		
BRIDGE AMM 1: Perform any bridge repair, retrofit, maintenance, and/or rehabilitation work outside of the active season. ²		
BRIDGE AMM 2: Bridge repair, retrofit, maintenance, and/or rehabilitation work outside of pup season (June 1 – July 31) will occur in the evening while the bats are feeding, starting one hour after sunset, and ending one hour before daylight excluding the hours between 10 pm and midnight. Lighting must be kept localized (See lighting AMM).		
BRIDGE AMM 3: If bridge repair, retrofit, maintenance, and/or rehabilitation work alters the bridge during the inactive season, then ensure suitable roosting sites remain after any bridge work. Suitable roosting sites may be incorporated into the design of a new bridge.		

STRUCTURE (ARTIFICIAL ROOSTS) MAINTENANCE, ALTERATION OR REMOVAL	Yes	No
Does the project involving any artificial roost such as a building, barn, shed, mobile home, telephone poles or other structure?		
Unless current surveys or inspections document that the species are not present, the AMMs listed below will be applied, as appropriate. Indicate on the project submittal form which of the following AMMs will be implemented.		
STRUCTURE AMM 1: If the goal of the project is to exclude bats, coordinate with the local Service field office.		
STRUCTURE AMM 2: Perform any maintenance and/or repair work outside of the active season.		
STRUCTURE AMM 3: If maintenance and/or repair work will be performed during the		

² Coordinate with the local Service field office for active season dates.

APPENDIX D: Bridge/Structure Assessment Form

Bridge Assessment Form

This form will be completed and submitted to the District Environmental Manager by the Contractor prior to conducting any work below the deck surface either from the underside, from activities above that bore down to the underside, or that could impact expansion joints, from deck removal on bridges, or from structure demolish. Each bridge/structure to be worked on must have a current bridge inspection. Any bridge/structure suspected of providing habitat for any species of bat will be removed from work schedules until such time that the DOT has obtained clearance from the US Fish and Wildlife Service, if required. Additional studies may be undertaken by the DOT to determine what species may be utilizing structures prior to allowing any work to proceed.

DOT Project #	Water Body	Date/Time of Inspection
---------------	------------	-------------------------

Route:	County:	Federal Structure ID:	Bat Indicators				Notes: (e.g., number & species of bats, if known. Include the results of thermal, emergent, or presence/absence summer survey)
Check all that apply. Presence of one or more indicators is sufficient evidence that bats may be using the structure.							
			Visual	Sound	Droppings	Staining	

Areas Inspected (Check all that apply)

Bridges		Culverts/Other Structures		Summary Info (circle all that apply)			
All vertical crevices sealed at the top and 0.5-1.25" wide & ≥4" deep		Crevices, rough surfaces or imperfections in concrete		Human disturbance or traffic under bridge/in culvert or at the structure	High	Low	None
All crevices >12" deep & not sealed		Spaces between walls, ceiling joists		Possible corridors for netting	None/poor	Marginal	excellent

All guardrails				Evidence of bats using bird nests, if present?	Yes	No	
All expansion joints							
Spaces between concrete end walls and the bridge deck							
Vertical surfaces on concrete I-beams							

Assessment Conducted By: _____ _____	Signature(s): _____
District Environmental Use Only:	Date Received by District Environmental Manager: _____

DOT Bat Assessment Form Instructions

1. Assessments must be completed a minimum of 1 year prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Informal Consultation, regardless of whether assessments have been conducted in the past. **Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that structure in subsequent years.**
2. Legible copies of this document must be provided to the District Environmental Manager within two (2) business days of completing the assessment. Failure to submit this information will result in that structure being removed from the planned work schedule.
3. Any bridge/structure suspected of providing habitat for any species of bat will be removed from work schedules until such time that the DOT has obtained clearance from the USFWS, if required. Additional studies may be undertaken by the DOT to determine what species may be utilizing each structure identified as supporting bats prior to allowing any work to proceed.
4. Estimates of numbers of bats observed should be place in the Notes column.
5. Any questions should be directed to the District Environmental Manager.

Photographic Log of the METRA Milwaukee District Westline Fox River Bridge (Z100)
City of Elgin, Kane County, Illinois
May 16, 2016



Photo 1: Facing southeast towards the METRA Milwaukee District Westline (MDW) Fox River Bridge.



Photo 2: Facing northwest towards the METRA MDW Fox River Bridge.



Photo 3: Facing southeast towards the METRA MDW Fox River Bridge.



Photo 4: Representative photograph from under the METRA MDW Fox River Bridge.

Photographic Log of the METRA Milwaukee District Westline Fox River Bridge (Z100)
City of Elgin, Kane County, Illinois
May 16, 2016



Photo 5: Representative photograph from under the METRA MDW Fox River Bridge.



Photo 6: Facing west towards the Metra UP West line, east of 25th Avenue.



Photo 7: Representative photograph from under the U.S. Route 20 Bridge over the Fox River.



Photo 8: Representative photograph from under the U.S. Route 20 Bridge over the Fox River.



Project Name: Milwaukee West Line Fox River Bridge Improvement Project (Metra Bridge Z-100)
Project #: GA4694 & GX4704 **Contract #:** K52919 **Task #:** 1



Date	Start	End	Next Meeting	Next Time	Prepared By	Company
6/29/2016	2:30 pm	3:00 pm			Grace Dysico	TranSystems

Purpose	Location	Next Location
Review and discuss Public Meeting strategy, tasks & schedule	Metra, 5 th Flr. East Engr. CR	

Attended By	Non-Attendees
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Kate Sullivan, Metra
 Brian Stepp, Metra
 Andy Roth, Metra
 Kathy Chernich, USACE
 Melyssa Navis, USACE
 Grace Dysico, TranSystems
 Jim Novak, Huff & Huff

Discussion Notes

USACE Project Number: LRC-2016-433

The purpose of the conference call meeting was to review the project with the US Army Corps of Engineers (USACE) and provide an update. The last meeting with USACE was on August 29, 2014.

A description of the existing bridge and adjacent area was provided. The project involves the Milwaukee District West Line Bridge Z-100 over the Fox River. The project is within the City of Elgin, Kane County. The existing bridge is a six-span structure, with five masonry piers. It carries one mainline track, but has double tracks on the approaches to the bridge. Figures 1-1 thru 1-3, location maps from the Draft Environmental Assessment (EA) were provided to USACE in advance of the meeting. The bridge is located east of the Elgin National Street station. It is also parallel to the Union Pacific (UP) Railroad bridge. The UP structure is approximately 50 feet downstream of the Z100 bridge. US Route 20 crosses overhead over both Metra and the UP bridges.

Metra has received TIGER Program funds for construction and is preparing an Environmental Assessment to comply with the FTA and NEPA requirements.

The Purpose and Need of the project is primarily to address the deteriorated condition of the bridge. Metra would like to bring the bridge condition to a state of good repair. The proposed improvements will eliminate a bottle neck at the river crossing. This will improve operational efficiencies and reduce delays and travel times along the Milwaukee District West Line.

Four build alternatives have been developed and considered by Metra Engineering. These four build alternatives and the No Build alternative are described and discussed in the Draft EA. Schematics of the proposed build alternatives were provide to USACE in advance of the meeting. Figures 2-1 thru 2-4 were provided as well as Table 2-1, Alternatives Comparison Matrix. The preferred alternative is Alternative 4 and proposes to construct a new bridge downstream of the existing bridge after which the existing bridge will be removed and replaced, providing a new double track crossing of the Fox River. The new bridge will have four spans and three piers. To construct the bridge, it is anticipated that Metra will use a temporary causeway around the new pier construction and removal of the existing piers.



The draft EA was submitted to the FTA for review on 6/10/16. Their review comments are anticipated in mid-July.

USACE asked when they anticipate submittal of the 404 permit. Metra indicated the permit could be submitted sometime in early spring. They are currently procuring design services and working towards getting the design engineering started.

USACE asked that they be provided a link to upload the permit when it is ready for submittal. Also, the assigned project number should be included in all correspondence, submittals and inquiries.

USACE would not provide any indication of the permitability of Alternative 4 as the preferred alternative, but they did comment that it appears that Metra is on track with project with submittal of the draft EA. They will review and comment on the permit and the project when the permit documents are submitted.

The meeting adjourned at 3:00 pm.

Any comments, additions, or corrections shall be made, in writing, within five (5) business days of the issue date of these minutes. If no comments, additions, or corrections are received within the five (5) business days period, these minutes shall be deemed approved and shall be binding on all parties.

OCT 22 2015

PRESERVATION SERVICES

IHPA REVIEW
H/A _____
AC _____
AR _____
File _____



U.S. Department
of Transportation
Federal Transit
Administration

REGION V
Illinois, Indiana,
Michigan, Minnesota,
Ohio, Wisconsin

200 West Adams Street
Suite 320
Chicago, IL 60606-5253
312-353-2789
312-886-0351 (fax)

October 14, 2015

Rachel Leibowitz, Ph.D.
Deputy State Historic Preservation Officer
Illinois Historic Preservation Agency
1 Old State Capitol Plaza
Springfield, IL 62701

RE: FTA Supplemental Section 106 Historic Review Initiation/Determination: Metra
Milwaukee District West Line Fox River Bridge Project, Kane County, City of Elgin,
Illinois - IHPA Log #010082310

Dear Ms. Leibowitz:

As part of its responsibilities under 36 CFR § 800 – Protection of Historic Properties and the National Historic Preservation Act (NHPA), the Federal Transit Administration (FTA) is reinitiating the Section 106 Consultation Process for the proposed Metra Milwaukee District West Line (MD-W) Fox River Bridge Project (proposed Project) in Kane County, City of Elgin, Illinois. FTA is also providing the associated Area of Potential Effects (APE), eligibility and effects determinations. FTA is taking this action due in part to the unusual length of time that has passed since consultation was initiated for the proposed Project with the Illinois Historic Preservation Agency (IHPA) in 2010.

The purpose of the proposed UP-W Fox River Bridge Project is to replace the existing single-track railroad bridge, which was originally constructed in 1881 and is nearing the end of its useful life. The existing bridge is about 500 feet long and consists of six steel spans resting on the original masonry abutments and piers. Three of the original spans were replaced in 1905 and the other three were replaced in 1926. The new bridge will be double-tracked, within and adjacent to the footprint of the existing structure, and will relieve an existing bottleneck that delays railroad traffic. Existing bridge piers will be removed and replaced while the current abutments will be partially removed and altered to accommodate the new structure. Double-track will be realigned / constructed on land about 500 feet northward and 650 feet southward from the bridge. FTA has determined that the proposed Project will be a Federal undertaking as defined in §800.16(y) and that it is a type of activity that has the potential to cause effects on historic properties.

The Section 106 consultation process consists of four steps, all of which are completed in consultation with the State Historic Preservation Officer (SHPO) and/or Tribal Historic Preservation Officer (THPO), and other consulting parties.

1. FTA initiates the Section 106 process, pursuant to §800.3 – Initiation of the Section 106 process, with the SHPO (or THPO if the property is on tribal lands) and other consulting parties if any.

RE: FTA Supplemental Section 106 Historic Review Initiation/Determination: Metra Milwaukee District West Line Fox River Bridge Project, Kane County, City of Elgin, Illinois - IHPA Log #010082310

2. FTA determines the project's Area of Potential Effects (APE) and the properties within the APE that are listed, or eligible for listing, in the National Register of Historic Places (NRHP). FTA evaluates properties eligible for listing using the processes established in 36 CFR § 60 and National Register Bulletin 15. FTA's determination of the APE requires consultation with and concurrence by the SHPO. If FTA determines there are no properties within the APE that are listed, or eligible for listing, in the NRHP, or if FTA determines there are historic properties present but the project will have no effect upon them, then FTA will determine "no historic properties affected" in consultation with the SHPO and / or THPO.
3. FTA determines adverse effects with respect to historic properties within the APE. FTA's determination considers whether the project will diminish those qualities that make any of the properties eligible for listing in the NRHP. FTA makes a determination of "adverse effect" when the project will diminish these qualities in one or more properties; if not, FTA makes a determination of "no adverse effect." FTA's determination of "no adverse effect," along with concurrence by the SHPO, completes the Section 106 consultation process.
4. If FTA determines an "adverse effect," it consults with the Advisory Council on Historic Preservation (ACHP), SHPO, affected tribes, and other interested parties, as appropriate, to resolve the adverse effects on historic properties. Resolution of adverse effects may involve redesigning a project to avoid, minimize, or mitigate impacts to historic properties. Actions that the consulting parties agree upon to mitigate adverse effects are documented in a Memorandum of Agreement (MOA). Once the agreement is signed by all appropriate parties, including the SHPO and other invited signatories, and the agreement is filed with the ACHP, the Section 106 process is completed, and the FTA's responsibilities are fulfilled when the MOA's stipulations are implemented.

Lin Engineering notified IHPA of the proposed MD-W Line Fox River Bridge Project via correspondence dated August 17, 2010. In correspondence dated September 17, 2010, Huff & Huff provided IHPA with topographical and zoning maps delineating the proposed Project location in addition to various site photographs. The proposed Project location boundary is about 1,700 feet long and 190 feet wide. In a letter dated September 24, 2010, IHPA stated "Based upon the information provided, no historic properties are affected. We, therefore, have no objection to the undertaking proceeding as planned.....This clearance remains in effect for two years from date of issuance."

In correspondence dated August 5, 2011, Huff & Huff advised IHPA that Metra had changed the design of the bridge to accommodate two tracks as opposed to one. IHPA provided a concurrence letter dated August 25, 2011 that was essentially the same as the aforementioned IHPA letter.

On August 17, 2012, FTA invited the following tribes to participate in consultation and help identify places that may have traditional religious and cultural importance to them at or near the proposed Project site: Citizen Potawatomi Nation; Forest County Potawatomi Community; Hannahville Indian Community; and Prairies Band of Potawatomi Nation. FTA received one response from the Forest County Potawatomi dated September 27, 2012. This tribe requested that they be provided with results of any archival review, cultural resource investigation studies, and archaeological reports. They would like to be consulted should there be an impact or effect to cultural and historic properties as a result of the proposed Project.

RE: FTA Supplemental Section 106 Historic Review Initiation/Determination: Metra
Milwaukee District West Line Fox River Bridge Project, Kane County, City of Elgin,
Illinois - IHPA Log #010082310

Metra responded to the Forest County Potawatomi in correspondence dated November 9, 2012 and provided the aforementioned IHPA August 25, 2011 letter. Metra confirmed that the SHPO and Forest County Potawatomi would be notified should any discovery result in the requirement for any archival reviews, cultural resource investigation studies or archaeological reports.

In the enclosed correspondence dated September 3, 2015, Metra provided FTA with an updated project description, APE, copies of the aforementioned IHPA letters, and an Inadvertent Discovery Plan (IDP). Apart from the IDP, there are no substantive changes to the proposed Project. The IDP has been prepared for the proposed Project regarding potential archaeological findings and is provided to IHPA for review and comment. If archaeological deposits are encountered from the post-contact period during monitoring, they will be evaluated by a qualified professional archaeologist regarding their eligibility for listing in the NRHP in consultation with the IHPA and Forest County Potawatomi. All archaeological recording will be done in accordance with the Secretary of the Interior's "Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716) and reports of the archaeological documentation will be submitted to the IHPA and Forest County Potawatomi for review and comment.

In compliance with Section 106 of the NHPA, and in accordance with the procedures related to the identification of historic properties described in the implementing regulations at 36 CFR § 800, based on the aforementioned documentation, FTA has determined the following for the proposed MD-W Fox River Bridge Project: the APE is the boundary as delineated on the enclosed aerial map; there are no properties on or eligible for the NRHP within the APE; and the Project would result in **no historic properties affected**. Pursuant to 36 CFR § 800, FTA is seeking IHPA concurrence with the aforementioned APE and eligibility/effects determinations within 30 days of receipt of this letter.

If FTA can provide any assistance or additional information which would aid in your prompt reply, please feel free to contact Reggie Arkell at 312-886-3704 or reginald.arkell@dot.gov. Thank you for your assistance.

Sincerely,



Marisol R. Simón
Regional Administrator

CC: Tom Weaver, Metra

Enclosures: Metra correspondence dated September 3, 2015 and attachments

CONCUR

By: R. Leitowitz
Deputy State Historic Preservation Officer

Date: 10/20/15 SJH



Office of the Mayor

David J. Kaptain
MAYOR

150 DEXTER COURT
ELGIN, ILLINOIS 60120

847/931-5595
mayor@cityofelgin.org

April 27, 2015

The Honorable Anthony Foxx
Secretary
United States Department of Transportation
1200 New Jersey Avenue, SE
Washington, D.C. 20590

Dear Secretary Foxx:

The City of Elgin would like to express its support for Metra's application for funding from the FY2015 TIGER program for the MD-W Fox River Bridge Improvement Project.

Elgin is committed to promoting compact, mixed-use development in our central area, continuing to attract residents and businesses to our historic downtown and enhancing pedestrian, bicycle and transit mobility throughout our community. The City completed the second phase of our Sustainability Action Plan in 2011, and a sidewalk gap and transit stop study to recommend specific pedestrian and transit access improvements was completed in January 2014. Elgin completed a transit-oriented development plan for the area near the Metra National Street Station in 2011, and a planning study of the Chicago Street Station area is nearing completion. The City has also made significant investments in a number of projects—including a riverfront park, riverwalk and extensive Central Business District streetscaping project—designed to transform our city center.

The MD-W Fox River Bridge Improvement Project supports Elgin's efforts to revitalize our downtown and improve mobility for our residents. Riders boarding at Elgin's three Metra stations depend on the bridge to connect them with employment, education and recreational opportunities along the MD-W corridor and in downtown Chicago. Reconstruction and expansion of the bridge will increase the attractiveness of commuter rail as an alternative to automobile use and preserve reliable connections to Pace service. In addition, the new bridge will have an improved appearance, and the reduced number of concrete piers will benefit recreational users of the Fox River.

Your consideration of Metra's grant application is appreciated. Please contact me if you have questions or need additional information.

Sincerely,

David J. Kaptain
Mayor, City of Elgin



Project Name: Fox River Bridge

Project #:

Contract #:

Task #: Environmental Assessment

Progress Meeting ##

Date	Start	End	Next Meeting	Next Time	Prepared By	Company
8/29/2014	3:00 pm	3:30 pm			Rich Ray	Huff & Huff
Purpose					Location	Next Location

IDNR Coordination

Conference Call

Attended By

Non-Attendees

- Kate Sullivan, Metra
- Andy Roth, Metra
- Brian Stepp, Metra
- Bill Wettstein, Metra
- Kathy Chernich, USACE
- Melyssa Navis, USACE
- Jim Novak, Huff & Huff, Inc.
- Rich Ray, Huff & Huff, Inc.

Discussion Notes

A conference call was held to coordinate water quality, wetlands, and flooding with the Corp of Engineers related to the replacement of the existing Fox River Bridge for the Milwaukee District Westline, in Elgin, Illinois.

A. Roth opened the discussion with a brief project description. The proposed bridge replacement is the subject of an Environmental Assessment (EA) that is in progress. Metra has applied for a Tiger 6 grant for the bridge. The existing bridge piers are 130 years old and are made of stone. The original wooden structure on the bridge was replaced by steel in the early 1900s. The bridge was originally built as a single track to save money by the Milwaukee Railroad. There are two tracks on land on either side of the bridge. This causes a bottleneck every day.

In 2015 there will be new passenger service to Rockford that will use the bridge. There will be one round trip per day in 2015 and two round trips per day in 2016. Other railroads use this track as a bypass when there are problems or constructions elsewhere.

Metra wants to build a double track system. A new single track bridge would be built next to the existing bridge. Then the existing bridge would be replaced. The current bridge has six spans. The replacement bridge will have four spans.

To receive the Tiger grant the project must have or be close to having the EA approved. Metra has conducted coordination with Elgin and IDNR. An EcoCAT was conducted in 2010 and IDNR found that adverse effects were unlikely at that time. After the EcoCAT a state listed mussel (spike mussel) was found during a site investigation. Coordination occurred with IDNR and it was determined an Incidental Take Authorization (ITA) will be required prior to construction and that any mussels in the project area are to be relocated.

In the project area the US 20 bridge goes over the railroad bridge. There is also a Union Pacific (UP) bridge that is close to the existing bridge. It is part of a different railroad system. The Fox River does an S



curve by the bridge. By placing the bridge right next to the existing bridge this will minimize the impact to the river. The channel width at the Metra bridge is wider than the UP bridge.

The Corp of Engineers asked why state agencies had been coordinate with but not Federal agencies. A. Roth stated that it was not known when funding would be available. This is the third time Metra has tried for a Tiger grant. Canadian Pacific (CP), who also uses the tracks, has stated they would provide some funds. Design had started at one time but ended at about 20 percent design due to the lack of funding. The project sat on the shelf for about four years after this. If Metra gets the grant design would start up again. If Metra does not get the grant Metra still wants to finish the EA. In comments on the draft EA, the FTA has asked Metra to coordinate with the Corp of Engineers on issues such as water quality, wetlands, and flooding.

No wetlands are expected to be impacted. There are pretty much no trees and no issues with the northern long-eared bat or any other Federal T+E or candidate species are expected. Metra would like a letter from the Corp of Engineers to use to document that coordination has occurred. The Corp of Engineers mentioned that the letter would have to be a general letter at this time and that it is too early to comment on some of the issues that FTA asked about at this point in the project.

Item	Description	Resp. Party	Status	Entry Date Due Date Compl'd
01.000	Schedule			
02.000	Budget & Scope			
03.000	Submittals			
04.000	Quality			
05.000	Permits / Agreements			
06.000	Environmental	Huff & Huff	In Prog	9/02/2014
07.000	Operations / Coordination			
08.000	Safety			
09.000	Other Issues & Concerns			
10.000	Design Criteria			
11.000	Data Collection			
12.000	Land Acquisition			

Any comments, additions, or corrections shall be made, in writing, within five (5) business days of the issue date of these minutes. If no comments, additions, or corrections are received within the five (5) business days period, these minutes shall be deemed approved and shall be binding on all parties.

Applicant: Huff & Huff, Inc.
Contact: Evan Markowitz
Address: 915 Harger Rd Suite 330
Oak Brook, IL 60523

IDNR Project Number: 1411846
Date: 06/02/2014
Alternate Number: 1201964

Project: Metra Fox River Bridge
Address: Fox River at Metra Bridge, Elgin

Description: The Fox River Bridge is a single-track structure which carries Metra's Milwaukee District West Line over the Fox River in Elgin, Illinois. The bridge was constructed in 1881, consisting of six steel spans resting on masonry abutments and piers. A new bridge will be constructed adjacent to the existing bridge to provide a second mainline track over the Fox River. Upon completion of the new bridge, the existing older structure will be removed with a new replacement bridge constructed in its place.

Natural Resource Review Results

This project was submitted for information only. It is not a consultation under Part 1075.

The Illinois Natural Heritage Database shows the following protected resources may be in the vicinity of the project location:

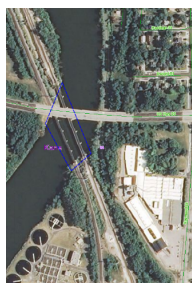
Bluff Spring Fen INAI Site
Bluff Spring Fen Nature Preserve
Black-Crowned Night Heron (*Nycticorax nycticorax*)
Osprey (*Pandion haliaetus*)
Spike (*Elliptio dilatata*)

Location

The applicant is responsible for the accuracy of the location submitted for the project.

County: Kane

Township, Range, Section:
41N, 8E, 24



IL Department of Natural Resources
Contact
Impact Assessment Section
217-785-5500
Division of Ecosystems & Environment

Local or State Government Jurisdiction
Other

Disclaimer

The Illinois Natural Heritage Database cannot provide a conclusive statement on the presence, absence, or condition of natural resources in Illinois. This review reflects the information existing in the Database at the time of this inquiry, and should not be regarded as a final statement on the site being considered, nor should it be a substitute for detailed site surveys or field surveys required for environmental assessments. If additional protected resources are encountered during the project's implementation, compliance with applicable statutes and regulations is required.

Terms of Use

By using this website, you acknowledge that you have read and agree to these terms. These terms may be revised by IDNR as necessary. If you continue to use the EcoCAT application after we post changes to these terms, it will mean that you accept such changes. If at any time you do not accept the Terms of Use, you may not continue to use the website.

1. The IDNR EcoCAT website was developed so that units of local government, state agencies and the public could request information or begin natural resource consultations on-line for the Illinois Endangered Species Protection Act, Illinois Natural Areas Preservation Act, and Illinois Interagency Wetland Policy Act. EcoCAT uses databases, Geographic Information System mapping, and a set of programmed decision rules to determine if proposed actions are in the vicinity of protected natural resources. By indicating your agreement to the Terms of Use for this application, you warrant that you will not use this web site for any other purpose.

2. Unauthorized attempts to upload, download, or change information on this website are strictly prohibited and may be punishable under the Computer Fraud and Abuse Act of 1986 and/or the National Information Infrastructure Protection Act.

3. IDNR reserves the right to enhance, modify, alter, or suspend the website at any time without notice, or to terminate or restrict access.

Security

EcoCAT operates on a state of Illinois computer system. We may use software to monitor traffic and to identify unauthorized attempts to upload, download, or change information, to cause harm or otherwise to damage this site. Unauthorized attempts to upload, download, or change information on this server is strictly prohibited by law.

Unauthorized use, tampering with or modification of this system, including supporting hardware or software, may subject the violator to criminal and civil penalties. In the event of unauthorized intrusion, all relevant information regarding possible violation of law may be provided to law enforcement officials.

Privacy

EcoCAT generates a public record subject to disclosure under the Freedom of Information Act. Otherwise, IDNR uses the information submitted to EcoCAT solely for internal tracking purposes.



Project Name: Milwaukee District Westline
Project #: Contract #: Task #: Environmental Assessment

Progress Meeting ##

Table with 7 columns: Date, Start, End, Next Meeting, Next Time, Prepared By, Company. Row 1: 3/28/2014, 11:15am, 11:45am, Jim Novak, Huff & Huff. Section: Attended By (Kate Sullivan, Steve Hamer, Jim Novak) and Non-Attendees (Conference Call).

Discussion Notes

A conference call was held to coordinate threatened and endangered species reviews with the IDNR related to the replacement of the existing Fox River Bridge for the Milwaukee District Westline, in Elgin, Illinois.

J. Novak opened the discussion with a brief project description. The proposed bridge replacement is the subject of an Environmental Assessment (EA) that is in progress. J. Novak reminded S. Hamer that this is the project in which a spike mussel had been found next to the existing bridge during a field visit.

J. Novak mentioned that we had communicated with the IDNR on this after finding the spike and it was decided that a Conservation Plan and Incidental Take Authorization (ITA) would be required before construction. S. Hamer indicated that from their standpoint it did not have to be conducted prior to the Finding of No Significant Impact (FONSI).

S. Hamer mentioned that although not really applicable to a Metra project, that reviews for State funded projects are now being done at the District level, through their Central Office. A new Memorandum of Understanding has been signed with IDOT to do those reviews at that level.

J. Novak asked if the team should prepare the ITA to cover all potential mussel species that could be encountered in the Fox River. S. Hamer indicated that this was a good idea, because if we encounter another species while work is going on, that could hold the project up while another ITA is completed for that species.



Item	Description	Resp. Party	Status	Entry Date Due Date Compl'd
01.000	Schedule			
02.000	Budget & Scope			
03.000	Submittals			
04.000	Quality			
05.000	Permits / Agreements			
06.000	Environmental	Huff & Huff	In Prog	4/16/201 4
07.000	Operations / Coordination			
08.000	Safety			
09.000	Other Issues & Concerns			
10.000	Design Criteria			
11.000	Data Collection			
12.000	Land Acquisition			

Any comments, additions, or corrections shall be made, in writing, within five (5) business days of the issue date of these minutes. If no comments, additions, or corrections are received within the five (5) business days period, these minutes shall be deemed approved and shall be binding on all parties.



Office of the Mayor

David J. Kaptain
MAYOR

150 DEXTER COURT
ELGIN, ILLINOIS 60120

847/931-5595
mayor@cityofelgin.org

March 26, 2014

The Honorable Anthony Foxx
Secretary
United States Department of Transportation
1200 New Jersey Avenue, SE
Washington, D.C. 20590

Dear Secretary Foxx:

The City of Elgin would like to express its support for Metra's application for funding from the FY2014 TIGER program for the MD-W Fox River Bridge Improvement Project.

Elgin is committed to promoting compact, mixed-use development in our central area, continuing to attract residents and businesses to our historic downtown and enhancing pedestrian, bicycle and transit mobility throughout our community. The City completed the second phase of our Sustainability Action Plan in 2011, and a sidewalk gap and transit stop study to recommend specific pedestrian and transit access improvements was completed in January 2014. Elgin completed a transit-oriented development plan for the area near the Metra National Street Station in 2011, and a planning study of the Chicago Street Station area is nearing completion. The City has also made significant investments in a number of projects—including a riverfront park, riverwalk and extensive Central Business District streetscaping project—designed to transform our city center.

The MD-W Fox River Bridge Improvement Project supports Elgin's efforts to revitalize our downtown and improve mobility for our residents. Riders boarding at Elgin's three Metra stations depend on the bridge to connect them with employment, education and recreational opportunities along the MD-W corridor and in downtown Chicago. Reconstruction and expansion of the bridge will increase the attractiveness of commuter rail as an alternative to automobile use and preserve reliable connections to Pace service. In addition, the new bridge will have an improved appearance, and the reduced number of concrete piers will benefit recreational users of the Fox River.

Your consideration of Metra's grant application is appreciated. Please contact me if you have questions or need additional information.

Sincerely,

David J. Kaptain
Mayor, City of Elgin



Office of the Mayor

David J. Kaptain
MAYOR

150 DEXTER COURT
ELGIN, ILLINOIS 60120

847/931-5595
mayor@cityofelgin.org

May 22, 2013

The Honorable Ray LaHood
Secretary
United States Department of Transportation
1200 New Jersey Avenue, SE
Washington, D.C. 20520

Dear Secretary LaHood:

The City of Elgin would like to express its support for Metra's application for funding from the FY2013 TIGER program for the MD-W Fox River Bridge Improvement Project.

Elgin is committed to promoting compact, mixed-use development in our central area, continuing to attract residents and businesses to our historic downtown, and enhancing pedestrian, bicycle, and transit mobility throughout our community. In August 2011, the City completed the second phase of its Sustainability Action Plan. Elgin also completed a transit-oriented development plan for the area near the Metra National Street Station in 2011, and a planning study of the Chicago Street Station area is currently underway.

The MD-W Fox River Bridge Improvement Project supports the recommendations of these planning efforts. By improving reliability and travel times on the MD-W Line, this project will increase the attractiveness of commuter rail as an alternative to automobile use and preserve reliable connections to Pace service. In addition, the new bridge will have an improved appearance, and the reduced number of concrete piers will benefit recreational users of the Fox River.

Your consideration of Metra's grant application is appreciated. Please contact me if you have questions or need additional information.

Sincerely,

David J. Kaptain
Mayor, City of Elgin



547 W. Jackson Blvd. Chicago, IL 60661 (312) 322-6900 TTY# 1-312-322-6774

November 9, 2012

Ms. Melissa Cook
Tribal Historic Preservation Officer
Forest County Potawatomi Community
8130 Mish ko swen Drive
P.O. Box 340
Crandon, Wisconsin 54520

**RE: Metra Fox River Bridge Replacement and Track Addition
Elgin, Kane County, Illinois**

Dear Melissa Cook:

In response to your letter to Lois Kimmelman at the FTA, dated September 27, 2012, Metra is forwarding the Illinois Historic Preservation Agency's concurrence that the Fox River Bridge Replacement and Track Addition project, as planned, will not affect any cultural or historic properties.

The extents of the Fox River Bridge replacement project will be limited to existing, and previously developed, railroad right-of-way property. It will occur within and over the Fox River in the same location as the existing bridge. As the State Historic Preservation Officer does not believe that any cultural or historic property will be affected, and therefore did not require any further study, Metra hopes that these attachments satisfy your request.

Should any discovery during construction flag the requirement for further archival reviews, cultural resource investigation studies or archeological reports, Metra will coordinate any findings with the State Historic Preservation Officer and the Forest County Potawatomi.

If you have any questions or require additional information, please feel free to call me at (312) 322-6922.

Sincerely,

Joseph L. Lorenzini, P.E.
Chief Engineering Officer

Cc: D. Werner, Federal Transit Administration
G. Peters
B. Stepp
A. Roth
K. Sullivan
J. Novak, Huff & Huff, Inc.



Forest County Potawatomi
Cultural Center and Museum

POTAWATOMI
(Keeper of the Fire)

September 27, 2012

Lois Kimmelman, Environmental Protection Specialist
U.S. Department of Transportation
FTA
200 West Adams Street
Chicago, IL 60606-5253

Re: Metra Fox River Bridge Replacement and Track Addition, Elgin, Kane County, Illinois

Dear Lois Kimmelman,

Thank you for your notice of intent for the proposed project references above, as provided in the FTA letter from Marisol Simon, dated August 17, 2012. As this project occurs within Potawatomi ancestral and previously occupied lands, we appreciate the opportunity to express our concerns with any impacts to historic and cultural properties located within the project area of potential effect for the project mentioned above.

We appreciate receiving results of an archival review, cultural resource investigation studies, and archaeological reports. Should there be an impact or effect to cultural and historic properties as a result of this project, we will request consultation pursuant to Section 106 of the National Historic Preservation Act, as amended.

If you have any questions or concerns, please contact me at 715-478-7248 or email at Melissa.Cook@fcpotawatomi-nsn.gov. You may send the results of the archival review and archaeological report to:

Forest County Potawatomi Community
Attn: Melissa Cook, Tribal Historic Preservation Officer
8130 Mish ko swen Drive
P.O. Box 340
Crandon, WI 54520
Melissa.Cook@fcpotawatomi-nsn.gov (for digital format)

We appreciate involvement with your initiative. Your interest in protecting Potawatomi's cultural and historic properties is appreciated.

Respectfully,

Melissa Cook
Tribal Historic Preservation Officer

10-02-12 14:38:44
5460 Everybody's Road • Crandon, Wisconsin 54520
Telephone (715) 478-7474 • (800) 960-5479 • Fax (715) 478-7482



U.S. Department
of Transportation
**Federal Transit
Administration**

REGION V
Illinois, Indiana,
Michigan, Minnesota,
Ohio, Wisconsin

200 West Adams Street
Suite 320
Chicago, IL 60606-5253
312-353-2789
312-886-0351 (fax)

4539

August 17, 2012

Ms. Kelli Mosteller
Tribal Historic Preservation Officer
Citizen Potawatomi Nation
1601 S. Gordon Cooper Dr.
Shawnee, OK 74801

Re: Metra Fox River Bridge Replacement and Track Addition, Elgin, Illinois

Dear Ms. Mosteller:

The Federal Transit Administration (FTA) and Metra have initiated the environmental evaluation process for the replacement of the existing single-track Milwaukee District West line railroad bridge over the Fox River in Elgin, Kane County, Illinois. The project involves the demolition of the single-track bridge and construction of a new double-track railroad bridge parallel to the existing bridge, in order to accommodate the current double-track configuration on either side of the river.

Please see attached figure showing the location of the project.

We are inviting you to participate in consultation to help us identify places that may have traditional religious and cultural importance to your tribal organization. Please note that we are requesting information only on such places that you believe may be impacted by the proposed project.

Thank you for your cooperation and interest in this project. Your timely response will greatly help us incorporate your concerns into project development.

If you have further questions, please contact Lois Kimmelman, Environmental Protection Specialist, at (312) 353-4060, or David Werner, Community Planner, at (312) 353-3879.

Sincerely,

for Marisol R. Simón
Regional Administrator

cc: Lois Kimmelman, FTA
Melody Hopson, FTA
David Werner, FTA
Tom Weaver, Metra



U.S. Department
of Transportation
**Federal Transit
Administration**

REGION V
Illinois, Indiana,
Michigan, Minnesota,
Ohio, Wisconsin

200 West Adams Street
Suite 320
Chicago, IL 60606-5253
312-353-2789
312-886-0351 (fax)

4889

August 17, 2012

Mr. Harold Frank, Chairman
Forest County Potawatomi Community
P.O. Box 340
Crandon, WI 54520

Re: Metra Fox River Bridge Replacement and Track Addition, Elgin, Illinois

Dear Mr. Frank:

The Federal Transit Administration (FTA) and Metra have initiated the environmental evaluation process for the replacement of the existing single-track Milwaukee District West line railroad bridge over the Fox River in Elgin, Kane County, Illinois. The project involves the demolition of the single-track bridge and construction of a new double-track railroad bridge parallel to the existing bridge, in order to accommodate the current double-track configuration on either side of the river.

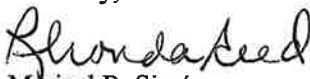
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Thank you for your cooperation and interest in this project. Your timely response will greatly help us incorporate your concerns into project development.

If you have further questions, please contact Lois Kimmelman, Environmental Protection Specialist, at (312) 353-4060, or David Werner, Community Planner, at (312) 353-3879.

Sincerely,

for 
Marisol R. Simón
Regional Administrator

cc: Lois Kimmelman, FTA
Melody Hopson, FTA
David Werner, FTA
Tom Weaver, Metra



U.S. Department
of Transportation
**Federal Transit
Administration**

REGION V
Illinois, Indiana,
Michigan, Minnesota,
Ohio, Wisconsin

200 West Adams Street
Suite 320
Chicago, IL 60606-5253
312-353-2789
312-886-0351 (fax)

4329

August 17, 2012

Mr. Steve Ortiz, Chairperson
Prairie Band of Potawatomi Nation
16281 Q Road
Mayetta, KS 66509

Re: Metra Fox River Bridge Replacement and Track Addition, Elgin, Illinois

Dear Mr. Ortiz:

The Federal Transit Administration (FTA) and Metra have initiated the environmental evaluation process for the replacement of the existing single-track Milwaukee District West line railroad bridge over the Fox River in Elgin, Kane County, Illinois. The project involves the demolition of the single-track bridge and construction of a new double-track railroad bridge parallel to the existing bridge, in order to accommodate the current double-track configuration on either side of the river.

Please see attached figure showing the location of the project.

We are inviting you to participate in consultation to help us identify places that may have traditional religious and cultural importance to your tribal organization. Please note that we are requesting information only on such places that you believe may be impacted by the proposed project.

Thank you for your cooperation and interest in this project. Your timely response will greatly help us incorporate your concerns into project development.

If you have further questions, please contact Lois Kimmelman, Environmental Protection Specialist, at (312) 353-4060, or David Werner, Community Planner, at (312) 353-3879.

Sincerely,

for Marisol R. Simón
Regional Administrator

cc: Lois Kimmelman, FTA
Melody Hopson, FTA
David Werner, FTA
Tom Weaver, Metra



U.S. Department
of Transportation
**Federal Transit
Administration**

REGION V
Illinois, Indiana,
Michigan, Minnesota,
Ohio, Wisconsin

4339
200 West Adams Street
Suite 320
Chicago, IL 60606-5253
312-353-2789
312-886-0351 (fax)

August 17, 2012

Mr. Kenneth Meshigaud, Chairperson
Hannahville Indian Community
N14911 Hannahville B1 Rd.
Wilson, MI 49896-9728

Re: Metra Fox River Bridge Replacement and Track Addition, Elgin, Illinois

Dear Mr. Meshigaud:

The Federal Transit Administration (FTA) and Metra have initiated the environmental evaluation process for the replacement of the existing single-track Milwaukee District West line railroad bridge over the Fox River in Elgin, Kane County, Illinois. The project involves the demolition of the single-track bridge and construction of a new double-track railroad bridge parallel to the existing bridge, in order to accommodate the current double-track configuration on either side of the river.

Please see attached figure showing the location of the project.

We are inviting you to participate in consultation to help us identify places that may have traditional religious and cultural importance to your tribal organization. Please note that we are requesting information only on such places that you believe may be impacted by the proposed project.

Thank you for your cooperation and interest in this project. Your timely response will greatly help us incorporate your concerns into project development.

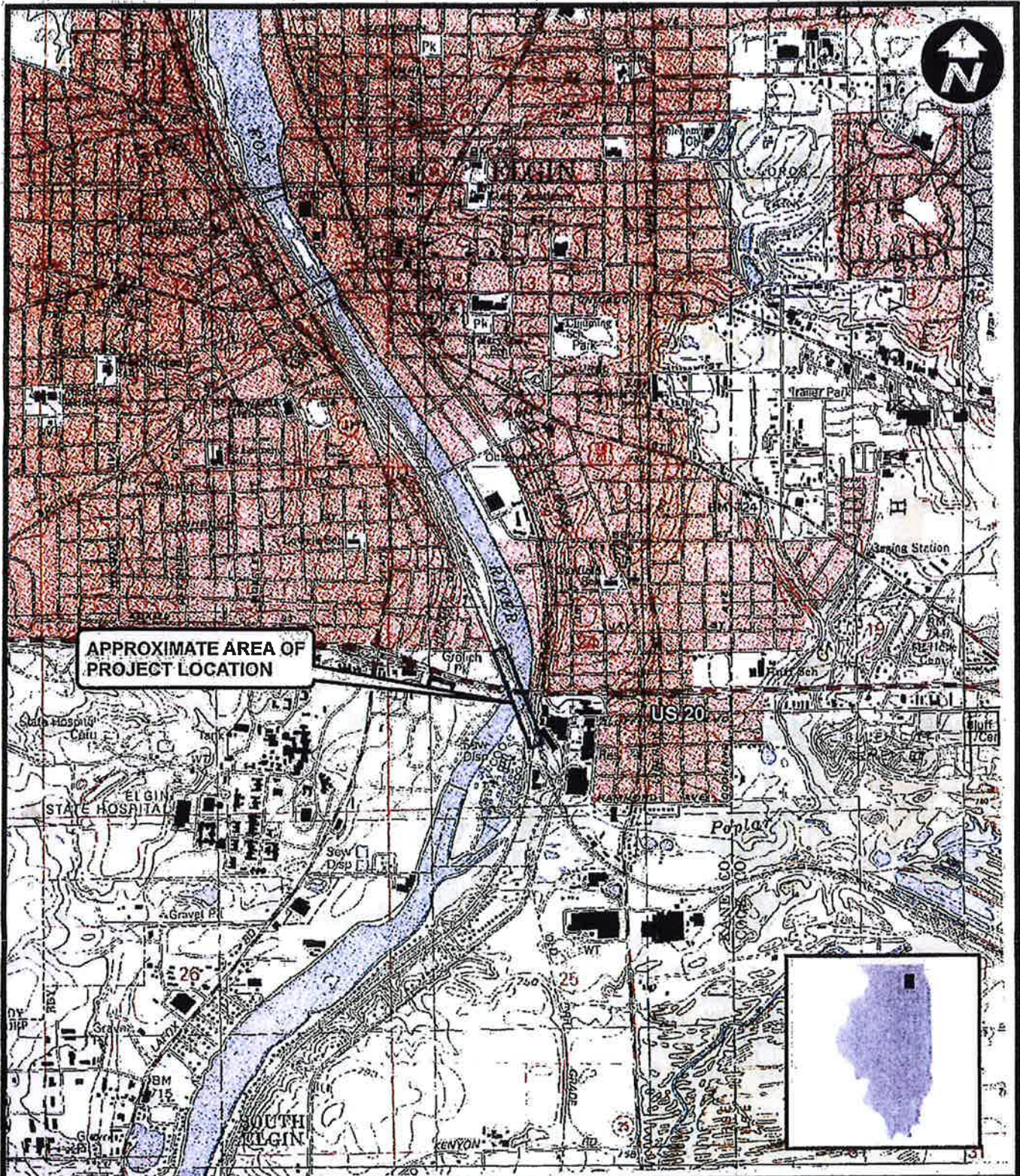
If you have further questions, please contact Lois Kimmelman, Environmental Protection Specialist, at (312) 353-4060, or David Werner, Community Planner, at (312) 353-3879.

Sincerely,

for 

Marisol R. Simón
Regional Administrator


cc: Lois Kimmelman, FTA
Melody Hopson, FTA
David Werner, FTA
Tom Weaver, Metra




APPROXIMATE AREA OF PROJECT LOCATION

FIGURE A
SITE LOCATION MAP
FOX RIVER BRIDGE REPLACEMENT
ELGIN, KANE COUNTY, ILLINOIS

Legend

 PROJECT LOCATION

2,000

 Feet



SOURCE: U.S. DEPARTMENT OF THE INTERIOR
 U.S. GEOLOGICAL SURVEY
 ELGIN QUADRANGLE

CADFILE WIGHT METRA 1030



**Illinois Historic
Preservation Agency**

FAX (217) 782-8161

1 Old State Capitol Plaza • Springfield, Illinois 62701-1512 • www.illinois-history.gov

Kane County

Elgin

Bridge Replacement, MD-W Bridge Z-100 (Revised)
U.S. Route 20 over the Fox River
IHPA Log #010082310

August 25, 2011

Jim Novak
Huff & Huff, Inc.
915 Harger Road, Suite 330
Oak Brook, IL 60523

Dear Mr. Novak:

We have reviewed the revised documentation submitted for the referenced project(s) in accordance with 36 CFR Part 800.4. Based upon the information provided, no historic properties are affected. We, therefore, have no objection to the undertaking proceeding as planned.

Please retain this letter in your files as evidence of compliance with section 106 of the National Historic Preservation Act of 1966, as amended. This clearance remains in effect for two years from date of issuance. It does not pertain to any discovery during construction, nor is it a clearance for purposes of the Illinois Human Skeletal Remains Protection Act (20 ILCS 3440).

If you have any further questions, please contact me at 217/785-5027.

Sincerely,

Anne E. Haaker

Anne E. Haaker
Deputy State Historic
Preservation Officer



Office of the Mayor

David J. Kaptain
MAYOR

150 DEXTER COURT
ELGIN, ILLINOIS 60120

847/931-5595
mayor@cityofelgin.org

August 25, 2011

Joseph L. Lorenzini
Chief Engineering Officer
Metra
547 West Jackson Boulevard
Chicago, Illinois 60661

Dear Mr. Lorenzini:

I am writing in support of Metra's proposal to replace the existing Milwaukee District West Line Bridge over the Fox River in Elgin, Illinois. From what I understand, the century old bridge is nearing the end of its useful life and the cost to maintain and repair it is no longer economically feasible. As Mayor, I believe investing in infrastructure upgrades is critical to keeping Metra service a viable commuting option for my constituents. I also view a new bridge structure as an enhancement to the area. Not only will the new structure have an improved appearance, but the reduced number of concrete piers embedded in the river will also benefit recreational users of the river.

The City of Elgin and Metra have enjoyed a positive and productive working partnership for many years. I support Metra's efforts to improve the Milwaukee District West Line and continue to provide the residents of Elgin and neighboring communities with safe, reliable and cost-efficient commuter rail service.

Please do not hesitate to contact me if I can be of assistance with this project.

Sincerely,

David Kaptain
Mayor
City of Elgin



Illinois Department of Natural Resources

One Natural Resources Way Springfield, Illinois 62702-1271
<http://dnr.state.il.us>

Pat Quinn, Governor

Marc Miller, Acting Director

August 17, 2011

Ms. Alycia A. Kluenenberg
Huff & Huff, Inc.
915 Harger Road, Suite 330
Oak Brook, Illinois 60523-1486

Fox River Bridge Replacement
Metra
IDNR Proj. No. 1201964
Kane County

Dear Ms. Kluenenberg,

The Department of Natural Resources (DNR) has reviewed the above referenced project which was submitted through the Eco-cat review program. Based on further review and information received from your office the project has potential for the need to apply for an Incidental Take Authorization (ITA). The project as described with in-stream work has potential for adverse impact to a listed mussel species, the Spike mussel (*Elliptio dilatata*). It is important that this process be implemented to assure the project meet any impending construction schedule. Consultation remains open on this project based on your application to implement the ITA.

This coordination effort should be addressed to Mr. Joseph Kath, Endangered Species Project Manager, Division of Natural Heritage, One Natural Resources Way, Springfield, Illinois 62702-1271.

If you have any questions on the above, please contact me at 217-785-4862.

Sincerely,

Steve Hamer
Transportation Review Program
Division of Ecosystems and Environment

cc: Joe Kath, IDNR/ORC/Natural Heritage

Applicant: Huff & Huff, Inc.
Contact: Alycia A Klueenberg
Address: 915 Harger Road
Suite 330
Oak Brook, IL 60523

IDNR Project #: 1201964
Date: 08/12/2011

Project: Fox River Bridge Reconstruction
Address: Fox River at Metra Bridge, Elgin

Description: Replacement of the existing bridge, which was built in 1881. Repairs are no longer economically feasible and replacement is necessary to come into compliance with current design criteria.

Natural Resource Review Results

Consultation for Endangered Species Protection and Natural Areas Preservation (Part 1075)

The Illinois Natural Heritage Database shows the following protected resources may be in the vicinity of the project location:

- Bluff Spring Fen INAI Site
- Bluff Spring Fen Nature Preserve
- Black-Crowned Night Heron (*Nycticorax nycticorax*)
- Osprey (*Pandion haliaetus*)
- Spike (*Elliptio dilatata*)

An IDNR staff member will evaluate this information and contact you within 30 days to request additional information or to terminate consultation if adverse effects are unlikely.

Location

The applicant is responsible for the accuracy of the location submitted for the project.

County: Kane
Township, Range, Section:
41N, 8E, 24



IL Department of Natural Resources Contact
Rick Pietruszka
217-785-5500
Division of Ecosystems & Environment

Local or State Government Jurisdiction
Metra
Andy Roth
547 W Jackson
Chicago, Illinois 60661

Disclaimer

The Illinois Natural Heritage Database cannot provide a conclusive statement on the presence, absence, or condition of natural resources in Illinois. This review reflects the information existing in the Database at the time of this inquiry, and should not be regarded as a final statement on the site being considered, nor should it be a substitute for detailed site surveys or field surveys required for environmental assessments. If additional protected resources are encountered during the project's implementation, compliance with applicable statutes and regulations is required.

Terms of Use

By using this website, you acknowledge that you have read and agree to these terms. These terms may be revised by IDNR as necessary. If you continue to use the EcoCAT application after we post changes to these terms, it will mean that you accept such changes. If at any time you do not accept the Terms of Use, you may not continue to use the website.

1. The IDNR EcoCAT website was developed so that units of local government, state agencies and the public could request information or begin natural resource consultations on-line for the Illinois Endangered Species Protection Act, Illinois Natural Areas Preservation Act, and Illinois Interagency Wetland Policy Act. EcoCAT uses databases, Geographic Information System mapping, and a set of programmed decision rules to determine if proposed actions are in the vicinity of protected natural resources. By indicating your agreement to the Terms of Use for this application, you warrant that you will not use this web site for any other purpose.
2. Unauthorized attempts to upload, download, or change information on this website are strictly prohibited and may be punishable under the Computer Fraud and Abuse Act of 1986 and/or the National Information Infrastructure Protection Act.
3. IDNR reserves the right to enhance, modify, alter, or suspend the website at any time without notice, or to terminate or restrict access.

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Privacy

EcoCAT generates a public record subject to disclosure under the Freedom of Information Act. Otherwise, IDNR uses the information submitted to EcoCAT solely for internal tracking purposes.



August 10, 2011

Mr. David Kaptain
Mayor
City of Elgin
150 Dexter Ct.
Elgin, Illinois 60120-5555

**Re: Metra – Milwaukee District Westline
Fox River Bridge Reconstruction
City of Elgin, Illinois
Metra Project Number 4394**

Dear Mayor Kaptain:

Metra is developing plans for the reconstruction of the Milwaukee District Westline (MDW) bridge over the Fox River in Elgin, Kane County, Illinois. The existing Fox River Bridge is a single-track structure which carries Metra's MDW over the Fox River. The bridge was constructed in 1881, consisting of six steel spans resting on masonry abutments and piers. Three of the original spans were replaced in 1905, and the other three were replaced in 1926. The piers and abutments date from the original 1881 construction, with cast-in-place concrete modifications as required (to accommodate the new girders).

The bridge is nearing the end of its useful life; although the masonry piers and abutments are in good condition, they need to be strengthened to come into compliance with current railroad design criteria. The bridge has reached the point where further repairs would not be economically feasible. Therefore, replacement of the bridge is required in the near future. It is important for maintenance and train operations to keep mainline track alignments straight, especially passenger train mainlines. Therefore, three alternatives were studied by Metra for the new structure keeping any track alignment changes as minimal as possible.

The single-track Fox River Bridge is the only single-track segment on the double and triple mainline track alignment between Elgin and Chicago. Although Metra originally notified the IDNR that the replacement bridge would be single track, through the course of completing an Environmental Assessment, Metra has decided that the replacement bridge will include two mainline tracks.

The bridge replacement will occur within and over the Fox River in the same location. The new track alignment will be adjusted to meet the existing alignment on both sides of the river. Figure 1 shows the project site location map. There is a parallel railroad bridge to the southwest of the

Mr. David Kaptain, Mayor of Elgin
August 10, 2011
Metra MDW – Fox River Bridge (#4394)

Page 2

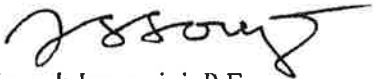
Metra MDW line bridge owned by the Union Pacific Railroad. This bridge is not included in the scope of the project.

The project is being processed as an Environmental Assessment by Metra for the Federal Transit Administration. The Environmental Assessment is still in progress and has not been released for public review at this time.

We are requesting your review of the information provided and your support for this project. At this time, no permits are being requested. Permitting will be initiated upon approval of the Environmental Assessment, completion of design, and when we secure construction funds.

If you have any questions or require additional information, please feel free to call me at (312) 322-6922.

Sincerely,



Joseph Lorenzini, P.E.
Chief Engineering Officer

cc: Jim Novak, Huff & Huff, Inc.
A. Roth

attachments



environmental engineers
and consultants

915 Harger Road, Suite 330
Oak Brook, IL 60523
Phone: (630) 684-9100
Fax: (630) 684-9120
Website: <http://huffnhuff.com>

August 5, 2011

Ms. Anne Haaker
Illinois Historic Preservation Agency
Preservation Services Division
One Old State Capital
Springfield, IL 62701

**Re: Agency project review
IHPA Log #010082310
Metra-Fox River Bridge Replacement
Milwaukee District West Line
Elgin, Illinois
T 41N, R 8E, Section 24**

Dear Ms. Haaker:

Metra is developing plans for the reconstruction of the Milwaukee District Westline (MDW) bridge over the Fox River in Elgin, Kane County, Illinois (Township 41 North, Range 8 East, Section 24). The existing Fox River Bridge is a single-track structure which carries Metra's MDW over the Fox River. The bridge was constructed in 1881, consisting of six steel spans resting on masonry abutments and piers. Three of the original spans were replaced in 1905, and the other three were replaced in 1926. The piers and abutments date from the original 1881 construction, with cast-in-place concrete modifications as required (to accommodate the new girders).

This bridge is nearing the end of its useful life; although the masonry piers and abutments are in good condition, they need to be strengthened to come into compliance with current railroad design criteria. The bridge has reached the point where further repairs would not be economically feasible. Therefore, replacement of the bridge is required in the near future. It is important for maintenance and train operations to keep mainline track alignments straight, especially passenger train mainlines. Therefore, three alternatives were studied by Metra for the new structure keeping any track alignment changes as minimal as possible.

The single-track Fox River Bridge is the only single-track segment on the double and triple mainline track alignment between Elgin and Chicago. Although Metra originally notified the Illinois Historic Preservation Agency (IHPA) that the replacement bridge

Ms. Anne Haaker, IHPA
August 5, 2011

Page 2

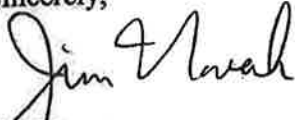
would be single track, through the course of completing this Environmental Assessment (EA), Metra has decided that the replacement bridge will include two Metra mainline tracks at this location. For your reference, a coordination request related to this project was submitted to IHPA by Lin Engineering in a letter dated August 17, 2010.

The bridge replacement will occur within and over the Fox River, and the alignment will be adjusted to meet existing alignment within approximately 500 to 600 feet on both sides of the river. There is a parallel railroad bridge to the southwest of the Metra MDW line bridge owned by the Union Pacific Railroad. This bridge is not included in the scope of the project.

According to the 2010 City of Elgin Zoning Map the adjacent areas to the north, west, south, and east of the site are shown as CF – “Community Facility”. There is also area zoned RC3 – Residence Conservation 3” to the northwest and northeast of the project location. At the southeast project limit is a CI – “Commercial Industrial” area.

This letter is being forwarded to your office on behalf of Metra to request updated information on historic or archeological resources within the project limits. The work is anticipated to commence in 2012. The project is being processed as an EA with the Federal Transit Administration. The EA is still in progress and has not been released for public review at this time. Enclosed please find a copy of the project location map, zoning map, and site photographs for your review. If you have any questions or require additional information, please feel free to call Kate Sullivan at Metra at (312) 322-6903.

Sincerely,



Jim Novak
Senior Scientist

cc: Andrew Roth, Metra
Kate Sullivan, Metra

attachments



May 16, 2011

Ms. Louise Clemency
Field Supervisor
U.S. Fish & Wildlife Service
1250 Grove Avenue
Suite 103
Barrington, Illinois 60010

Re: **Agency NEPA Project Notification**
Metra – Milwaukee District West Line
Fox River Bridge Replacement Project (Z-100)
City of Elgin, Kane County, Illinois
Township 41 North, Range 8 East, Section 24
(42.019789 lat., - 88.275433 long)
Metra Project Number 4339

Dear Ms. Clemency:

Metra is proposing the removal and replacement of the existing Milwaukee District West Line bridge over the Fox River in Elgin, Illinois. This bridge is nearing the end of its useful life; it has reached the point where further repairs would not be economically feasible. Therefore, replacement of the bridge is required.

The bridge was constructed in 1881, consisting of six steel spans resting on masonry abutments and piers. Three of the original spans were replaced in 1905, and the other three were replaced in 1926. The piers and abutments date from the original 1881 construction, with cast-in-place concrete modifications as required (accommodating the new girders).

The bridge replacement will occur within and over the Fox River. The existing track approaches will be adjusted to meet the new bridge alignment within 500 to 600 feet on both sides of the river. The activities will occur between the current Metra track and the adjacent Union Pacific track and bridge. This letter is being forwarded to your office to notify your office of the proposed project. The project is being processed by the Federal Transit Administration as an Environmental Assessment. As part of the NEPA process, Metra is coordinating with the other state and federal resource agencies. We are requesting information from your office on natural resources including federally threatened and endangered species.

The existing Metra railroad bridge, including the support piers in the river, will be removed after the new bridge is completed and in operation. The existing Metra bridge abutments along with the existing railroad embankment leading up to these abutments will remain in place. Adjacent land use includes the Fox River, Marie Grolich Park, commercial, and industrial facilities.

Metra has conducted the on-line Section 7 Consultation process (September 1, 2010) to determine the potential for encountering federally threatened or endangered species. The results of this exercise concluded that the project will *not affect* critical habitat or the following species that have been identified as federally endangered, threatened, proposed, and/or candidate species by the USFWS:

- Sheepnose mussel (*Plethobasus cyphus*), and
- Eastern prairie fringed orchid (*Platanthaera leucophaea*),

The following summarizes the results of the review.

- This project will *not affect* the sheepnose mussel. Suitable habitat for the sheepnose mussel includes large rivers which is present within the project area. According to the Illinois Natural History Survey (INHS), biologists recorded the sheepnose in the Kankakee River, where it remains a rare species. According to the INHS collections database, the most recent sighting of the sheepnose in the Kankakee River, Will County was in 2008. The sheepnose is a large river species that has been found in the Fox River Basin which is adjacent to the Des Plaines River Watershed. In a 2004 study performed by the INHS: The Freshwater Mussels (Bivalvia: Unionidae) of the Fox River Basin, Illinois and Wisconsin, it was concluded that the sheepnose was last collected from the Fox River in Dundee, Kane County in 1906 and is most likely extirpated from the Fox River Basin. H&H has conducted two mussel surveys on the Fox River downstream of the proposed Metra bridge in August 2010 and July 2009. No live individuals or relicts of the sheepnose mussel were encountered during these mussel surveys.
- This project will *not affect* the eastern prairie fringed orchid as its suitable habitat includes moderate to high quality wetlands, sedge meadows, marshes, mesic to wet prairies. These types of habitats are not present within the project area. No critical habitat rules have been published for the eastern prairie fringed orchid.

The Kane County ADID Wetland map depicts one wetland (ADID #1413) area located along the east side of the existing Metra track and southeast of the Fox River. According to the USFWS guidance, wetlands that are not high quality will not support eastern prairie fringed orchid. A wetland delineation was conducted in the project area on August 25, 2010. No wetlands were identified within the project area.

Ms. Louise Clemency, USFWS
Metra - Fox River Bridge Project MDW
May 10, 2011

Page 2

The work is anticipated to commence in 2012, pending available funding. Enclosed please find a copy of the project location map for your review. If you have any questions or require additional information, please feel free to call me at (312) 322-6922.

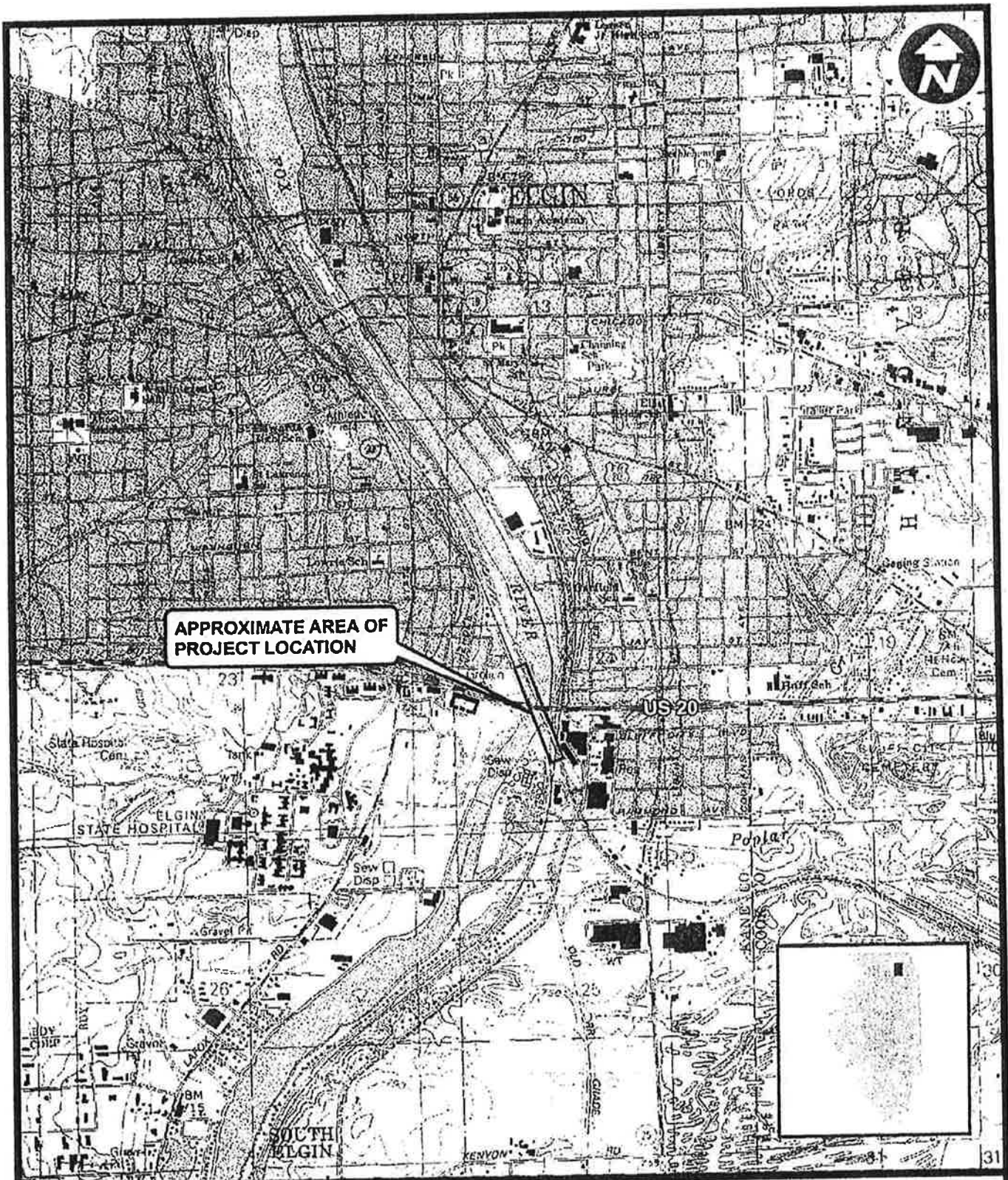
Sincerely,



Joseph L. Lorenzini
Chief Engineering Officer
Metra

cc: Jim Novak, Huff & Huff, Inc.
A. Roth
T. Weaver
K. Sullivan

attachments



APPROXIMATE AREA OF PROJECT LOCATION



CADRE, WASHINGTON, D.C.

**FIGURE 1
SITE LOCATION MAP
FOX RIVER BRIDGE REPLACEMENT
ELGIN, KANE COUNTY, ILLINOIS**

SOURCE: U.S. DEPARTMENT OF THE INTERIOR
U.S. GEOLOGICAL SURVEY
ELGIN QUADRANGLE

2,000
Feet



Chicago Metropolitan Agency for Planning

233 South Wacker Drive
Suite 800
Chicago, Illinois 60606

312 454 0400
www.cmap.illinois.gov

March 28, 2011

Mr. Thomas A. Weaver, Senior Manager
Grant Administration
Metra
547 West Jackson
Chicago IL 60661

Re: **Milwaukee District West Line, Bridge Z-100 Rehabilitation**
TIP Project # 18-08-2500 -Programmer: Metra

Dear Mr. Weaver:

This project is included in the FY10-15 Transportation Improvement Program (TIP) endorsed by the Chicago Metropolitan Agency for Planning and the MPO Policy Committee for Northeastern Illinois, the Metropolitan Planning Organization (MPO) for the region in which the project is located.

The FHWA and FTA determined that the current TIP conforms to the SIP and Clean Air Act Amendments. These findings were in accordance with 40 CFR part 51, "Criteria and Procedures for Determining Conformity to State or Federal Implementation Plans of Transportation Plans, Programs and Projects Funded or Approved Under Title 23 USC or the Federal Transit Act."

The project is consistent with the information used for the TIP conformity analysis. Therefore, this project conforms to the existing State Implementation Plan and the transportation-related requirements of the 1990 Clean Air Act Amendments.

Sincerely,

A handwritten signature in cursive script, appearing to read "Claire Bozic".

Claire Bozic
Senior Analyst



Memorandum

February 24, 2011

Ms. Claire Bozic
Senior Analyst, Research & Analysis
Chicago Metropolitan Agency for Planning
233 South Wacker Drive, Suite 800
Chicago, IL 60606-6415

SUBJECT: Air Quality Impact Conformity for Bridge Rehabilitation

Dear Ms. Bozic:

Metra is in the process of preparing environmental documentation to satisfy the requirements of National Environmental Policy Act for projects in Metra's capital programs. As in the past, Metra is asking CMAP to provide information regarding the air quality impacts of our projects and their status in a conformed TIP.

The following project is currently being evaluated:

18-08-2550 EP4339 Bridge Z-100, Milwaukee District West Line

This project was included in Metra's FY 2009 capital program. In the FY 2009 select year of the FY 2007-2012 TIP, Section 5307, TRA5309, and 5309B funding were assigned to "Project" 18-08-2550, as shown in the attachment. 18-08-2500 has been programmed for the rehabilitation of a variety of existing bridges on the Metra system, including #Z-100.

We appreciate your assistance with this documentation. Please contact me at (312) 322-6649 with any questions.

Sincerely,

Thomas A. Weaver
Senior Manager,
Grant Administration
twcaver@metrarr.com

Attachment: FY 2007-2012 TIP, October 8, 2009, page 128 of 177.

TW G:\GDP\Environmental\NEA-ER-DCE\PE4339_AQ.doc

MEMORANDUM

To: Kate Sullivan, Project Engineer, Metra
Andy Roth, Manager, Metra

From: Roger Klocek, Senior Scientist, Huff & Huff, Inc.
Jim Novak, Senior Scientist, Huff & Huff, Inc.

Date: November 2, 2010

RE: State Threatened Mussel Specs - Metra -- Milwaukee District, West Line -- Fox River
Bridge Reconstruction - Elgin, Illinois

Huff & Huff (H&H) conducted a reconnaissance for live mussels in the Fox River for the proposed reconstruction of the Metra Milwaukee District West (MDW) Line Bridge over the Fox River in Elgin, Kane County, Illinois. Figure 1 shows this location and our survey area. This reconnaissance was conducted as a result of the finding of a live spike mussel (*Elliptio dilatata*), a state threatened species at this location on August 25, 2010 during preliminary natural resource investigations for this project. Photographs of the spike mussel are presented in Figure 2.

The reconnaissance was conducted north and south of the existing Metra MDW bridge. This is one of three bridges constructed at this location. The other two bridges include a separate Union Pacific Railroad bridge west of the Metra MDW bridge and the US Route 20 Bridge which spans the river and the two railroad bridges. The reconnaissance was conducted on September 23, 2010. The reconnaissance was limited to the north and south shore near the Metra tracks to record living mussels. H&H combed the near shore area and shoreline for living and dead shells in shallow water on both the north and south shore.

H&H searched the sediments by tactile means for approximately 180 feet of shoreline on the north side of the river and 190 feet on the south shore of the river. The reconnaissance started at the northeast corner abutment of the Metra MDW Bridge and proceeded downstream along the shore. The first 60 feet had fine sediment approximately one to two inches deep covering cobble and had few live mussels. The remaining shoreline was sandier with fine gravel and some scattered cobble. The shoreline dropped off to unworkable depths within approximately six feet of the shore. H&H found six living giant floaters, *Pyganodon grandis*, and two plain pocketbooks, *Lampsilis cardium*, both are common and widespread species in Illinois. Relic shells of the giant floater were plentiful, along with several relic plain pocketbook shells. Photographs of the live collection are found in Figure 3.

H&H accompanied a Metra flagger to the south shore and examined approximately 190 feet of shoreline. The shoreline waters were generally rocky under the bridges where large boulders and cobble were placed to armor the shoreline. Small pockets of sandy gravel were present here, and larger exposures of sandy substrates were found northeast of the Metra MDW Bridge. This shoreline was the area where the living spike was encountered. H&H found one living mussel, a Threeridge (*Amblema plicata*). In addition, well worn relic shells of the purple wartyback (*Cyclonaias tuberculata*, - state threatened), spike, mucket (*Actinonaias ligamentina*), and round pigtoe (*Pleurobema sintoxia*) were found (Fig. 4). In addition to these relic shells, H&H found a fresher specimen of the spike, which was probably dead earlier this year. Also, many live and dead specimens of zebra mussels, (*Dreissena polymorpha*) were present with the living zebra mussels attached underneath the large boulders. There is a large amount of predation ongoing here with several diffuse middens present. The recently preyed upon spike shell, which was a smaller and younger individual than the one photographed in August, was taken from a midden, which was predominantly composed of zebra mussel shells. H&H searched for the photographed specimen, which was a large old individual, but could not locate it.

Zebra mussel presence is estimated to be low, with six live specimens taken from under one boulder, where they are hard to reach by fish predators. Zebra mussels can overgrow native mussel shells and eventually cause death, but the infestation of zebra mussels on native mussels is not apparent at all at this location.

The immediate environment on the south shoreline of the river provides shelter for many of the fish hosts that carry the larval stage of the spike. However, the habitat of boulders does not provide evident habitat for spike mussels, which are often found in softer bottom conditions. It is possible that these specimens of recent spike have been washed from somewhere upstream by high water events. It is difficult to say if some small population of native mussels is existing in the interstices between or underneath large boulders. Certainly there are many areas like this that are not searchable by hand.

Recommendations: Live mussels are not abundant at the Metra MDW site in Elgin, especially on the south shore. While it is intriguing that a live spike was found, there seem to be no further spike mussels close to shore in the immediate vicinity of the bridge, which is typical of current spike distribution in the Fox River.

Due to the conditions of the site, no further mussel surveys should be conducted prior to construction as it is unlikely that the spike mussel will be observed; however, because the spike mussel was found in 2010, it is recommended that additional reconnaissance be conducted prior to construction. The reconnaissance can be localized to the exact areas of proposed river disturbance for construction activities. The methods of construction have not been identified at this time. If coffer dams are required for new pier construction, the dewatered areas can be investigated for live mussels and any non-invasive species found will be relocated to safe habitat outside the project limits.

It is also recommended that strict water quality controls be placed on the construction activities, including sediment and erosion control measures in the event that live mussels, including the spike may still be present.



FIGURE 1. Aerial View of Mussel Survey Stations, Fox River, Shown as Dotted Lines



FIGURE 2. Living Spike Mussel Encountered South Shore Fox River 8-25-10



FIGURE 3. Living Giant Floaters and Plain Pocketbooks from the North Shore



FIGURE 4. One Living Threeridge from South Shore Plus Dead and Relic Shells



environmental engineers
and consultants

915 Harger Road, Suite 330
Oak Brook, IL 60523
Phone: (630) 684-9100
Fax: (630) 684-9120
Website: <http://huffnhuff.com>

October 15, 2010

Ms. Kate Sullivan, Associate Architect
Stations and Parking Design
Metra Engineering Department
547 West Jackson Boulevard
Chicago, Illinois 60661

**Re: Federal Endangered Species Act Section 7 Review
Metra Bridge (Z-100) over the Fox River Replacement Project
Elgin, Kane County, Illinois
Township 41 North, Range 8 East, Section 24
(42.019789 lat., - 88.275433 long)**

Dear Ms. Sullivan,

The US Fish & Wildlife Service (USFWS) has instituted a new coordination policy regarding review of federally threatened and endangered species relative to Section 7 of the Endangered Species Act. The USFWS no longer conducts project by project review upon request; rather, the applicant for a particular project is required to conduct an assessment of their project to determine if the project will impact federally listed species. To conduct this review, applicants use the Section 7 Consultation guidance, provided by the USFWS website, to document all findings.

Huff & Huff, Inc. (H&H) conducted the website consultation, unrelated to wetland permitting, and is providing endangered species information for the proposed Bridge Replacement Project located in Elgin, Kane County, Illinois. Enclosed please find the project location map (Figure 1) and representative photographs for your review.

Metra is proposing the removal of the existing Milwaukee District West Line bridge over the Fox River and the construction of a new railroad bridge. The existing Metra railroad bridge, including the support piers in the river, will be removed after the new bridge is completed and in operation. However, the existing Metra bridge abutments along with the existing railroad embankment leading up to these abutments will remain in place. The project limits is the existing Metra bridge over the Fox River plus 500 to 600 feet of approach on either side of the river. Adjacent land use is the Fox River, Marie Grolich Park, commercial, and industrial.

Based on the review of information provided by the USFWS website on September 1, 2010, as well as conditions observed in the field, H&H has determined that the project will *not affect*

critical habitat or the following species that have been identified as federally endangered, threatened, proposed, and/or candidate species by the USFWS.

- Sheepnose mussel (*Plethobasus cyphus*),
- Eastern prairie fringed orchid (*Platanthaera leucophaea*),

The following summarizes the results of the review.

- This project will *not affect* the sheepnose mussel. Suitable habitat for the sheepnose mussel includes large rivers which is present within the project area. According to the Illinois Natural History Survey (INHS), biologists recorded the sheepnose in the Kankakee River, where it remains a rare species. According to the INHS collections database, the most recent sighting of the sheepnose in the Kankakee River, Will County was in 2008. The sheepnose is a large river species that has been found in the Fox River Basin which is adjacent to the Des Plaines River Watershed. In a 2004 study performed by the INHS: The Freshwater Mussels (Bivalvia: Unionidae) of the Fox River Basin, Illinois and Wisconsin, it was concluded that the sheepnose was last collected from the Fox River in Dundee, Kane County in 1906 and is most likely extirpated from the Fox River Basin. H&H has conducted two mussel surveys on the Fox River downstream of the proposed Metra bridge in August 2010 and July 2009. No live individuals or relicts of the sheepnose mussel were encountered during these mussel surveys.
- This project will *not affect* the eastern prairie fringed orchid as its suitable habitat includes moderate to high quality wetlands, sedge meadows, marshes, mesic to wet prairies. These types of habitats are not present within the project area. No critical habitat rules have been published for the eastern prairie fringed orchid.

The Kane County ADID Wetland map depicts one wetland (ADID #1413) area located along the east side of the existing Metra track and southeast of the Fox River. According to the USFWS guidance, wetlands that are not high quality will not support eastern prairie fringed orchid. H&H conducted a wetland delineation of the project area on August 25, 2010. No wetlands were identified within the project area. Therefore, suitable habitat for the eastern prairie fringed orchid is not present. For these reasons, we conclude that the Eastern prairie fringed orchid is *not present* within the project limits.

For these reasons, the removal and construction of a new railroad bridge project will *not affect* the sheepnose mussel, eastern prairie fringed orchid, or critical habitat. Detailed surveys for these species were not conducted. Determinations are based on information provided by the USFWS, Section 7 Consultation website.

Ms. Kate Sullivan, Associate Architect
Metra Bridge (Z-100) over the Fox River Replacement Project
October 15, 2010

Page 3 of 4

If you have questions or require additional information, please contact me at 630-684-4416.

Sincerely,



Evan Markowitz
Wetland Scientist

References

- Illinois Natural History Survey Database. (n.d.). Retrieved February 20, 2010 from INHS Website, <http://ellipse.inhs.uiuc.edu:591/INHSCollections/FMPro>
- U. S. Fish and Wildlife Service. Federally Endangered, Threatened, Proposed, and Candidate Species, Illinois County Distribution. U.S. Department of the Interior, Fish and Wildlife Service, Washington, D.C. <http://www.fws.gov/midwest/Endangered/Lists/illinois-cty.html> accessed 7/28/2010.
- Young, Dick. Kane County Wild Plants and Natural Areas. Kane County Forest Preserve District. Geneva, Illinois. 1994.



**Illinois Historic
Preservation Agency**

FAX (217) 782-8161

1 Old State Capitol Plaza • Springfield, Illinois 62701-1512 • www.Illinois-history.gov

Kane County
Elgin

Bridge Replacement, MD-W Bridge Z-100
U.S. Route 20 over the Fox River
IHPA Log #010082310

September 24, 2010

Samuel Lahniers
LIN Engineering, Ltd.
210 W. Chestnut
Chatham, IL 62629

Dear Mr. Lahniers:

We have reviewed the documentation submitted for the referenced project(s) in accordance with 36 CFR Part 800.4. Based upon the information provided, no historic properties are affected. We, therefore, have no objection to the undertaking proceeding as planned.

Please retain this letter in your files as evidence of compliance with section 106 of the National Historic Preservation Act of 1966, as amended. This clearance remains in effect for two years from date of issuance. It does not pertain to any discovery during construction, nor is it a clearance for purposes of the Illinois Human Skeletal Remains Protection Act (20 ILCS 3440).

If you have any further questions, please contact me at 217/785-5027.

Sincerely,

Anne E. Haaker

Anne E. Haaker
Deputy State Historic
Preservation Officer



Office of the Mayor

Ed Schock
MAYOR

150 DEXTER COURT
ELGIN, ILLINOIS 60120

847/931-5595
Fax 847/931-6023

September 20, 2010

Joseph L. Lorenzini
Chief Engineering Officer
Metra
547 West Jackson Boulevard
7th Floor
Chicago, Illinois 60661

Dear Mr. Lorenzini:

I am writing in support of Metra's proposal to replace the existing Milwaukee District West Line bridge over the Fox River in Elgin, Illinois. From what I understand, the century old bridge is nearing the end of its useful life and the cost to maintain and repair it is no longer economically feasible. As Mayor, I believe investing in infrastructure upgrades is critical to keeping Metra service a viable commuting option for my constituents. I also view a new bridge structure as an enhancement to the area. Not only will the new structure have an improved appearance, but the reduced number of concrete piers embedded in the river will also benefit recreational users of the river.

The City of Elgin and Metra have enjoyed a positive and productive working partnership for many years. I support Metra's efforts to improve the Milwaukee District West Line and continue to provide the residents of Elgin and neighboring communities with safe, reliable and cost-efficient commuter rail service.

Please do not hesitate to contact me if I can be of any assistance with this project.

Sincerely,

Ed Schock
Mayor
City of Elgin



environmental engineers
and consultants

915 Harger Road, Suite 330
Oak Brook, IL 60523
Phone: (630) 684-9100
Fax: (630) 684-9120
Website: <http://huffhuff.com>

RECEIVED

SEP 23 2010

Preservation Services

IHPA REVIEW

H/A _____
AC _____
AR _____
File _____

September 17, 2010

Ms. Anne Haaker
Illinois Historic Preservation Agency
Preservation Services Division
One Old State Capital
Springfield, IL 62701

**Re: Agency project review
Metra-Fox River Bridge Replacement
Milwaukee District West Line
Elgin, Illinois
T 41N, R 8E, Section 24**

Dear Ms. Haaker:

Metra plans to replace the bridge under U.S. Route 20 that spans the Fox River in Elgin, Kane County, Illinois (Township 41 North, Range 8 East, Section 24). This bridge, on the Metra Milwaukee District West Line service, is nearing the end of its useful life; it has reached the point where further repairs would not be economically feasible. Therefore, replacement of the bridge is required.

The bridge was constructed in 1881, consisting of six steel spans resting on masonry abutments and piers. Three of the original spans were replaced in 1905, and the other three were replaced in 1926. The piers and abutments date from the original 1881 construction, with cast-in-place concrete modifications as required (to accommodate the new girders).

The bridge replacement will occur within and over the Fox River, and proposed track alignment will be adjusted to meet existing track alignment within approximately 500 to 600 feet on both sides of the river. The activities will occur between the current Metra track and the Union Pacific track. This letter is being forwarded to your office on behalf of Metra to request information on historic or archeological resources within the project limits. For your reference, a coordination request related to this project was submitted to IHPA by Lin Engineering in a letter dated August 17, 2010.

According to the 2010 City of Elgin Zoning Map the adjacent areas to the north, west, south, and east of the site are shown as CF - "Community Facility". There is also area

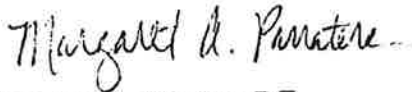
Ms. Anne Haaker, IHPA
September 17, 2010

Page 2

zoned RC3 – Residence Conservation 3” to the northwest and northeast of the project location. At the southeast project limit is a CI – “Commercial Industrial” area.

The work is anticipated to commence in 2011. The project is anticipated to be processed as an Environmental Assessment with the Federal Transit Administration. Enclosed please find a copy of the project location map, zoning map, and site photographs for your review. If you have any questions or require additional information, please feel free to call me at (630) 684-4422.

Sincerely,



Margaret A. Panatera, P.E.
Project Engineer

cc: Kate Sullivan, Metra
attachments



Illinois Department of Natural Resources

One Natural Resources Way Springfield, Illinois 62702-1271
<http://dnr.state.il.us>

Pat Quinn, Governor
Marc Miller, Director

May 12, 2010

Sam Lahniers
Lin Engineering, Ltd.
210 West Chestnut Street
Chatham, IL 62629

Re: Metra Bridge Z-100
Project Number(s): 1009275 [0517-8]
County: Kane

Dear Applicant:

This letter is in reference to the project you recently submitted for consultation. The natural resource review provided by EcoCAT identified protected resources that may be in the vicinity of the proposed action. The Department has evaluated this information and concluded that adverse effects are unlikely. Therefore, consultation under 17 Ill. Adm. Code Part 1075 is terminated.

This consultation is valid for two years unless new information becomes available that was not previously considered; the proposed action is modified; or additional species, essential habitat, or Natural Areas are identified in the vicinity. If the project has not been implemented within two years of the date of this letter, or any of the above listed conditions develop, a new consultation is necessary.

The natural resource review reflects the information existing in the Illinois Natural Heritage Database at the time of the project submittal, and should not be regarded as a final statement on the site being considered, nor should it be a substitute for detailed site surveys or field surveys required for environmental assessments. If additional protected resources are encountered during the project's implementation, you must comply with the applicable statutes and regulations. Also, note that termination does not imply IDNR's authorization or endorsement of the proposed action.

Please contact me if you have questions regarding this review.

Steve Hamer
Division of Ecosystems and Environment
217-785-5500

Illinois Natural Heritage Database Endangered /Threatened Species Occurrence and Sighting Report Form											
Name of Species:		Spike mussel (<i>Elliptio dilatata</i>)				Date Observed:		Aug 25, 2010			
New Sighting	<input checked="" type="checkbox"/>	or Update	Entire extent of occurrence is:				known OR	<input checked="" type="checkbox"/>	not known		
Naturally Occurring	<input checked="" type="checkbox"/>	or	Introduced Location		When?		From Where?				
Location: (For more accurate mapping, please provide a map showing the exact location) Along east bank of Fox River south of Elgin.											
County:	Kane		Latitude	42.019463		Longitude	-88.275177				
Direction from Nearest Landmark:			Approximately 279 feet south of US Route 20 Bridge over Fox River.								
On east bank of the Fox River, under the Metra Milwaukee District line bridge over the Fox River.											
Of two railroad bridges here, the Milwaukee District line is the east bridge.											
Natural Division and Section:											
Legal Description:	Township	41N	Range	8E	Section	24	Quad name	Elgin			
INAI Site Name:	N/A				Survey Site Name (alias)	N/A					
Observations : (evidence of breeding or # of ♂, ♀ & juvenile animals or # fruiting/flowering/seedling plants, etc.): fruiting/flowering/seedling plants											
One individual spike mussel located on the east shoreline/bank of the Fox River. Approximately 15 years old based on shell.											
Description of Area:		On the outside bend of the Fox River south of Elgin and underneath the Metra Bridge over the Fox River. Degraded area consisting of large boulders and riprap used to armor the river bank on the outside of the bend.									
Comments:		Spike mussel was found at water's edge. No other live mussels were noted. Mussel was only partially within the water. After photographs taken, mussel was placed back in river within the boulder riprap area in approximately 18 inches of water. At the time, identification was not known until verified by R. Klocek.									
Specimen/voucher #(s):		Photos attached				Where deposited?	Within 3 feet of finding.				
Name of Observer:		Jim Novak, Senior Scientist, Huff & Huff, verified by Roger Klocek, Senior Scientist Huff & Huff									
Observer's Phone Number	(630)	684	-	4411	jnovak@huffnhuff.com				
Return to: Illinois Natural Heritage Database Program Manager, Illinois Department of Natural Resources, One Natural Resources Way, Springfield IL 62702-1271											
											Rev 11/07



Applicant: Lin Engineering, Ltd.
Contact: Sam Lahniars
Address: 210 West Chestnut Street
Chatham, IL 62829

IDNR Project #: 1009275
Alternate #: 0617-8
Date: 05/11/2010

Project: Metra Bridge Z-100
Address: US 20 over Fox River, Elgin

Description: Metra bridge over Fox River in Elgin, IL to be replaced and removed.

Natural Resource Review Results

Consultation for Endangered Species Protection and Natural Areas Preservation (Part 1075)

The Illinois Natural Heritage Database shows the following protected resources may be in the vicinity of the project location:

Bluff Spring Fen INAI Site
Bluff Spring Fen Nature Preserve
Black-Crowned Night Heron (*Nycticorax nycticorax*)
Elfin Skimmer (*Nannothemis bella*)
Osprey (*Pandion haliaetus*)

An IDNR staff member will evaluate this information and contact you within 30 days to request additional information or to terminate consultation if adverse effects are unlikely.

Location

The applicant is responsible for the accuracy of the location submitted for the project.

County: Kane

Township, Range, Section:
41N, 8E, 24



IL Department of Natural Resources Contact
Keith Shank
217-785-5500
Division of Ecosystems & Environment

Local or State Government Jurisdiction
IL Department of Natural Resources
Steve Hamer
One Natural Resources Way
Springfield, Illinois 62702-1271

Disclaimer

The Illinois Natural Heritage Database cannot provide a conclusive statement on the presence, absence, or condition of natural resources in Illinois. This review reflects the information existing in the Database at the time of this inquiry, and should not be regarded as a final statement on the site being considered, nor should it be a substitute for detailed site surveys or field surveys required for environmental assessments. If additional protected resources are encountered during the project's implementation, compliance with applicable statutes and regulations is required.

Terms of Use

By using this website, you acknowledge that you have read and agree to these terms. These terms may be revised by IDNR as necessary. If you continue to use the EcoCAT application after we post changes to these terms, it will mean that you accept such changes. If at any time you do not accept the Terms of Use, you may not continue to use the website.

1. The IDNR EcoCAT website was developed so that units of local government, state agencies and the public could request information or begin natural resource consultations on-line for the Illinois Endangered Species Protection Act, Illinois Natural Areas Preservation Act, and Illinois Interagency Wetland Policy Act. EcoCAT uses databases, Geographic Information System mapping, and a set of programmed decision rules to determine if proposed actions are in the vicinity of protected natural resources. By indicating your agreement to the Terms of Use for this application, you warrant that you will not use this web site for any other purpose.
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EcoCAT operates on a state of Illinois computer system. We may use software to monitor traffic and to identify unauthorized attempts to upload, download, or change information, to cause harm or otherwise to damage this site. Unauthorized attempts to upload, download, or change information on this server is strictly prohibited by law. Unauthorized use, tampering with or modification of this system, including supporting hardware or software, may subject the violator to criminal and civil penalties. In the event of unauthorized intrusion, all relevant information regarding possible violation of law may be provided to law enforcement officials.

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EcoCAT generates a public record subject to disclosure under the Freedom of Information Act. Otherwise, IDNR uses the information submitted to EcoCAT solely for internal tracking purposes.

Appendix D Noise Analysis Memorandum



**Fox River Bridge Noise Analysis
Noise Evaluation Summary
August 22, 2011**

Project Summary

Metra is proposing a new bridge over the Fox River in Elgin, Illinois. The proposed improvements include the addition of approximately 0.3 miles of track adjacent to the existing bridge over the Fox River. The new bridge will be located approximately 12 feet to the west of the existing bridge.

The general land use in the area is recreational (park) and residential north of US Route 20 and light industrial south of US Route 20. As part of the environmental analysis for the proposed improvements, potential train noise impacts were evaluated for the park and residences and vibration impacts were analyzed at one residence. The FTA *Transit Noise and Vibration Impact Assessment* methodology (May 2006) developed for train traffic evaluations was used for the analysis. Figure 1 depicts the receptor locations.

Noise Screening Assessment

Based on the FTA *Transit Noise and Vibration Impact Assessment* methodology (May 2006), the noise screening distance for the project is 750 feet. As residential units are within the screening distance, a General Assessment was conducted.

General Noise Assessment

- Method: Existing overall noise levels, overall build noise levels were predicted using the FTA *Transit Noise and Vibration Impact Assessment* methodology (May 2006). The noise levels were then compared to the FTA noise impact criteria.
- Receptor Selection: A receptor is typically representative of an area, a group, or cluster of noise sensitive receptors, such as residences. Three receptor locations have been identified for this project (R1 through R3). These receptor locations include two single-family residences (R1 and R3) and a park (R2). The noise metrics used for the evaluation include the L_{dn} (R1 and R3) and the L_{eq} (R2).
- Existing Background Noise Levels: Noise monitoring was conducted March 21, 2011 at the three receptor locations along the proposed project corridor. One-hour monitoring sessions were conducted to measure the $L_{eq}(h)$. The three monitoring results indicated $L_{eq}(h)$ readings of 52 dB(A) at R1, 62 dB(A) at R2, and 60 dB(A) at R3. The background noise monitoring data was adjusted to include all noise sources except the freight and passenger train noise. Based on the FTA *Transit Noise and Vibration Impact Assessment* methodology (May 2006), the estimated background L_{dn} noise level for receptors R1 and R3 are 50 dB(A) and 58 dB(A), respectively. No

adjustment factor was needed for the measurement at R2 as it is evaluated using the L_{eq} . The main noise sources observed during the monitoring was vehicular traffic along US Route 20.

- **General Assessment Spreadsheet Input Parameters and Results:**

Based on the General Assessment analysis, the predicted existing and build train noise level at three receptor locations are 70 dB(A) at R1, 62 dB(A) at R2, and 58 dB(A) at R3. The train volume information anticipates seven freight trains and 54 passenger trains. The predicted noise level is based on both the freight train and passenger train volumes using the input parameters listed in Table 1.

**TABLE 1
 NOISE ANALYSIS INPUT PARAMETERS**

Input Parameter	Input Value		
	R1	R2	R3
Distance between existing track and receptor, ft (Freight/Commuter Track 1/Commuter Track 2)	68/163/175	580/670/682	558/478/NE
Distance between proposed track and receptor, ft (Freight/Commuter Track 1/Commuter Track 2)	68/163/175	580/658	558/478/490
Noise sources	Freight locomotives and railcars; and passenger locomotive and railcars		
Freight daytime train volume (7 am to 10 pm)	3 trains / 15 hours = 0.2 trains/hr		
Freight nighttime train volume (10 pm to 7 am)	4 trains / 9 hours = 0.4 trains/hr		
Passenger daytime train volume (7 am to 10 pm)	41 trains / 15 hours = 2.7 trains/hr		
Passenger nighttime train volume (10 pm to 7 am)	13 trains / 9 hours = 1.4 trains/hr		
Average speed (freight/passenger)	40 mph / 55 mph		
Average freight train locomotives per train (daytime/nighttime)	1.7 / 2.4		
Average passenger locomotives per train (daytime/nighttime)	1.0 / 1.0		
Freight train railcar length (daytime/nighttime)	2438 feet / 4875 feet		
Average number of passenger cars (daytime/nighttime)	6.6 / 7.2		
Railcar wheel flats (freight/passenger)	1% / 0%		
Jointed track	No		

NE: Second commuter track is "Not Existing" in existing condition

Potential Noise Impacts: The build condition noise impacts were evaluated for the first row receptor locations. The background noise level is based on the monitoring at each location. The build train noise level is 70 dB(A) at R1, 62 dB(A) at R2, and 58 dB(A) at R3. These noise levels result in a projected overall build noise level of 70 dB(A), 65 dB(A), and 61 dB(A), respectively. The overall build noise level includes the background noise, the freight train noise and the passenger train noise. The projected overall build noise levels do not change from the existing overall noise levels at any of the receptor locations. Consequently, there are no noise impacts associated with the proposed improvement. The results of the general analysis are detailed in Table 2.

**TABLE 2
 NOISE ANALYSIS RESULTS**

Receptor Location	Receptor Type	Noise Metric	Adjusted Background Noise, dB(A)	Existing Train Noise Level, dB(A) ⁽¹⁾	Build Train Noise Level, dB(A) ⁽¹⁾	Overall Existing Noise Level, dB(A) ⁽²⁾	Overall Build Noise Level, dB(A) ⁽²⁾	Overall Build Noise Increase over Existing Noise Level, dB(A) ⁽²⁾	Allowable Noise Level Increase (Mod./Sev.)	Impact Assessed
R1	Single-Family Residence	Ldn	50	70	70	70	70	0	1/3	No Impact
R2	Park	Leq	62	62	62	65	65	0	3/7	No Impact
R3	Single-Family Residence	Ldn	58	58	58	61	61	0	2/5	No Impact

⁽¹⁾ Includes both freight train and passenger train noise.

⁽²⁾ Includes background noise, freight train noise, and passenger train noise.

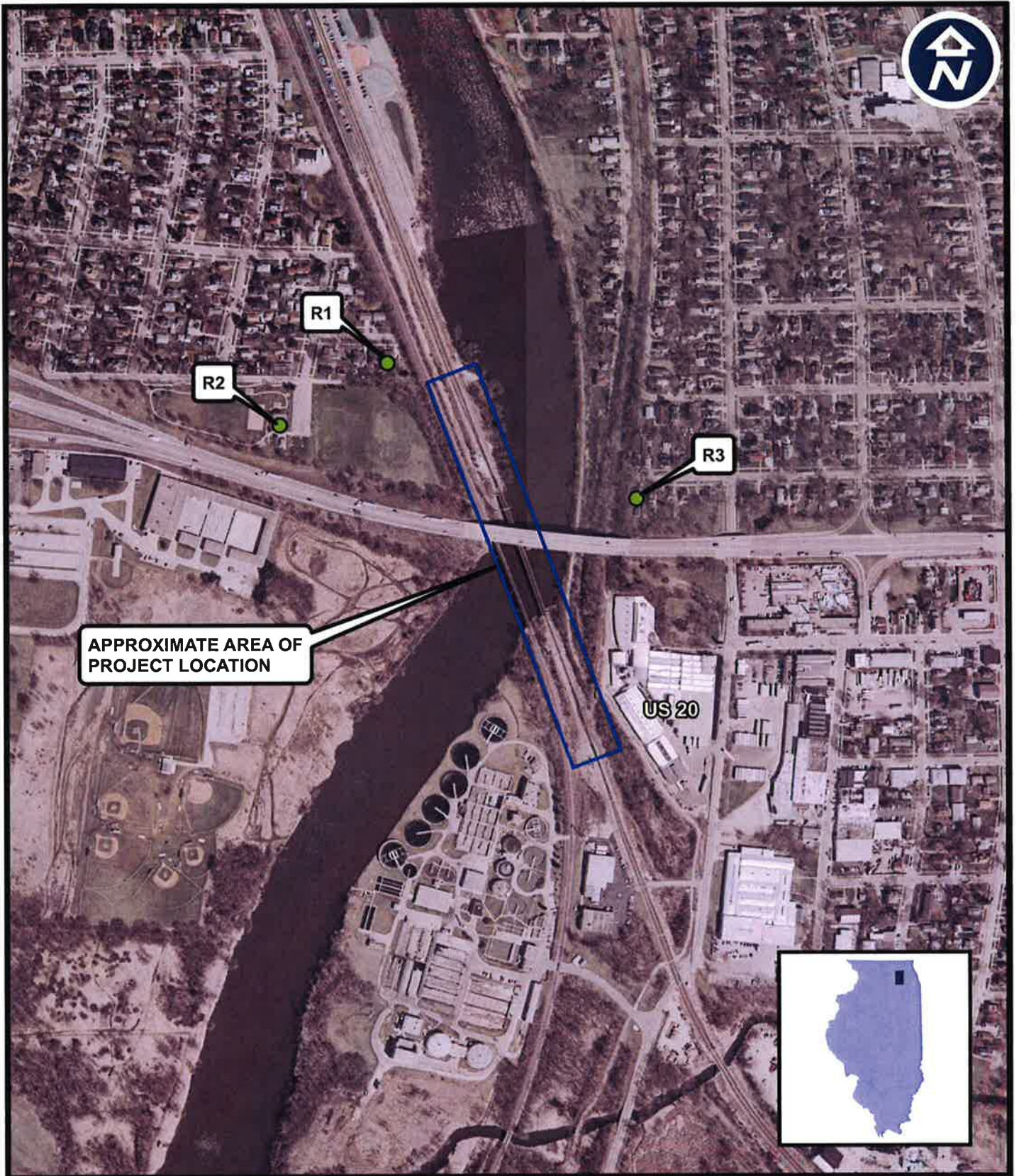
General Vibration Assessment

- **Method:** Existing vibration levels and build vibration levels were predicted using the *FTA Transit Noise and Vibration Impact Assessment* methodology (May 2006).
- **Receptor Selection:** Receptor R1 was analyzed for vibration impacts as it is the only vibration sensitive receptor within the FTA vibration screening distance (200 ft).
- **General Assessment Spreadsheet Input Parameters and Results:**
 The vibration analysis is based on the input parameters listed in Table 3.

**TABLE 3
 VIBRATION ANALYSIS INPUT PARAMETERS (R1 ANALYSIS)**

Input Parameter	Input Value
Distance between existing track and receptor, ft (Freight Track/Closest Commuter Track)	75/170
Distance between proposed track and receptor, ft (Freight Track/Closest Commuter Track)	75/170
Vibration sources	Locomotives, freight railcars, and passenger railcars
Total Existing Volume, per day	61
Total Predicted Volume, per day	61
Event Frequency	Occasional / Locomotives Frequent / Rail Car
Average speed (freight/passenger)	40 mph / 55 mph
Railcar wheel flats (freight/passenger)	Yes / No
Jointed track	No

Potential Vibration Impacts: There are no vibration impacts associated with the project at receptor R1. This includes both ground-borne vibration and ground-borne noise.



APPROXIMATE AREA OF PROJECT LOCATION

US 20

FIGURE 1
NOISE RECEPTOR LOCATION MAP
FOX RIVER BRIDGE REPLACEMENT
ELGIN, KANE COUNTY, ILLINOIS

SOURCE: U.S. DEPARTMENT OF THE INTERIOR
 U.S. GEOLOGICAL SURVEY
 ELGIN QUADRANGLE

Legend

- NOISE RECEPTOR
- PROJECT LOCATION

500



Feet



CADFILE: WIGHT\METRA\FIGURES

Noise Model Based on Federal Transit Administration General Transit Noise Assessment
 Developed for Chicago Create Project
 Copyright 2006, HMMH Inc.

Case: Existing - Receptor - R1
 Project: Fox River Bridge

RESULTS			
Noise Source	Ldn (dB)	Leq - daytime (dB)	Leq - nighttime (dB)
All Sources	70	60	64
Source 1	65	55	59
Source 2	68	56	62
Source 3	54	50	47
Source 4	53	49	47
Source 5	53	49	46
Source 6	53	49	46
Source 7	0	0	0
Source 8	0	0	0

Source 1/2 = Freight
 Source 3/4 = Metra

Enter noise receiver land use category below.

LAND USE CATEGORY	
Noise receiver land use category (1, 2 or 3)	2

Enter data for up to 8 noise sources below - see reference list for source numbers.

NOISE SOURCE PARAMETERS													
Parameter	Source 1	Source 2	Source 3	Source 4	Source 5	Source 6	Source 7	Source 8					
Source Num.	Freight Locomotive	9	Freight Cars	10	Commuter Diesel Locomotive	2	Commuter Rail Cars	3	Commuter Diesel Locomotive	2	Commuter Rail Cars	3	
Distance (source to receiver)	distance (ft)	68	distance (ft)	68	distance (ft)	163	distance (ft)	163	distance (ft)	175	distance (ft)	175	
Daytime Hours (7 AM - 10 PM)	speed (mph)	40	speed (mph)	40	speed (mph)	55	speed (mph)	55	speed (mph)	55	speed (mph)	55	
	trains/hour	0.2	trains/hour	0.2	trains/hour	1.35	trains/hour	1.35	trains/hour	1.35	trains/hour	1.35	
	locos/train	1.7	length of cars (ft) / train	2438	locos/train	1	cars/train	6.64	locos/train	1	cars/train	6.64	
Nighttime Hours (10 PM - 7 AM)	speed (mph)	40	speed (mph)	40	speed (mph)	55	speed (mph)	55	speed (mph)	55	speed (mph)	55	
	trains/hour	0.4	trains/hour	0.4	trains/hour	0.7	trains/hour	0.7	trains/hour	0.7	trains/hour	0.7	
	locos/train	2.5	length of cars (ft) / train	4875	locos/train	1	cars/train	7.226667	locos/train	1	cars/train	7.226667	
Wheel Flats?		% of cars w/ wheel flats	1.00%		% of cars w/ wheel flats	0.00%		% of cars w/ wheel flats	0.00%		% of cars w/ wheel flats	0.00%	
Jointed Track?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	
Embedded Track?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	
Aerial Structure?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	
Barrier Present?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	
Intervening Rows of Buildings	number of rows	0	number of rows	0	number of rows	0	number of rows	0	number of rows	0	number of rows	0	

SOURCE REFERENCE LIST	
Source	Number
Commuter Electric Locomotive	1
Commuter Diesel Locomotive	2
Commuter Rail Cars	3
RRT/LRT	4
AGT, Steel Wheel	5
AGT, Rubber Tire	6
Monorail	7
Maglev	8
Freight Locomotive	9
Freight Cars	10
Hopper Cars (empty)	11
Hopper Cars (full)	12
Crossover	13
Automobiles	14
City Buses	15
Commuter Buses	16
Rail Yard or Shop	17
Layover Tracks	18
Bus Storage Yard	19
Bus Op. Facility	20
Bus Transit Center	21
Parking Garage	22
Park & Ride Lot	23

Noise Model Based on Federal Transit Administration General Transit Noise Assessment
 Developed for Chicago Create Project
 Copyright 2006, HMMH Inc.

Case: No Build - Receptor - R1
 Project: Fox River Bridge

RESULTS			
Noise Source	Ldn (dB)	Leq - daytime (dB)	Leq - nighttime (dB)
All Sources	70	60	64
Source 1	65	55	59
Source 2	68	56	62
Source 3	54	50	47
Source 4	53	49	47
Source 5	53	49	46
Source 6	53	49	46
Source 7	0	0	0
Source 8	0	0	0

Source 1/2 = Freight
 Source 3/4 = Metra

Enter noise receiver land use category below.

LAND USE CATEGORY
Noise receiver land use category (1, 2 or 3) 2

Enter data for up to 8 noise sources below - see reference list for source numbers.

NOISE SOURCE PARAMETERS														
Parameter	Source 1		Source 2		Source 3		Source 4		Source 5		Source 6		Source 7	Source 8
Source Num.	Freight Locomotive	9	Freight Cars	10	Commuter Diesel Locomotive	2	Commuter Rail Cars	3	Commuter Diesel Locomotive	2	Commuter Rail Cars	3		
Distance (source to receiver)	distance (ft)	68	distance (ft)	68	distance (ft)	163	distance (ft)	163	distance (ft)	175	distance (ft)	175		
Daytime Hours (7 AM - 10 PM)	speed (mph)	40	speed (mph)	40	speed (mph)	55	speed (mph)	55	speed (mph)	55	speed (mph)	55		
	trains/hour	0.2	trains/hour	0.2	trains/hour	1.35	trains/hour	1.35	trains/hour	1.35	trains/hour	1.35		
	locos/train	1.7	length of cars (ft) / train	2438	locos/train	1	cars/train	6.64	locos/train	1	cars/train	6.64		
Nighttime Hours (10 PM - 7 AM)	speed (mph)	40	speed (mph)	40	speed (mph)	55	speed (mph)	55	speed (mph)	55	speed (mph)	55		
	trains/hour	0.4	trains/hour	0.4	trains/hour	0.7	trains/hour	0.7	trains/hour	0.7	trains/hour	0.7		
	locos/train	2.5	length of cars (ft) / train	4875	locos/train	1	cars/train	7.226667	locos/train	1	cars/train	7.22667		
Wheel Flats?		% of cars w/ wheel flats	1.00%		% of cars w/ wheel flats	0.00%		% of cars w/ wheel flats	0.00%		% of cars w/ wheel flats	0.00%		
Jointed Track?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N			
Embedded Track?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N			
Aerial Structure?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N			
Barrier Present?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N			
Intervening Rows of Buildings	number of rows	0	number of rows	0	number of rows	0	number of rows	0	number of rows	0	number of rows	0		

SOURCE REFERENCE LIST	
Source	Number
Commuter Electric Locomotive	1
Commuter Diesel Locomotive	2
Commuter Rail Cars	3
RRT/LRT	4
AGT, Steel Wheel	5
AGT, Rubber Tire	6
Monorail	7
Maglev	8
Freight Locomotive	9
Freight Cars	10
Hopper Cars (empty)	11
Hopper Cars (full)	12
Crossover	13
Automobiles	14
City Buses	15
Commuter Buses	16
Rail Yard or Shop	17
Layover Tracks	18
Bus Storage Yard	19
Bus Op. Facility	20
Bus Transit Center	21
Parking Garage	22
Park & Ride Lot	23

Noise Model Based on Federal Transit Administration General Transit Noise Assessment
 Developed for Chicago Create Project
 Copyright 2006, HMMH Inc.

Case: Build - Receptor - R1
 Project: Fox River Bridge

RESULTS			
Noise Source	Ldn (dB)	Leq - daytime (dB)	Leq - nighttime (dB)
All Sources	70	60	64
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Source 5	53	49	46
Source 6	53	49	46
Source 7	0	0	0
Source 8	0	0	0

Source 1/2 = Freight
 Source 3/4 = Metra

Enter noise receiver land use category below.

LAND USE CATEGORY
Noise receiver land use category (1, 2 or 3) 2

Enter data for up to 8 noise sources below - see reference list for source numbers.

NOISE SOURCE PARAMETERS														
Parameter	Source 1		Source 2		Source 3		Source 4		Source 5		Source 6		Source 7	Source 8
Source Num.	Freight Locomotive	9	Freight Cars	10	Commuter Diesel Locomotive	2	Commuter Rail Cars	3	Commuter Diesel Locomotive	2	Commuter Rail Cars	3		
Distance (source to receiver)	distance (ft)	68	distance (ft)	68	distance (ft)	163	distance (ft)	163	distance (ft)	175	distance (ft)	175		
Daytime Hours (7 AM - 10 PM)	speed (mph)	40	speed (mph)	40	speed (mph)	55	speed (mph)	55	speed (mph)	55	speed (mph)	55		
	trains/hour	0.2	trains/hour	0.2	trains/hour	1.35	trains/hour	1.35	trains/hour	1.35	trains/hour	1.35		
	locos/train	1.7	length of cars (ft) / train	2438	locos/train	1	cars/train	6.64	locos/train	1	cars/train	6.64		
Nighttime Hours (10 PM - 7 AM)	speed (mph)	40	speed (mph)	40	speed (mph)	55	speed (mph)	55	speed (mph)	55	speed (mph)	55		
	trains/hour	0.4	trains/hour	0.4	trains/hour	0.7	trains/hour	0.7	trains/hour	0.7	trains/hour	0.7		
	locos/train	2.5	length of cars (ft) / train	4875	locos/train	1	cars/train	7.226667	locos/train	1	cars/train	7.226667		
Wheel Flats?		% of cars w/ wheel flats	1.00%		% of cars w/ wheel flats	0.00%		% of cars w/ wheel flats	0.00%		% of cars w/ wheel flats	0.00%		
Jointed Track?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N			
Embedded Track?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N			
Aerial Structure?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N			
Barrier Present?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N			
Intervening Rows of Buildings	number of rows	0	number of rows	0	number of rows	0	number of rows	0	number of rows	0	number of rows	0		

SOURCE REFERENCE LIST	
Source	Number
Commuter Electric Locomotive	1
Commuter Diesel Locomotive	2
Commuter Rail Cars	3
RRT/LRT	4
AGT, Steel Wheel	5
AGT, Rubber Tire	6
Monorail	7
Maglev	8
Freight Locomotive	9
Freight Cars	10
Hopper Cars (empty)	11
Hopper Cars (full)	12
Crossover	13
Automobiles	14
City Buses	15
Commuter Buses	16
Rail Yard or Shop	17
Layover Tracks	18
Bus Storage Yard	19
Bus Op. Facility	20
Bus Transit Center	21
Parking Garage	22
Park & Ride Lot	23

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Case: Existing - Receptor - R2 1:00 PM
 Project: Fox River Bridge

Source 1/2 = Freight
 Source 3/4 = Metra

RESULTS	
Noise Source	Leq - 1-hr (dB)
All Sources	62
Source 1	51
Source 2	53
Source 3	29
Source 4	58
Source 5	29
Source 6	58
Source 7	0
Source 8	0

Enter noise receiver land use category below.

LAND USE CATEGORY	
Noise receiver land use category (1, 2 or 3)	3

Enter data for up to 8 noise sources below - see reference list for source numbers.

NOISE SOURCE PARAMETERS													
Parameter	Source 1	Source 2	Source 3	Source 4	Source 5	Source 6	Source 7	Source 8					
Source Num.	Freight Locomotive	9	Freight Cars	10	Commuter Diesel Locomotive	2	Commuter Rail Cars	3	Commuter Diesel Locomotive	2	Commuter Rail Cars	3	
Distance (source to receiver)	distance (ft)	580	distance (ft)	580	distance (ft)	670	distance (ft)	670	distance (ft)	682	distance (ft)	682	
Noisiest Hour of Activity During	speed (mph)	40	speed (mph)	40	speed (mph)	585	speed (mph)	585	speed (mph)	585	speed (mph)	585	
Sensitive Hours	trains/hour	3	trains/hour	3	trains/hour	1	trains/hour	1	trains/hour	1	trains/hour	1	
	locos/train	1.3	length of cars (ft) / train	2100	locos/train	1	cars/train	5	locos/train	1	cars/train	5	
Wheel Flats?			% of cars w/ wheel flats	1.00%			% of cars w/ wheel flats	0.00%			% of cars w/ wheel flats	0.00%	
Jointed Track?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N		
Embedded Track?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N		
Aerial Structure?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N		
Barrier Present?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N		
Intervening Rows of Buildings	number of rows	0	number of rows	0	number of rows	0	number of rows	0	number of rows	0	number of rows	0	

SOURCE REFERENCE LIST	
Source	Number
Commuter Electric Locomotive	1
Commuter Diesel Locomotive	2
Commuter Rail Cars	3
RRT/LRT	4
AGT, Steel Wheel	5
AGT, Rubber Tire	6
Monorail	7
Maglev	8
Freight Locomotive	9
Freight Cars	10
Hopper Cars (empty)	11
Hopper Cars (full)	12
Crossover	13
Automobiles	14
City Buses	15
Commuter Buses	16
Rail Yard or Shop	17
Layover Tracks	18
Bus Storage Yard	19
Bus Op. Facility	20
Bus Transit Center	21
Parking Garage	22
Park & Ride Lot	23

Noise Model Based on Federal Transit Administration General Transit Noise Assessment
 Developed for Chicago Create Project
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Case: No Build - Receptor - R2 1:00 PM
 Project: Fox River Bridge

RESULTS	Leq - 1-hr (dB)		
Noise Source			
All Sources	62		
Source 1	51		
Source 2	53		
Source 3	29		
Source 4	58		
Source 5	29		
Source 6	58		
Source 7	0		
Source 8	0		

Source 1/2 = Freight
 Source 3/4 = Metra

Enter noise receiver land use category below.

LAND USE CATEGORY
Noise receiver land use category (1, 2 or 3) 3

Enter data for up to 8 noise sources below - see reference list for source numbers.

NOISE SOURCE PARAMETERS												
Parameter	Source 1	Source 2	Source 3	Source 4	Source 5	Source 6	Source 7	Source 8				
Source Num.	Freight Locomotive 9	Freight Cars 10	Commuter Diesel Locomotive 2	Commuter Rail Cars 3	Commuter Diesel Locomotive 2	Commuter Rail Cars 3						
Distance (source to receiver)	distance (ft) 580	distance (ft) 580	distance (ft) 670	distance (ft) 670	distance (ft) 682	distance (ft) 682						
Noisiest Hour of Activity During Sensitive Hours	speed (mph) 40 trains/hour 3 locos/train 1.3	speed (mph) 40 trains/hour 3 length of cars (ft) / train 2100	speed (mph) 585 trains/hour 1 locos/train 1	speed (mph) 585 trains/hour 1 cars/train 5	speed (mph) 585 trains/hour 1 locos/train 1	speed (mph) 585 trains/hour 1 cars/train 1	speed (mph) 585 trains/hour 1 cars/train 5					
Wheel Flats?		% of cars w/ wheel flats 1.00%		% of cars w/ wheel flats 0.00%		% of cars w/ wheel flats 0.00%						
Jointed Track?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N						
Embedded Track?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N						
Aerial Structure?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N						
Barrier Present?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N						
Intervening Rows of Buildings	number of rows 0	number of rows 0	number of rows 0	number of rows 0	number of rows 0	number of rows 0						

SOURCE REFERENCE LIST	
Source	Number
Commuter Electric Locomotive	1
Commuter Diesel Locomotive	2
Commuter Rail Cars	3
RRT/LRT	4
AGT, Steel Wheel	5
AGT, Rubber Tire	6
Monorail	7
Maglev	8
Freight Locomotive	9
Freight Cars	10
Hopper Cars (empty)	11
Hopper Cars (full)	12
Crossover	13
Automobiles	14
City Buses	15
Commuter Buses	16
Rail Yard or Shop	17
Layover Tracks	18
Bus Storage Yard	19
Bus Op. Facility	20
Bus Transit Center	21
Parking Garage	22
Park & Ride Lot	23

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Case: Build - Receptor - R2 1:00 PM
 Project: Fox River Bridge

RESULTS	Leq - 1-hr (dB)		
Noise Source			
All Sources	62		
Source 1	51		
Source 2	53		
Source 3	29		
Source 4	58		
Source 5	29		
Source 6	58		
Source 7	0		
Source 8	0		

Source 1/2 = Freight
 Source 3/4 = Metra

Enter noise receiver land use category below.

LAND USE CATEGORY
Noise receiver land use category (1, 2 or 3) 3

Enter data for up to 8 noise sources below - see reference list for source numbers.

NOISE SOURCE PARAMETERS												
Parameter	Source 1	Source 2	Source 3	Source 4	Source 5	Source 6	Source 7	Source 8				
Source Num.	Freight Locomotive 9	Freight Cars 10	Commuter Diesel Locomotive 2	Commuter Rail Cars 3	Commuter Diesel Locomotive 2	Commuter Rail Cars 3						
Distance (source to receiver)	distance (ft) 580	distance (ft) 580	distance (ft) 658	distance (ft) 658	distance (ft) 670	distance (ft) 670						
Noisiest Hour of Activity During Sensitive Hours	speed (mph) 40 trains/hour 3 locos/train 1.3	speed (mph) 40 trains/hour 3 length of cars (ft) / train 2100	speed (mph) 585 trains/hour 1 locos/train 1	speed (mph) 585 trains/hour 1 cars/train 5	speed (mph) 585 trains/hour 1 locos/train 1	speed (mph) 585 trains/hour 1 cars/train 1	speed (mph) 585 trains/hour 1 cars/train 5					
Wheel Flats?		% of cars w/ wheel flats 1.00%		% of cars w/ wheel flats 0.00%		% of cars w/ wheel flats 0.00%						
Jointed Track?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N						
Embedded Track?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N						
Aerial Structure?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N						
Barrier Present?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N						
Intervening Rows of Buildings	number of rows 0	number of rows 0	number of rows 0	number of rows 0	number of rows 0	number of rows 0						

SOURCE REFERENCE LIST	
Source	Number
Commuter Electric Locomotive	1
Commuter Diesel Locomotive	2
Commuter Rail Cars	3
RRT/LRT	4
AGT, Steel Wheel	5
AGT, Rubber Tire	6
Monorail	7
Maglev	8
Freight Locomotive	9
Freight Cars	10
Hopper Cars (empty)	11
Hopper Cars (full)	12
Crossover	13
Automobiles	14
City Buses	15
Commuter Buses	16
Rail Yard or Shop	17
Layover Tracks	18
Bus Storage Yard	19
Bus Op. Facility	20
Bus Transit Center	21
Parking Garage	22
Park & Ride Lot	23

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Noise Model Based on Federal Transit Administration General Transit Noise Assessment
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Case: Existing - Receptor - R3
 Project: Fox River Bridge

RESULTS			
Noise Source	Ldn (dB)	Leq - daytime (dB)	Leq - nighttime (dB)
All Sources	58	50	52
Source 1	52	41	46
Source 2	54	43	49
Source 3	50	46	43
Source 4	49	45	43
Source 5	0	0	0
Source 6	0	0	0
Source 7	0	0	0
Source 8	0	0	0

Source 1/2 = Freight
 Source 3/4 = Metra

Enter noise receiver land use category below.

LAND USE CATEGORY	
Noise receiver land use category (1, 2 or 3)	2

Enter data for up to 8 noise sources below - see reference list for source numbers.

NOISE SOURCE PARAMETERS													
Parameter	Source 1	Source 2	Source 3	Source 4	Source 5	Source 6	Source 7	Source 8					
Source Num.	Freight Locomotive	9	Freight Cars	10	Commuter Diesel Locomotive	2	Commuter Rail Cars	3					
Distance (source to receiver)	distance (ft)	558	distance (ft)	558	distance (ft)	478	distance (ft)	478					
Daytime Hours (7 AM - 10 PM)	speed (mph)	40	speed (mph)	40	speed (mph)	55	speed (mph)	55					
	trains/hour	0.2	trains/hour	0.2	trains/hour	2.7	trains/hour	2.7					
	locos/train	1.7	length of cars (ft) / train	2438	locos/train	1	cars/train	6.64					
Nighttime Hours (10 PM - 7 AM)	speed (mph)	40	speed (mph)	40	speed (mph)	55	speed (mph)	55					
	trains/hour	0.4	trains/hour	0.4	trains/hour	1.4	trains/hour	1.4					
	locos/train	2.5	length of cars (ft) / train	4875	locos/train	1	cars/train	7.226667					
Wheel Flats?		% of cars w/ wheel flats	1.00%		% of cars w/ wheel flats	0.00%							
Jointed Track?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N					
Embedded Track?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N					
Aerial Structure?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N					
Barrier Present?	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N	Y/N					
Intervening Rows of Buildings	number of rows	0	number of rows	0	number of rows	0	number of rows	0					

SOURCE REFERENCE LIST	
Source	Number
Commuter Electric Locomotive	1
Commuter Diesel Locomotive	2
Commuter Rail Cars	3
RRT/LRT	4
AGT, Steel Wheel	5
AGT, Rubber Tire	6
Monorail	7
Maglev	8
Freight Locomotive	9
Freight Cars	10
Hopper Cars (empty)	11
Hopper Cars (full)	12
Crossover	13
Automobiles	14
City Buses	15
Commuter Buses	16
Rail Yard or Shop	17
Layover Tracks	18
Bus Storage Yard	19
Bus Op. Facility	20
Bus Transit Center	21
Parking Garage	22
Park & Ride Lot	23

Noise Model Based on Federal Transit Administration General Transit Noise Assessment
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Case: No Build - Receptor - R3
 Project: Fox River Bridge

RESULTS			
Noise Source	Ldn (dB)	Leq - daytime (dB)	Leq - nighttime (dB)
All Sources	58	50	52
Source 1	52	41	46
Source 2	54	43	49
Source 3	50	46	43
Source 4	49	45	43
Source 5	0	0	0
Source 6	0	0	0
Source 7	0	0	0
Source 8	0	0	0

Source 1/2 = Freight
 Source 3/4 = Metra

Enter noise receiver land use category below.

LAND USE CATEGORY
Noise receiver land use category (1, 2 or 3) 2

Enter data for up to 8 noise sources below - see reference list for source numbers.

NOISE SOURCE PARAMETERS	Source 1	Source 2	Source 3	Source 4	Source 5	Source 6	Source 7	Source 8
Parameter	Source 1	Source 2	Source 3	Source 4	Source 5	Source 6	Source 7	Source 8
Source Num.	Freight Locomotive 9	Freight Cars 10	Commuter Diesel Locomotive 2	Commuter Rail Cars 3				
Distance (source to receiver)	distance (ft) 558	distance (ft) 558	distance (ft) 478	distance (ft) 478				
Daytime Hours (7 AM - 10 PM)	speed (mph) 40	speed (mph) 40	speed (mph) 55	speed (mph) 55				
	trains/hour 0.2	trains/hour 0.2	trains/hour 2.7	trains/hour 2.7				
	locos/train 1.7	length of cars (ft) / train 2438	locos/train 1	cars/train 6.64				
Nighttime Hours (10 PM - 7 AM)	speed (mph) 40	speed (mph) 40	speed (mph) 55	speed (mph) 55				
	trains/hour 0.4	trains/hour 0.4	trains/hour 1.4	trains/hour 1.4				
	locos/train 2.5	length of cars (ft) / train 4875	locos/train 1	cars/train 7.226667				
Wheel Flats?		% of cars w/ wheel flats 1.00%		% of cars w/ wheel flats 0.00%				
Jointed Track?	Y/N	Y/N	Y/N	Y/N				
Embedded Track?	Y/N	Y/N	Y/N	Y/N				
Aerial Structure?	Y/N	Y/N	Y/N	Y/N				
Barrier Present?	Y/N	Y/N	Y/N	Y/N				
Intervening Rows of Buildings	number of rows 0	number of rows 0	number of rows 0	number of rows 0				

SOURCE REFERENCE LIST	Number
Source	
Commuter Electric Locomotive	1
Commuter Diesel Locomotive	2
Commuter Rail Cars	3
RRT/LRT	4
AGT, Steel Wheel	5
AGT, Rubber Tire	6
Monorail	7
Maglev	8
Freight Locomotive	9
Freight Cars	10
Hopper Cars (empty)	11
Hopper Cars (full)	12
Crossover	13
Automobiles	14
City Buses	15
Commuter Buses	16
Rail Yard or Shop	17
Layover Tracks	18
Bus Storage Yard	19
Bus Op. Facility	20
Bus Transit Center	21
Parking Garage	22
Park & Ride Lot	23

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Case: Build - Receptor - R3
 Project: Fox River Bridge

RESULTS			
Noise Source	Ldn (dB)	Leq - daytime (dB)	Leq - nighttime (dB)
All Sources	58	50	52
Source 1	52	41	46
Source 2	54	43	49
Source 3	47	42	40
Source 4	46	42	39
Source 5	47	43	40
Source 6	46	42	40
Source 7	0	0	0
Source 8	0	0	0

Source 1/2 = Freight
 Source 3/4 = Metra

Enter noise receiver land use category below.

LAND USE CATEGORY
Noise receiver land use category (1, 2 or 3) 2

Enter data for up to 8 noise sources below - see reference list for source numbers.

NOISE SOURCE PARAMETERS														
Parameter	Source 1		Source 2		Source 3		Source 4		Source 5		Source 6		Source 7	Source 8
Source Num.	Freight Locomotive	9	Freight Cars	10	Commuter Diesel Locomotive	2	Commuter Rail Cars	3	Commuter Diesel Locomotive	2	Commuter Rail Cars	3		
Distance (source to receiver)	distance (ft)	558	distance (ft)	558	distance (ft)	490	distance (ft)	490	distance (ft)	478	distance (ft)	478		
Daytime Hours (7 AM - 10 PM)	speed (mph)	40	speed (mph)	40	speed (mph)	55	speed (mph)	55	speed (mph)	55	speed (mph)	55		
	trains/hour	0.2	trains/hour	0.2	trains/hour	1.35	trains/hour	1.35	trains/hour	1.35	trains/hour	1.35		
	locos/train	1.7	length of cars (ft) / train	2438	locos/train	1	cars/train	6.64	locos/train	1	cars/train	6.64		
Nighttime Hours (10 PM - 7 AM)	speed (mph)	40	speed (mph)	40	speed (mph)	55	speed (mph)	55	speed (mph)	55	speed (mph)	55		
	trains/hour	0.4	trains/hour	0.4	trains/hour	0.7	trains/hour	0.7	trains/hour	0.7	trains/hour	0.7		
	locos/train	2.5	length of cars (ft) / train	4875	locos/train	1	cars/train	7.226667	locos/train	1	cars/train	7.226667		
Wheel Flats?		% of cars w/ wheel flats	1.00%		% of cars w/ wheel flats	0.00%		% of cars w/ wheel flats	0.00%		% of cars w/ wheel flats	0.00%		
Jointed Track?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N			
Embedded Track?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N			
Aerial Structure?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N			
Barrier Present?	Y/N		Y/N		Y/N		Y/N		Y/N		Y/N			
Intervening Rows of Buildings	number of rows	0	number of rows	0	number of rows	0	number of rows	0	number of rows	0	number of rows	0		

SOURCE REFERENCE LIST	
Source	Number
Commuter Electric Locomotive	1
Commuter Diesel Locomotive	2
Commuter Rail Cars	3
RRT/LRT	4
AGT, Steel Wheel	5
AGT, Rubber Tire	6
Monorail	7
Maglev	8
Freight Locomotive	9
Freight Cars	10
Hopper Cars (empty)	11
Hopper Cars (full)	12
Crossover	13
Automobiles	14
City Buses	15
Commuter Buses	16
Rail Yard or Shop	17
Layover Tracks	18
Bus Storage Yard	19
Bus Op. Facility	20
Bus Transit Center	21
Parking Garage	22
Park & Ride Lot	23

Appendix E Compensatory Storage Plan and Calculations

FLOODWAY COMPENSATION

Summary of Results

	<i>Avg. Ground Elev. to Normal Flow Elev.</i>	<i>Normal Flow Elev. to 10-yr NHWE</i>	<i>10-yr NHWE to 100-yr NHWE</i>
	<i>FT³</i>	<i>FT³</i>	<i>FT³</i>
Ex Pier #1	+842.7	+669.0	+267.4
Ex Pier #2	+1067.5	+669.0	+267.4
Ex Pier #3	+112.1	+667.0	+267.2
Ex Pier #4	+686.3	+662.3	+266.6
Ex Pier #5	+733.6	+660.9	+266.5
Pr Pier #1	-1132.5	-774.4	-320.3
Pr Pier #2	-1214.6	-780.1	-322.8
Pr Pier #3	-683.9	-773.3	-319.8
Sub-Total	411.2	1000.4	372.2
West Abut.			
Sec A-A			-1120.5
Sec B-B		-43.5	-388.5
Appr. Embkt.			-315.5
East Abut.			
Sec C-C		+1110.0	+1295
Sub-Total	0	1066.5	-529.5
Total	+2538		-157.3

NOTES:

1. "-" Represents fill within the floodway.
2. "+" Represents compensation provided within the floodway.
3. 18,730 Ft³ of estimated fill will be placed with the 100-yr Special Flood Hazard Area (along the west approach between the Metra and UPRR bridges) from station 1870+50 to station 1879+50 (see page 10 of 17 for plan).
4. Recommend providing compensation for the 157.3 Ft³ of net fill within the 10-yr to 100-yr floodway by placing retaining wall along western approach from approximately 1870+30 LT. to 1871+00 LT.
5. Please note that the estimated cut and fill volumes for the west and east abutment are based on spot elevation shots (no tin model provided, also no cross-sections provided)

FLOODWAY COMPENSATION (Dual Track Alternate)

Summary of Results

	<i>Avg. Ground Elev. to Normal Flow Elev.</i>	<i>Normal Flow Elev. to 10-yr NHWE</i>	<i>10-yr NHWE to 100-yr NHWE</i>
	<i>FT³</i>	<i>FT³</i>	<i>FT³</i>
Ex Pier #1	+842.7	+669.0	+267.4
Ex Pier #2	+1067.5	+669.0	+267.4
Ex Pier #3	+112.1	+667.0	+267.2
Ex Pier #4	+686.3	+662.3	+266.6
Ex Pier #5	+733.6	+660.9	+266.5
Pr Pier #1	-1941	-1387	-593
Pr Pier #2	-2068	-1390	-597
Pr Pier #3	-1183	-1386	-593
Sub-Total	-1749.8	-834.8	-447.9
West Abut.			
Sec A-A			-952.2
Sec B-B		-1807.2	-1695.6
Appr. Embkt.			-315.5
East Abut.			
Sec C-C		+1710	+1995
Sub-Total	0	-97.2	-968.3
Total		-2681.8	-1416.2

NOTES:

The Dual Track alternate structure is very preliminary and subject to change.

1. "-" Represents fill within the floodway.
2. "+" Represents compensation provided within the floodway.
3. Please note that the estimated cut and fill volumes for the west and east abutment are based on spot elevation shots (no tin model provided, also no cross-sections provided)

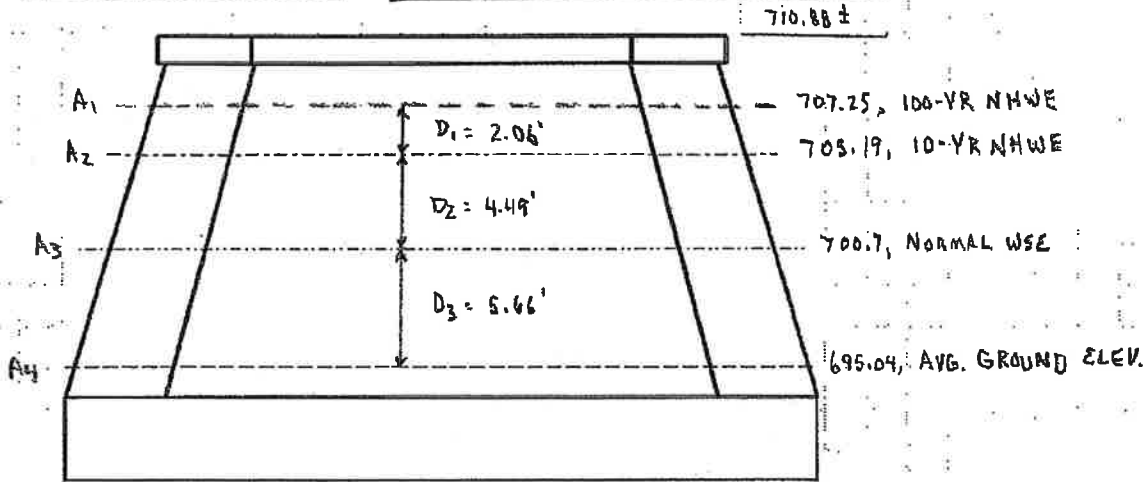


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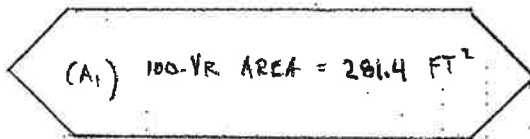
STRUCT. NO. MDW E-100
ROUTE METRA
COUNTY KANE

SHEET NO. OF
DESIGNED SGL DATE 8/03/11
CHECKED DATE

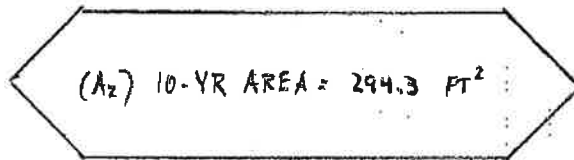
Floodway Compensation (Dual Track Alternate)



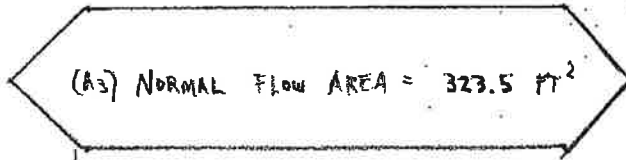
Proposed Pier # 1



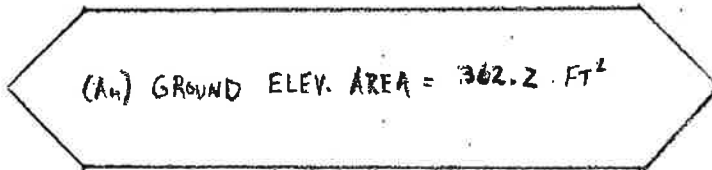
(A₁) 100-YR AREA = 281.4 FT²



(A₂) 10-YR AREA = 294.3 FT²



(A₃) NORMAL FLOW AREA = 323.5 FT²



(A₄) GROUND ELEV. AREA = 362.2 FT²

Volume (10-YR to 100-YR)

$$= (D_1/2)(A_1 + A_2); \frac{(2.06)}{2}(281.4 + 294.3) = 593 \text{ FT}^3$$

Volume (10-YR to Normal Flow)

$$= (D_2/2)(A_2 + A_3); \frac{(4.49)}{2}(294.3 + 323.5) = 1387 \text{ FT}^3$$

Volume (Normal Flow to Ground)

$$= (D_3/2)(A_3 + A_4); \frac{(5.66)}{2}(323.5 + 362.2) = 1941 \text{ FT}^3$$

0-10 YR
(1387 + 1941) FT³
3328 FT³

10-100 YR
= 593 FT³

A AREAS MEASURED IN MICROSTATION
(SCALE DRAWING)

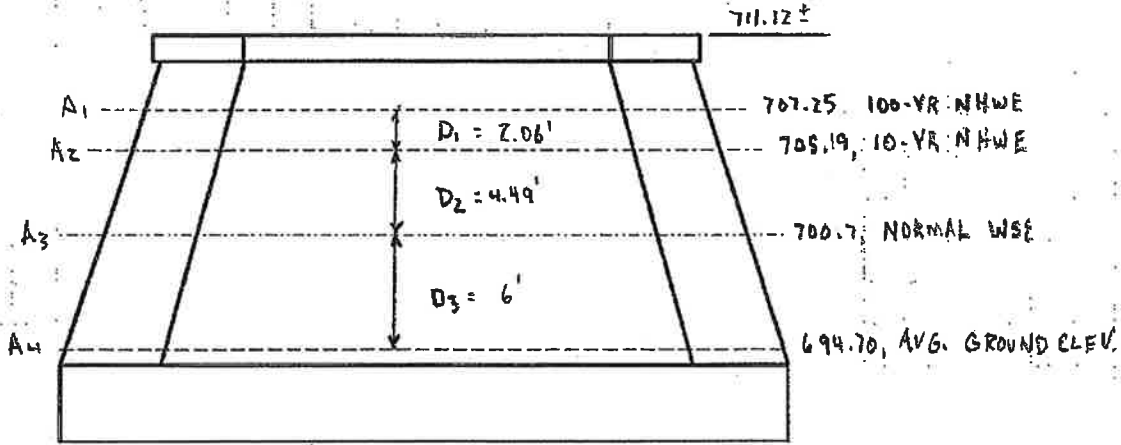


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STRUCT. NO. MDW Z-100
 ROUTE METRA
 COUNTY KANE

SHEET NO. OF
 DESIGNED SGL DATE 8/23/11
 CHECKED DATE

Floodway Compensation (Dual Track Alternate)



Proposed Pier # 2

(A₁) 100-YR AREA = 283.0 FT²

Volume (10-YR to 100 YR)

$$= (D_1/2)(A_1 + A_2); (2.06/2)(283 + 296) = 596.3 \text{ FT}^3$$

(A₂) 10-YR AREA = 295.9 FT²

Volume (10-YR to NORMAL FLOW)

$$= (D_2/2)(A_2 + A_3); (4.49/2)(295.9 + 323.1) = 1389.7 \text{ FT}^3$$

(A₃) NORMAL FLOW AREA = 323.1 FT²

Volume (NORMAL FLOW TO GROUND)

$$= (D_3/2)(A_3 + A_4); (6/2)(323.1 + 366.3) = 2068 \text{ FT}^3$$

(A₄) GROUND ELEV. AREA = 366.3 FT²

0-10 YR
 $(2068 + 1389.7) \text{ FT}^3$
 = 3458 FT³

10-100 YR
 = 597 FT³

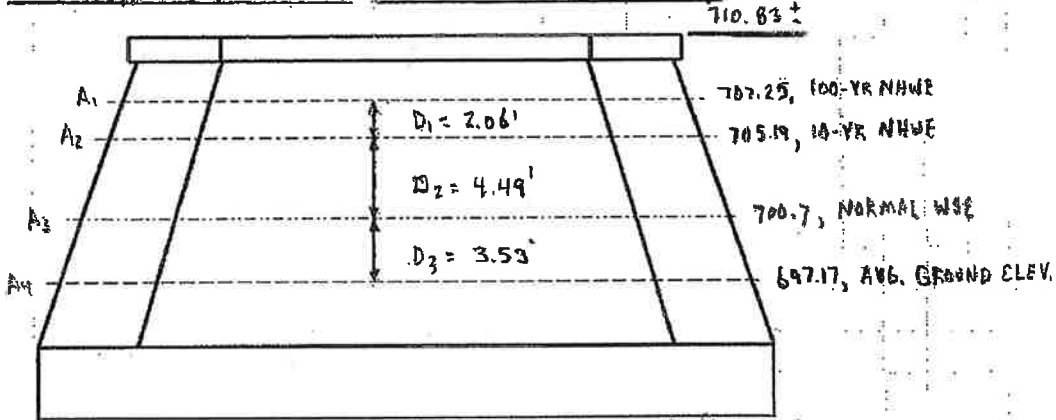


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COUNTY KANE

SHEET NO. OF
DESIGNED SGL DATE 8/03/11
CHECKED DATE

Floodway Compensation (Dual Track Alternate)



Proposed Pier # 3

(A1) 100-YR AREA = 281.2 FT²

(A2) 10-YR AREA = 294.1 FT²

(A3) NORMAL FLOW AREA = 323.2 FT²

(A4) GROUND ELEV. AREA = 347.1 FT²

Volume (10-YR to 100-YR)

$$= (D_1/2)(A_1 + A_2); \left(\frac{2.06}{2}\right)(281.2 + 294.1)$$

$$= \underline{593 \text{ FT}^3}$$

Volume (10-YR to Normal Flow)

$$= (D_2/2)(A_2 + A_3); \left(\frac{4.49}{2}\right)(294.1 + 323.2)$$

$$= \underline{1386 \text{ FT}^3}$$

Volume (Normal Flow to Ground)

$$= (D_3/2)(A_3 + A_4); \left(\frac{3.53}{2}\right)(323.2 + 347.1)$$

$$= \underline{1183 \text{ FT}^3}$$

0-10 YR

$$(1183 + 1386) \text{ FT}^3$$

$$= \underline{\underline{2569 \text{ FT}^3}}$$

10-100 YR

$$= \underline{\underline{593 \text{ FT}^3}}$$

LIN ENGINEERING, LIMITED
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 Chatham, Illinois

ROUTE MDW Z-100

SHEET OF

SECTION METRA

DESIGNED SGL DATE 8/3/11

COUNTY KANE

CHECKED DATE

Floodway Compensation (Dual Track Alternate)

West Abutment

Sec A-A Volume

$$= (10\text{-YR TO } 100\text{-YR FILL AREA})(L1); (41.4 \text{ FT}^2)(23.0 \text{ FT}) = 952.2 \text{ FT}^3$$

Sec B-B Volumes

$$= (10\text{-YR TO } 100\text{-YR FILL AREA})(L2); (47.1 \text{ FT}^2)(36 \text{ FT}) = 1695.6 \text{ FT}^3$$

$$(GROUND ELEV TO 10\text{-YR FILL AREA})(L2); (56.2 \text{ FT}^2)(36 \text{ FT}) = 1807.2 \text{ FT}^3$$

East Abutment

Sec C-C Volumes

$$(10\text{-YR TO } 100\text{-YR CUT AREA})(L3); (35.0 \text{ FT}^2)(57 \text{ FT}) = 1995 \text{ FT}^3$$

$$(GROUND ELEV TO 10\text{-YR CUT AREA})(L3); (30.0 \text{ FT}^2)(57 \text{ FT}) = 1710 \text{ FT}^3$$

West Approach

(10-YR TO 100-YR FILL Volume); Sta. 1870+30 (Limits of Sec A-A) to Sta. 1871+00

$$= \left[\frac{(7.5 \text{ FT}^2 + 2.5 \text{ FT}^2)}{2} \right] (1870+00 - 1870+30) + \left[\frac{(7.7 \text{ FT}^2 + 6.7 \text{ FT}^2)}{2} \right] (1870+50 - 1870+40)$$

$$+ \left[\frac{(6.7 \text{ FT}^2 + 0)}{2} \right] (1871+00 - 1870+50) = 76 \text{ FT}^3 + 72 \text{ FT}^3 + 162.5 \text{ FT}^3$$

$$= 310.5 \text{ FT}^3 \Rightarrow 10\text{-YR to } 100\text{-YR Floodway Fill$$

(Fill within 100-yr Special Flood Hazard Area SFHA) Sta. 1872+50 to 1879+50

$$= \frac{(1.1 + 8.7)}{2} (1871+00 - 1870+50) + \frac{(8.7 + 3.3)}{2} (1872+00 - 1871+00) + \frac{(3.3 + 6.4)}{2} (1873+00 - 1872+00)$$

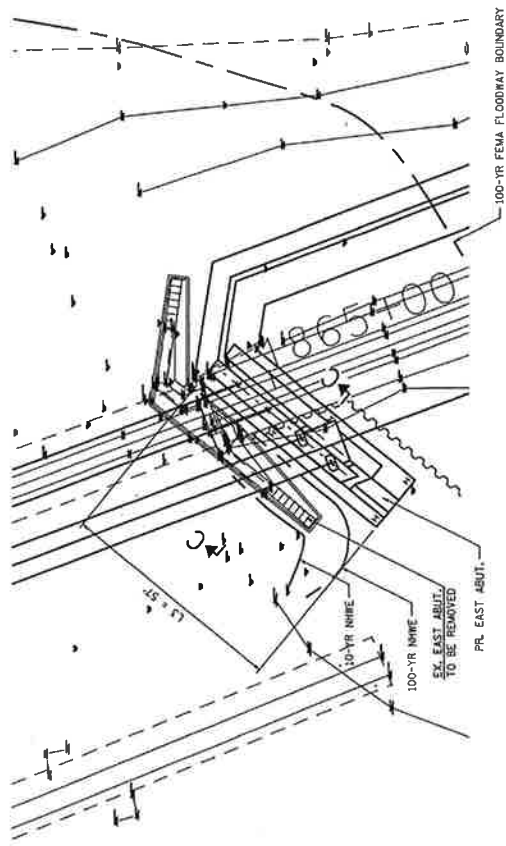
$$+ \frac{(6.4 + 22.8)}{2} (1874+00 - 1873+00) + \frac{(22.8 + 36.7)}{2} (1875+00 - 1874+00) + \frac{(36.7 + 33.5)}{2} (1876+00 - 1875+00)$$

$$+ \frac{(24.9 + 33.5)}{2} (1877+00 - 1876+00) + \frac{(24.9 + 32.5)}{2} (1878+00 - 1877+00) + \frac{(20.4 + 32.5)}{2} (1879+00 - 1878+00)$$

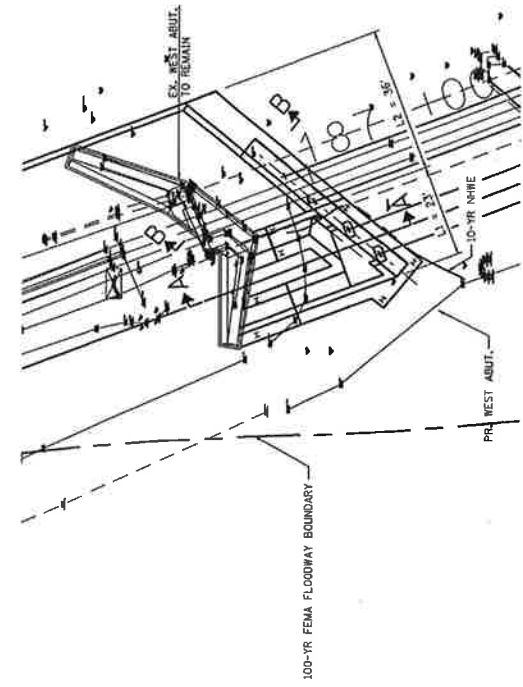
$$+ \frac{(20.4 + 0)}{2} (1880+00 - 1879+00)$$

$$= 245 \text{ FT}^3 + 600 \text{ FT}^3 + 485 \text{ FT}^3 + 1450 \text{ FT}^3 + 2975 \text{ FT}^3 + 3510 \text{ FT}^3 + 2920 \text{ FT}^3 + 2070 \text{ FT}^3$$

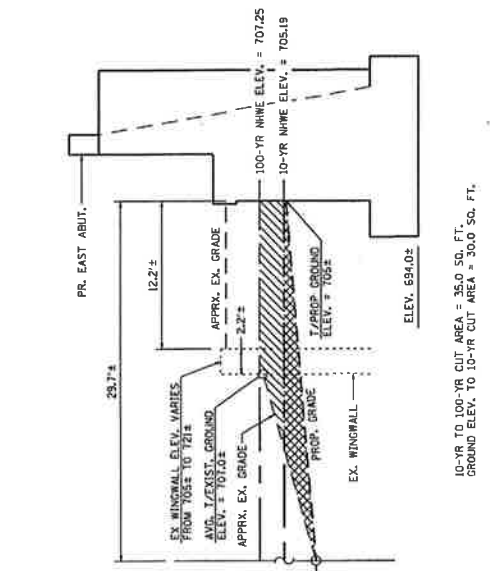
$$+ 2645 \text{ FT}^3 + 1020 \text{ FT}^3 = 18,730 \text{ FT}^3 \Rightarrow \text{Fill within } 100\text{-yr SFHA}$$



PLAN - EAST ABUTMENT/APPROACH

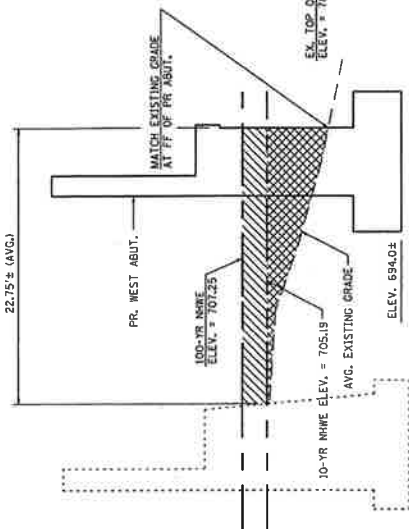


PLAN - WEST ABUTMENT/APPROACH



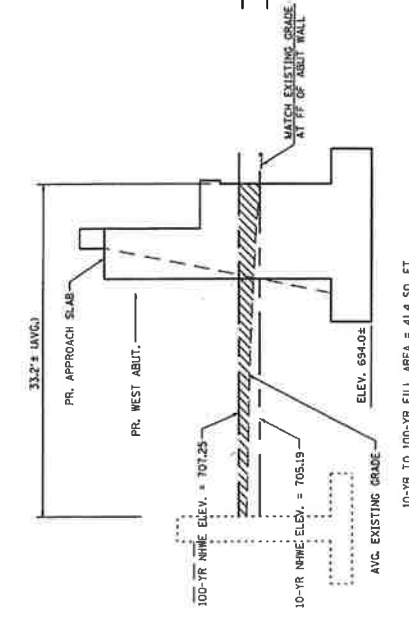
10-YR TO 100-YR CUT AREA = 35.0 SQ. FT.
GROUND ELEV. TO 10-YR CUT AREA = 300 SQ. FT.

SECTION C-C



10-YR TO 100-YR FILL AREA = 47.1 SQ. FT.
GROUND ELEV. TO 10-YR FILL AREA = 302 SQ. FT.

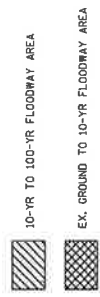
SECTION B-B



10-YR TO 100-YR FILL AREA = 41.4 SQ. FT.

SECTION A-A

*AREAS MEASURED IN MICROSTATION
N.T.S.

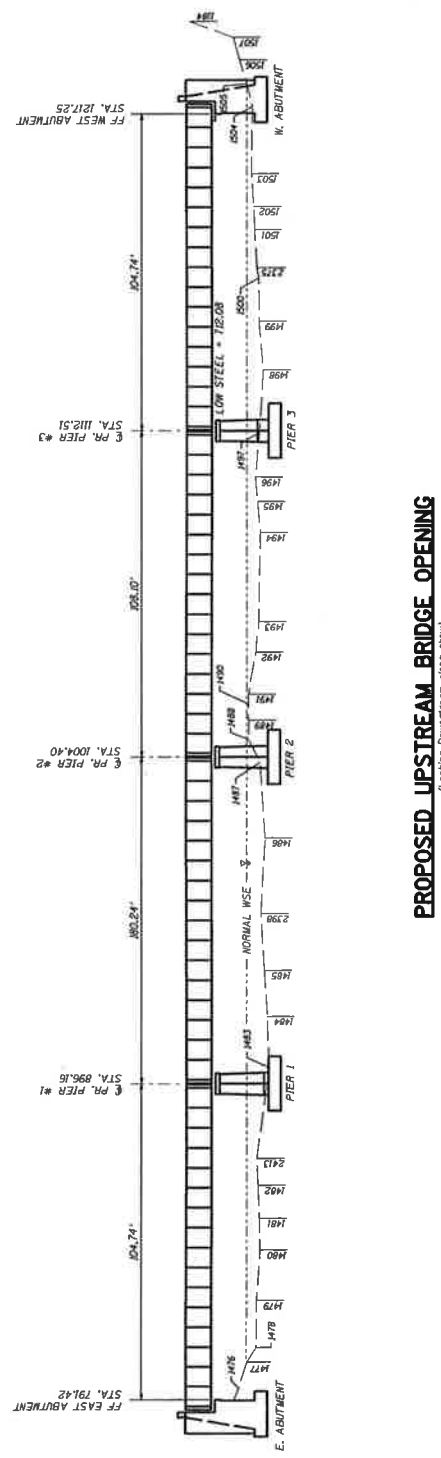


FLOODWAY COMPENSATION

POINT	STATION	STATION
2300	706.64	717.92
M16	707.58	718.86
M17	804.01	702.58
M18	804.56	697.40
M19	804.45	697.39
M20	804.52	697.40
M21	804.57	697.40
M22	804.55	696.84
M23	804.59	697.21
M24	804.57	697.21
M25	804.55	697.21
M26	804.54	697.21
M27	804.53	697.21
M28	804.52	697.21
M29	804.51	697.21
M30	804.50	697.21
M31	804.49	697.21
M32	804.48	697.21
M33	804.47	697.21
M34	804.46	697.21
M35	804.45	697.21
M36	804.44	697.21
M37	804.43	697.21
M38	804.42	697.21
M39	804.41	697.21
M40	804.40	697.21
M41	804.39	697.21
M42	804.38	697.21
M43	804.37	697.21
M44	804.36	697.21
M45	804.35	697.21
M46	804.34	697.21
M47	804.33	697.21
M48	804.32	697.21
M49	804.31	697.21
M50	804.30	697.21
M51	804.29	697.21
M52	804.28	697.21
M53	804.27	697.21
M54	804.26	697.21
M55	804.25	697.21
M56	804.24	697.21
M57	804.23	697.21
M58	804.22	697.21
M59	804.21	697.21
M60	804.20	697.21
M61	804.19	697.21
M62	804.18	697.21
M63	804.17	697.21
M64	804.16	697.21
M65	804.15	697.21
M66	804.14	697.21
M67	804.13	697.21
M68	804.12	697.21
M69	804.11	697.21
M70	804.10	697.21
M71	804.09	697.21
M72	804.08	697.21
M73	804.07	697.21
M74	804.06	697.21
M75	804.05	697.21
M76	804.04	697.21
M77	804.03	697.21
M78	804.02	697.21
M79	804.01	697.21
M80	804.00	697.21
M81	803.99	697.21
M82	803.98	697.21
M83	803.97	697.21
M84	803.96	697.21
M85	803.95	697.21
M86	803.94	697.21
M87	803.93	697.21
M88	803.92	697.21
M89	803.91	697.21
M90	803.90	697.21
M91	803.89	697.21
M92	803.88	697.21
M93	803.87	697.21
M94	803.86	697.21
M95	803.85	697.21
M96	803.84	697.21
M97	803.83	697.21
M98	803.82	697.21
M99	803.81	697.21
M100	803.80	697.21
M101	803.79	697.21
M102	803.78	697.21
M103	803.77	697.21
M104	803.76	697.21
M105	803.75	697.21
M106	803.74	697.21
M107	803.73	697.21
M108	803.72	697.21
M109	803.71	697.21
M110	803.70	697.21
M111	803.69	697.21
M112	803.68	697.21
M113	803.67	697.21
M114	803.66	697.21
M115	803.65	697.21
M116	803.64	697.21
M117	803.63	697.21
M118	803.62	697.21
M119	803.61	697.21
M120	803.60	697.21
M121	803.59	697.21
M122	803.58	697.21
M123	803.57	697.21
M124	803.56	697.21
M125	803.55	697.21
M126	803.54	697.21
M127	803.53	697.21
M128	803.52	697.21
M129	803.51	697.21
M130	803.50	697.21
M131	803.49	697.21
M132	803.48	697.21
M133	803.47	697.21
M134	803.46	697.21
M135	803.45	697.21
M136	803.44	697.21
M137	803.43	697.21
M138	803.42	697.21
M139	803.41	697.21
M140	803.40	697.21
M141	803.39	697.21
M142	803.38	697.21
M143	803.37	697.21
M144	803.36	697.21
M145	803.35	697.21
M146	803.34	697.21
M147	803.33	697.21
M148	803.32	697.21
M149	803.31	697.21
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M153	803.27	697.21
M154	803.26	697.21
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M156	803.24	697.21
M157	803.23	697.21
M158	803.22	697.21
M159	803.21	697.21
M160	803.20	697.21
M161	803.19	697.21
M162	803.18	697.21
M163	803.17	697.21
M164	803.16	697.21
M165	803.15	697.21
M166	803.14	697.21
M167	803.13	697.21
M168	803.12	697.21
M169	803.11	697.21
M170	803.10	697.21
M171	803.09	697.21
M172	803.08	697.21
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M185	802.95	697.21
M186	802.94	697.21
M187	802.93	697.21
M188	802.92	697.21
M189	802.91	697.21
M190	802.90	697.21
M191	802.89	697.21
M192	802.88	697.21
M193	802.87	697.21
M194	802.86	697.21
M195	802.85	697.21
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M198	802.82	697.21
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M201	802.79	697.21
M202	802.78	697.21
M203	802.77	697.21
M204	802.76	697.21
M205	802.75	697.21
M206	802.74	697.21
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M208	802.72	697.21
M209	802.71	697.21
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M211	802.69	697.21
M212	802.68	697.21
M213	802.67	697.21
M214	802.66	697.21
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M223	802.57	697.21
M224	802.56	697.21
M225	802.55	697.21
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M227	802.53	697.21
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M229	802.51	697.21
M230	802.50	697.21
M231	802.49	697.21
M232	802.48	697.21
M233	802.47	697.21
M234	802.46	697.21
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M243	802.37	697.21
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M283	801.97	697.21
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M285	801.95	697.21
M286	801.94	697.21
M287	801.93	697.21
M288	801.92	697.21
M289	801.91	697.21
M290	801.90	697.21
M291	801.89	697.21
M292	801.88	697.21
M293	801.87	697.21
M294	801.86	697.21
M295	801.85	697.21
M296	801.84	697.21
M297	801.83	697.21
M298	801.82	697.21
M299	801.81	697.21
M300	801.80	697.21
M301	801.79	697.21
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M311	801.69	697.21
M312	801.68	697.21
M313	801.67	697.21
M314	801.66	697.21
M315	801.65	697.21
M316	801.64	697.21
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M318	801.62	697.21
M319	801.61	697.21
M320	801.60	697.21
M321	801.59	697.21
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M357	801.23	697.21
M358	801.22	697.21
M359	801.21	697.21
M360	801.20	697.21
M361	801.19	697.21
M362	801.18	697.21
M363	801.17	697.21
M364	801.16	697.21
M365	801.15	697.21
M366	801.14	697.21
M367	801.13	697.21
M368	801.12	697.21

STATION 104.3

POINT	STATION	STATION
M16	796.30	703.26
M17	804.01	700.59
M18	811.72	697.92
M19	819.43	695.25
M20	827.14	692.58
M21	834.85	689.91
M22	842.56	687.24
M23	850.27	684.57
M24	857.98	681.90
M25	865.69	679.23
M26	873.40	676.56
M27	881.11	673.89
M28	888.82	671.22
M29	896.53	668.55
M30	904.24	665.88
M31	911.95	663.21
M32	919.66	660.54
M33	927.37	657.87
M34	935.08	655.20
M35	942.79	652.53
M36	950.50	649.86
M37	958.21	647.19
M38	965.92	644.52
M39	973.63	641.85
M40	981.34	639.18
M41	989.05	636.51
M42	996.76	633.84
M43	1004.47	631.17
M44	1012.18	628.50
M45	1019.89	625.83
M46	1027.60	623.16
M47	1035.31	620.49
M48	1043.02	617.82
M49	1050.73	615.15
M50	1058.44	612.48
M51	1066.15	609.81
M52	1073.86	607.14
M53	1081.57	604.47
M54	1089.28	601.80
M55	1096.99	599.13
M56	1104.70	596.46
M57	1112.41	593.79
M58	1120.12	591.12
M59	1127.83	588.45
M60	1135.54	585.78
M61	1143.25	583.11
M62	1150.96	580.44
M63	1158.67	577.77
M64	1166.38	575.10
M65	1174.09	572.43
M66	1181.80	569.76
M67	1189.51	567.09
M68	1197.22	564.42
M69	1204.93	561.75
M70	1212.64	559.08
M71	1220.35	556.41
M72	1228.06	553.74
M73	1235.77	551.07
M74	1243.48	548.40
M75	1251.19	545.73
M76	1258.90	543.06
M77	1266.61	540.39
M78	1274.32	537.72
M79	1282.03	535.05
M80	1289.74	532.38
M81	1297.45	529.71
M82	1305.16	527.04
M83	1312.87	524.37
M84	1320.58	521.70
M85	1328.29	519.03
M86	1336.00	516.36
M87	1343.71	513.69
M88	1351.42	511.02
M89	1359.13	508.35
M90	1366.84	505.68
M91	1374.55	503.01
M92	1382.26	500.34
M93	1389.97	497.67
M94	1397.68	495.00
M95	1405.39	492.33
M96	1413.10	489.66
M97	1420.81	486.99
M98	1428.52	484.32
M99	1436.23	481.65
M100	1443.94	478.98
M101	1451.65	476.31
M102	1459.36	473.64
M103	1467.07	470.97
M104	1474.78	468.30
M105	1482.49	465.63
M106	1490.20	462.96
M107	1497.91	460.29
M108	1505.62	457.62
M109	1513.33	454.95
M110	1521.04	452.28
M111	1528.75	449.61
M112	1536.46	446.94
M113	1544.17	444.27
M114	1551.88	441.60
M115	1559.59	438.93
M116	1567.30	436.26
M117	1575.01	433.59
M118	1582.72	430.92
M119	1590.43	428.25
M120	1598.14	425.58
M121	1605.85	422.91
M122	1613.56	420.24
M123	1621.27	417.57
M124	1628.98	414.90
M125	1636.69	412.23
M126	1644.40	409.56
M127	1652.11	406.89
M128	1659.82	404.22
M129	1667.53	401.55
M130	1675.24	398.88
M131	1682.95	396.21
M132	1690.66	393.54
M133	1698.37	390.87
M134	1706.08	388.20
M135	1713.79	385.53
M136	1721.50	382.86
M137	1729.21	380.19
M138	1736.92	377.52
M139	1744.63	374.85
M140	1752.34	372.18
M141	1760.05	369.51
M142	1767.76	366.84
M143	1775.47	364.17
M144	1783.18	361.50
M145	1790.89	358.83
M146	1798.60	356.16
M147	1806.31	353.49
M148	1814.02	350.82
M149	1821.73	348.15
M150	1829.44	345.48
M151	1837.15	342.81
M152	1844.86	340.14
M153	1852.57	337.47
M154	1860.28	334.80
M155	1867.99	332.13
M156	1875.70	329.46
M157	1883.41	326.79
M158	1891.12	324.12
M159	1898.83	321.45
M160	1906.54	318.78
M161	1914.25	316.11
M162	1921.96	313.44
M163	1929.67	310.77
M164	1937.38	308.10
M165	1945.09	305.43
M166	1952.80	302.76
M167	1960.51	300.09
M168	1968.22	297.42
M169	1975.93	294.75
M170	1983.64	292.08
M171	1991.35	289.41
M172	1999.06	286.74
M173	2006.77	284.07
M174	2014.48	281.40
M175	2022.19	278.73
M176	2029.90	276.06
M177	2037.61	273.39
M178	2045.32	270.72
M179	2053.03	268.05
M180	2060.74	265.38
M181	2068.45	262.71
M182	2076.16	260.04
M183	2083.87	257.37
M184	2091.58	254.70
M185	2099.29	252.03
M186	2107.00	249.36
M187	2114.71	246.69
M188	2122.42	244.02
M189	2130.13	241.35
M190	2137.84	238.68
M191	2145.55	236.01
M192	2153.26	233.34
M193	2160.97	230.67
M194	2168.68	228.00
M195	2176.39	225.33
M196	2184.10	222.66
M197	2191.81	220.00
M198	2199.52	217.33
M199	2207.23	214.66
M200	2214.94	212.00
M201	2222.65	209.33
M202	2230.36	206.66
M203	2238.07	204.00
M204	2245.78	201.33
M205	2253.49	198.66
M206	2261.20	196.00
M207	2268.91	193.33
M208	2276.62	190.66
M209	2284.33	188.00
M210	2292.04	185.33
M211	2299.75	182.66
M212	2307.46	180.00
M213	2315.17	177.33
M214	2322.88	174.66
M215	2330.59	172.00
M216	2338.30	169.33
M217	2346.01	166.66
M218	2353.72	164.00
M219	2361.43	161.33
M220	2369.14	158.66
M221	2376.85	156.00
M222	2384.56	153.33
M223	2392.27	150.66
M224	2400.00	148.00



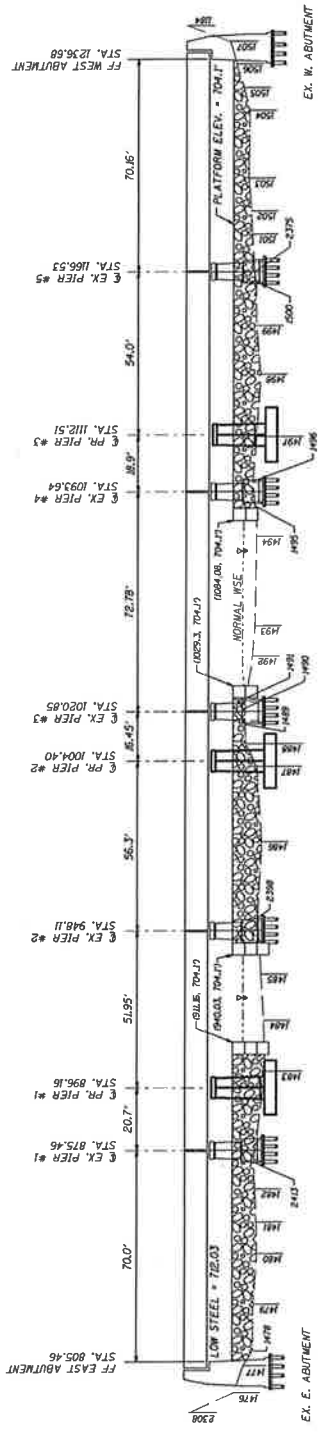
PROPOSED UPSTREAM BRIDGE OPENING
 (Looking Downstream along stem)

STATION 1063.7

POINT	STATION	STATION
A53	776.37	703.00
A54	804.09	700.33
A55	811.80	697.66
A56	819.51	695.00
A57	827.22	692.33
A58	834.93	689.66
A59	842.64	687.00
A60	850.35	684.33
A61	858.06	681.66
A62	865.77	679.00
A63	873.48	676.33
A64	881.19	673.66
A65	888.90	671.00
A66	896.61	668.33
A67	904.32	665.66
A68	912.03	663.00
A69	919.74	660.33
A70	927.45	657.66
A71	935.16	655.00
A72	942.87	652.33
A73	950.58	649.66
A74	958.29	647.00
A75	966.00	644.33
A76	973.71	641.66
A77	981.42	639.00
A78	989.13	636.33
A79	996.84	633.66
A80	1004.55	631.00
A81	1012.26	628.33
A82	1019.97	625.66
A83	1027.68	623.00
A84	1035.39	620.33
A85	1043.10	617.66
A86	1050.81	615.00
A87	1058.52	612.33
A88	1066.23	609.66
A89	1073.94	607.00
A90	1081.65	604.33
A91	1089.36	601.66
A92	1097.07	599.00
A93	1104.78	596.33
A94	1112.49	593.66
A95	1120.20	591.00
A96	1127.91	588.33
A97	1135.62	585.66
A98	1143.33	583.00
A99	1151.04	580.33
A100	1158.75	577.66
A101	1166.46	575.00
A102	1174.17	572.33
A103	1181.88	569.66
A104	1189.59	567.00
A105	1197.30	564.33
A106	1205.01	561.66
A107	1212.72	559.00
A108	1220.43	556.33
A109	1228.14	553.66
A110	1235.85	551.00
A111	1243.56	548.33
A112	1251.27	545.66
A113	1258.98	543.00
A114	1266.69	540.33
A115	1274.40	537.66
A116	1282.11	535.00
A117	1289.82	532.33
A118	1297.53	529.66
A119	1305.24	527.00
A120	1312.95	524.33
A121	1320.66	521.66
A122	1328.37	519.00
A123	1336.08	516.33
A124	1343.79	513.66
A125	1351.50	511.00
A126	1359.21	508.33
A127	1366.92	505.66
A128	1374.63	503.00
A129	1382.34	500.33
A130	1390.05	497.66
A131	1397.76	495.00
A132	1405.47	492.33
A133	1413.18	489.66
A134	1420.89	487.00
A135	1428.60	484.33
A136	1436.31	481.66
A137	1444.02	479.00
A138	1451.73	476.33
A139	1459.44	473.66
A140	1467.15	471.00
A141	1474.86	468.33
A142	1482.57	465.66
A143	1490.28	463.00
A144	1497.99	460.33
A145	1505.70	457.66
A146	1513.41	455.00
A147	1521.12	452.33
A148	1528.83	449.66
A149	1536.54	447.00
A150	1544.25	444.33
A151	1551.96	441.66
A152	1559.67	439.00
A153	1567.38	436.33
A154	1575.09	433.66
A155	1582.80	431.00
A156	1590.51	428.33
A157	1598.22	425.66
A158	1605.93	423.00
A159	1613.64	420.33
A160	1621.35	417.66
A161	1629.06	415.00
A162	1636.77	412.33
A163	1644.48	409.66
A164	1652.19	407.00
A165	1659.90	404.33
A166	1667.61	401.66
A167	1675.32	399.00
A168	1683.03	396.33
A169	1690.74	393.66
A170	1698.45	391.00
A171	1706.16	388.33
A172	1713.87	385.66
A173	1721.58	383.00
A174	1729.29	380.33
A175	1737.00	377.66
A176	1744.71	375.00
A177	1752.42	372.33
A178	1760.13	369

STATION 1094.3

POINT	STATION	STATION
2308	705.87	710.00
1177	705.87	710.00
1178	804.01	700.59
1179	824.45	697.40
1180	845.83	697.39
1181	851.71	698.40
1182	862.55	696.84
2413	871.59	697.21
1184	888.44	693.97
1185	831.75	694.87
2389	952.51	696.02
1186	977.51	694.72
1187	977.51	694.72
1188	999.97	699.97
1189	1000.21	700.08
1190	1022.52	700.21
1191	1033.13	697.55
1192	1033.13	697.55
1193	1076.74	696.22
1194	1076.74	696.22
1195	1076.74	696.22
1196	1076.74	696.22
1197	1111.97	697.03
1198	1111.97	697.03
1199	1111.97	697.03
1200	1111.97	697.03
1201	1111.97	697.03
1202	1111.97	697.03
1203	1111.97	697.03
1204	1111.97	697.03
1205	1111.97	697.03
1206	1111.97	697.03
1207	1111.97	697.03
1208	1111.97	697.03
1209	1111.97	697.03
1210	1111.97	697.03
1211	1111.97	697.03
1212	1111.97	697.03
1213	1111.97	697.03
1214	1111.97	697.03
1215	1111.97	697.03
1216	1111.97	697.03
1217	1111.97	697.03
1218	1111.97	697.03
1219	1111.97	697.03
1220	1111.97	697.03
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1223	1111.97	697.03
1224	1111.97	697.03
1225	1111.97	697.03
1226	1111.97	697.03
1227	1111.97	697.03
1228	1111.97	697.03
1229	1111.97	697.03
1230	1111.97	697.03
1231	1111.97	697.03
1232	1111.97	697.03
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1234	1111.97	697.03
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1236	1111.97	697.03
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1253	1111.97	697.03
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1279	1111.97	697.03
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1281	1111.97	697.03
1282	1111.97	697.03
1283	1111.97	697.03
1284	1111.97	697.03
1285	1111.97	697.03
1286	1111.97	697.03
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1288	1111.97	697.03
1289	1111.97	697.03
1290	1111.97	697.03
1291	1111.97	697.03
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1298	1111.97	697.03
1299	1111.97	697.03
1300	1111.97	697.03

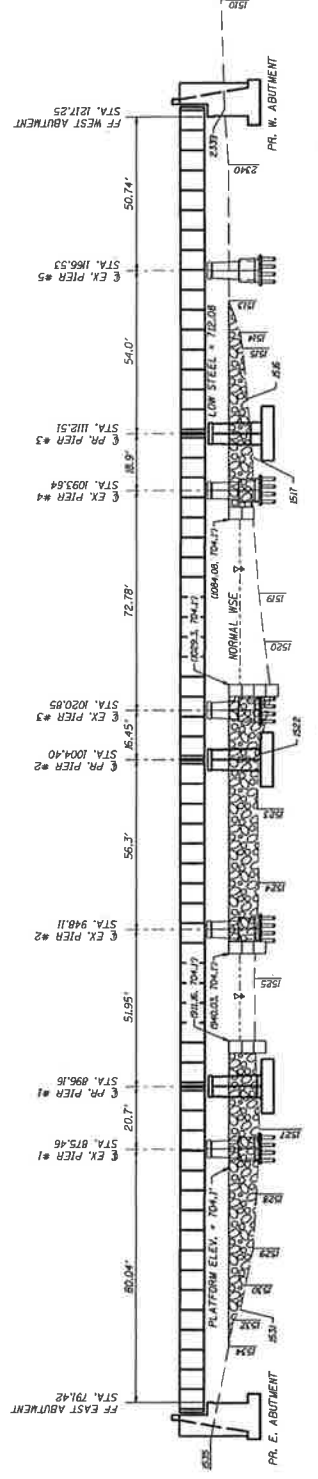


UPSTREAM BRIDGE OPENING - CONSTRUCTION

(Looking Downstream along flow)

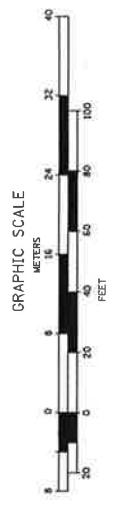
STATION 1096.1.7

POINT	STATION	STATION
2308	705.87	710.00
1177	705.87	710.00
1178	804.01	700.59
1179	824.45	697.40
1180	845.83	697.39
1181	851.71	698.40
1182	862.55	696.84
2413	871.59	697.21
1184	888.44	693.97
1185	831.75	694.87
2389	952.51	696.02
1186	977.51	694.72
1187	977.51	694.72
1188	999.97	699.97
1189	1000.21	700.08
1190	1022.52	700.21
1191	1033.13	697.55
1192	1033.13	697.55
1193	1076.74	696.22
1194	1076.74	696.22
1195	1076.74	696.22
1196	1076.74	696.22
1197	1111.97	697.03
1198	1111.97	697.03
1199	1111.97	697.03
1200	1111.97	697.03
1201	1111.97	697.03
1202	1111.97	697.03
1203	1111.97	697.03
1204	1111.97	697.03
1205	1111.97	697.03
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1207	1111.97	697.03
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1213	1111.97	697.03
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1217	1111.97	697.03
1218	1111.97	697.03
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1231	1111.97	697.03
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1240	1111.97	697.03
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1271	1111.97	697.03
1272	1111.97	697.03
1273	1111.97	697.03
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1279	1111.97	697.03
1280	1111.97	697.03
1281	1111.97	697.03
1282	1111.97	697.03
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1293	1111.97	697.03
1294	1111.97	697.03
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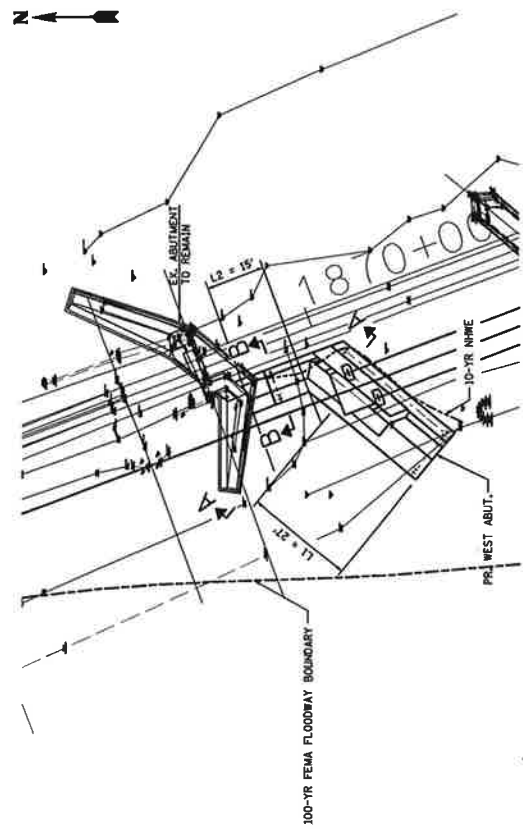
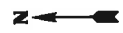
DOWNSTREAM BRIDGE OPENING - CONSTRUCTION

(Looking Downstream along flow)

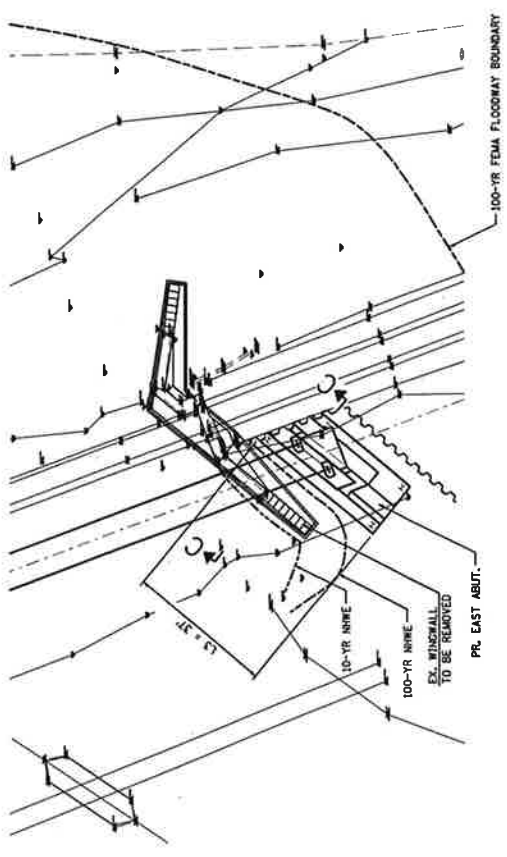


LE LIN ENGINEERING, LTD.
Consulting Engineers
Cheriton, Bexley

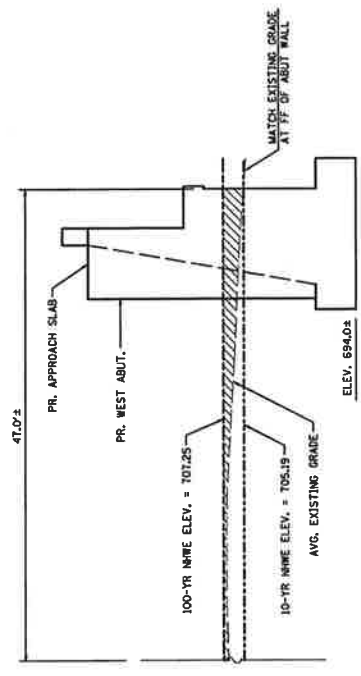
**PROPOSED BRIDGE OPENINGS
FINAL STAGE OF CONSTRUCTION**



PLAN - WEST ABUTMENT/APPROACH



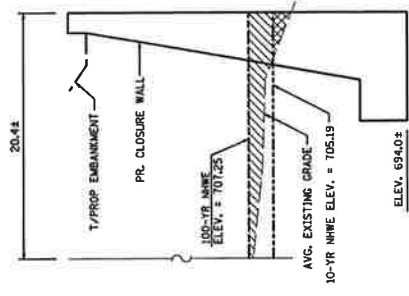
PLAN - EAST ABUTMENT/APPROACH



10-YR TO 100-YR FILL AREA = 41.5 SQ. FT.

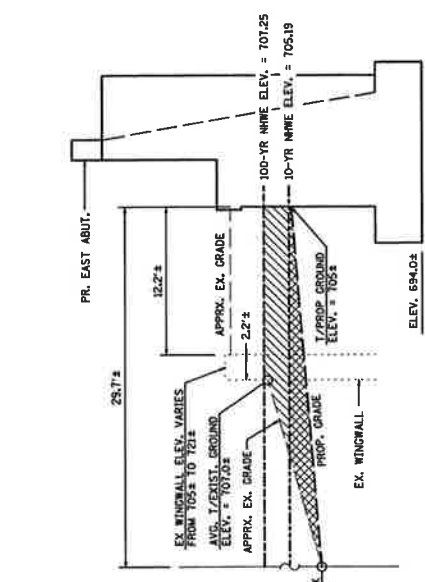
SECTION A-A

*AREAS MEASURED IN MICROSTATION
N.T.S.



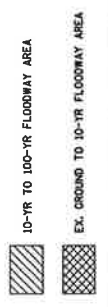
10-YR TO 100-YR FILL AREA = 25.9 SQ. FT.
GROUND ELEV. TO 10-YR FILL AREA = 2.9 SQ. FT.

SECTION B-B

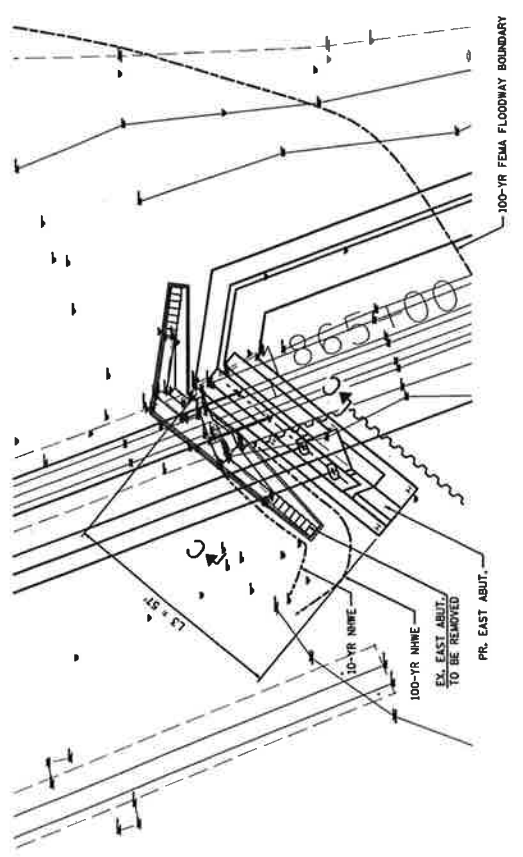
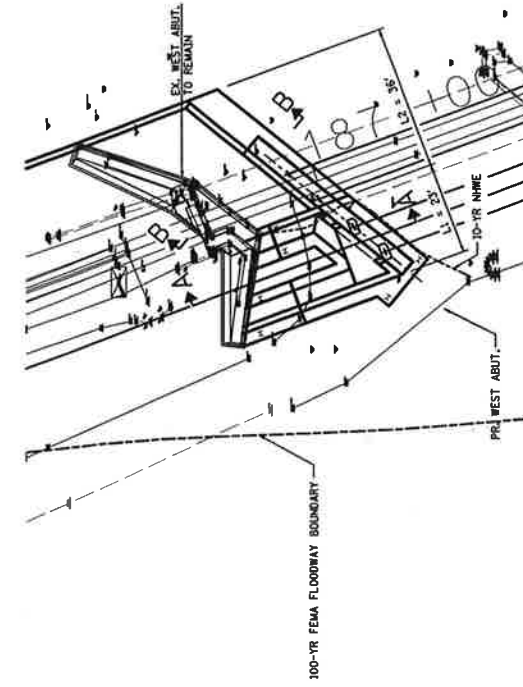


10-YR TO 100-YR CUT AREA = 35.0 SQ. FT.
GROUND ELEV. TO 10-YR CUT AREA = 30.0 SQ. FT.

SECTION C-C

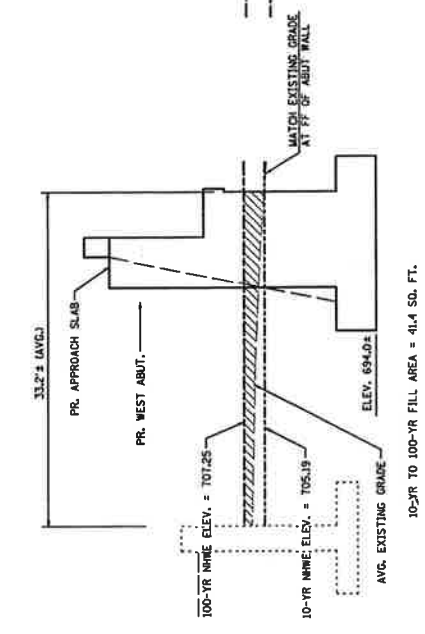


FLOODWAY COMPENSATION

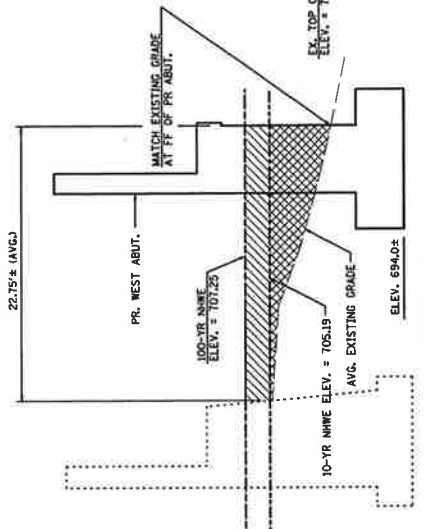


PLAN - WEST ABUTMENT/APPROACH

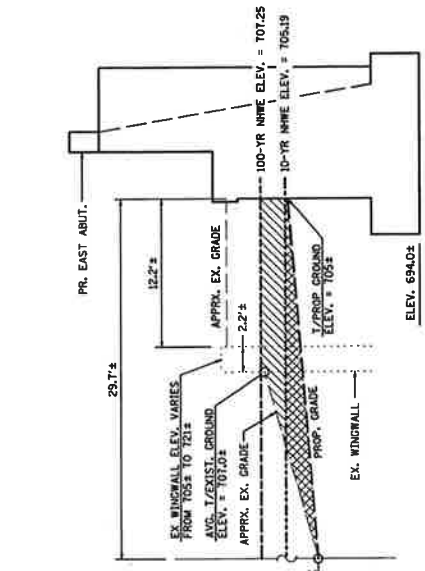
PLAN - EAST ABUTMENT/APPROACH



SECTION A-A

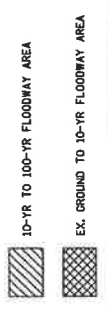


SECTION B-B



SECTION C-C

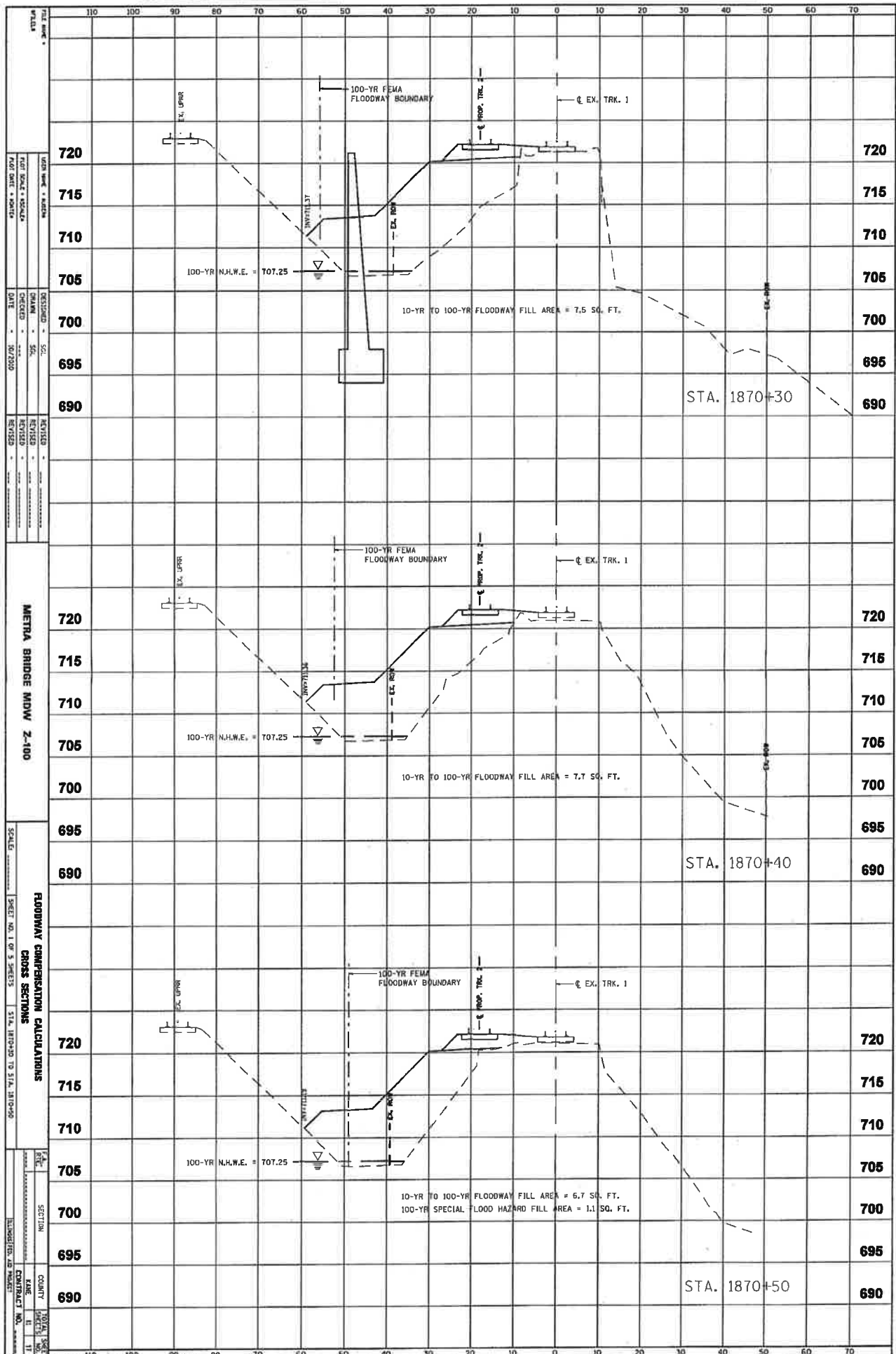
*AREAS MEASURED IN MICROSTATION
N.T.S.



FLOODWAY COMPENSATION

DESIGN SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLATED		
NO.	TEMPLATE		
	AREA		
	SQUARE FEET		

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLATED		
NO.	TEMPLATE		
	AREA		
	SQUARE FEET		



DESIGNED	BY	DATE
CHECKED	BY	DATE
REVISED	BY	DATE

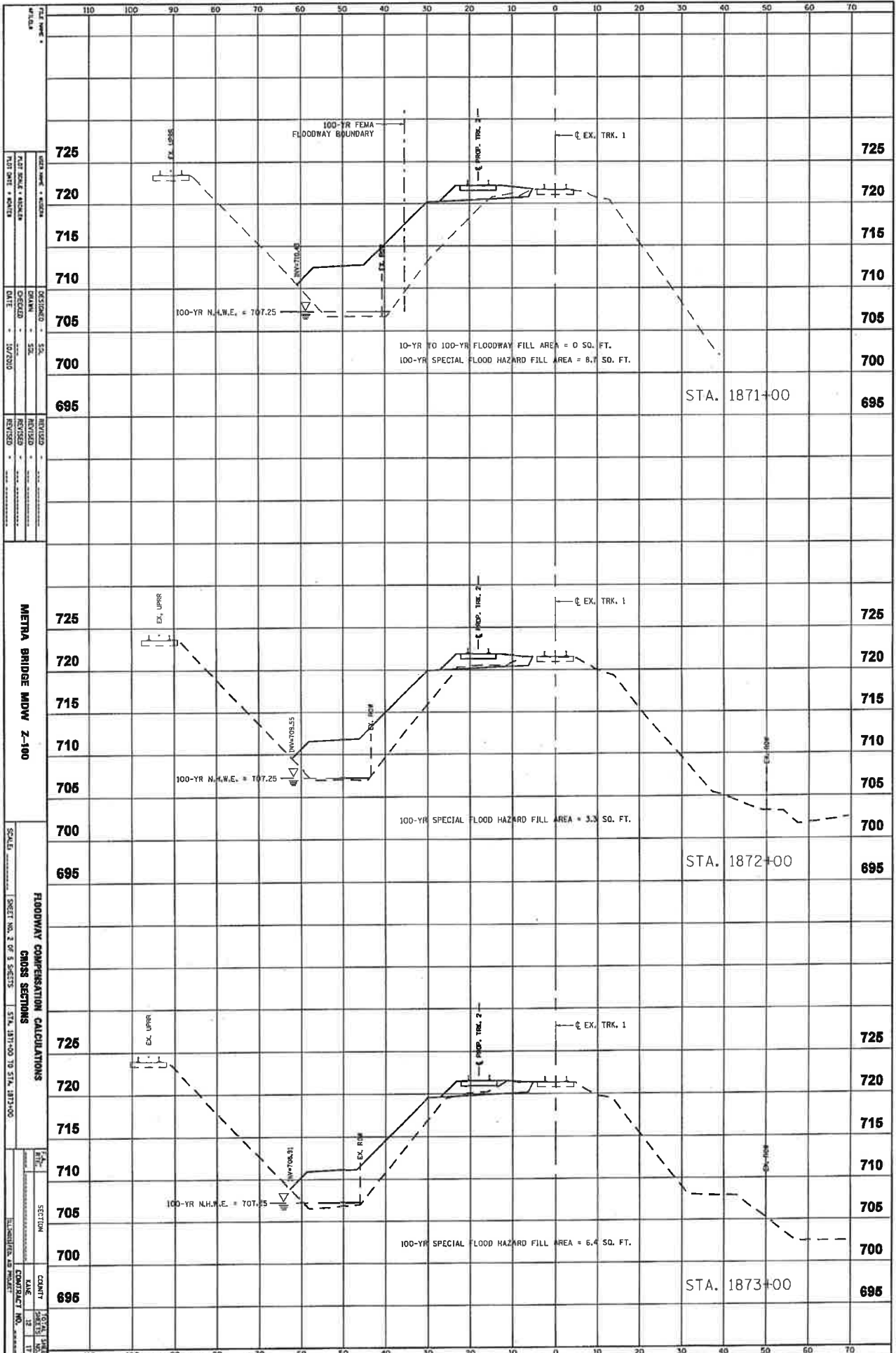
METRA BRIDGE MDW Z-100

FLOODWAY COMPENSATION CALCULATIONS CROSS SECTIONS

COUNTY	SECTION
TOWNSHIP	RANGE
CONTRACT NO.	PROJECT NO.

DESIGN SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLANNED		
NO.	TEMPLATE		
	AREA		
	AREA DECOR		

FORM SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLANNED		
NO.	TEMPLATE		
	AREA		
	AREA DECOR		



FILED	DATE	BY
APPROVED	DATE	BY
CHECKED	DATE	BY
DATE	12/20/00	

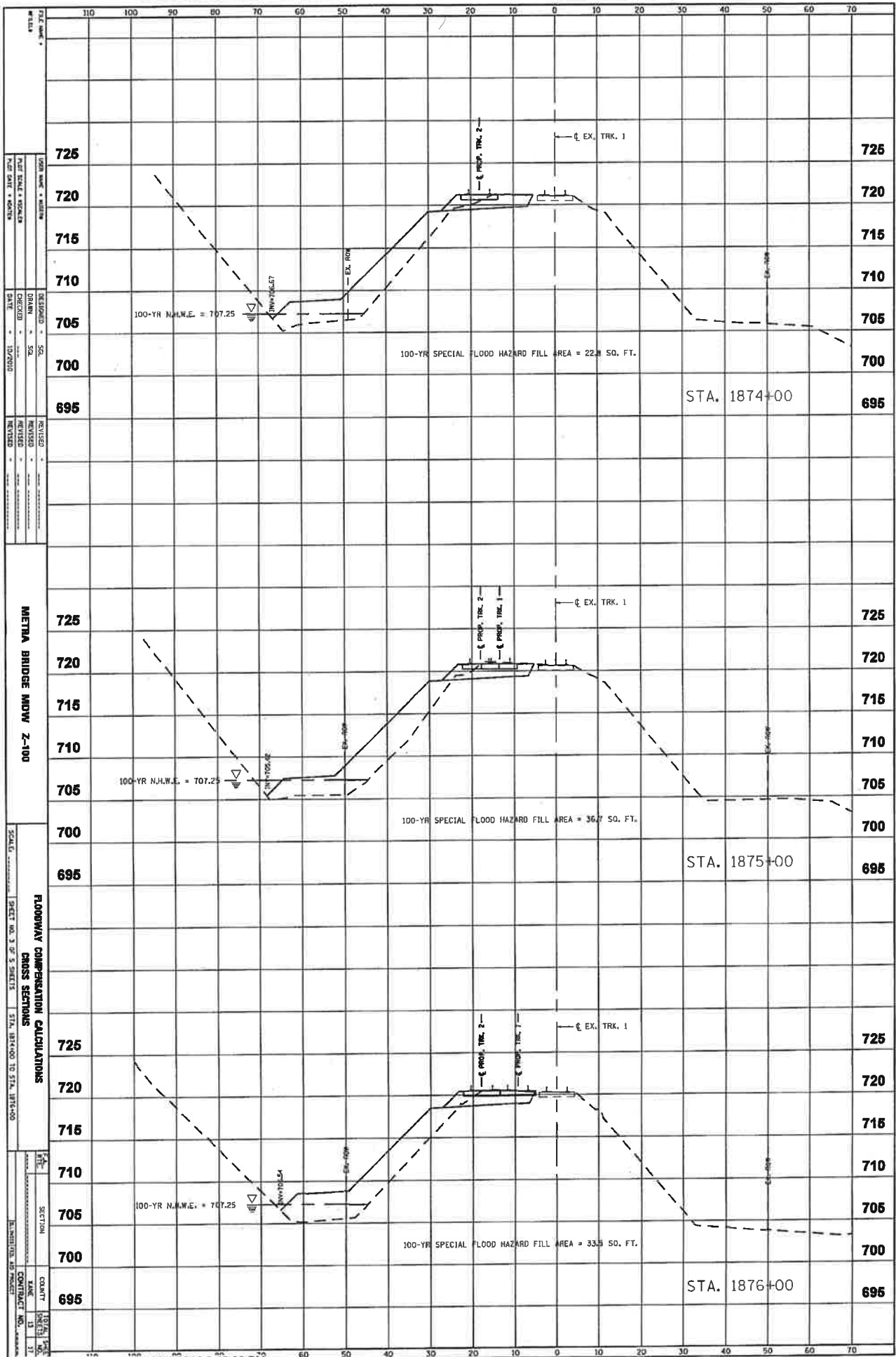
METRA BRIDGE MDW Z-100

FLOODWAY COMPENSATION CALCULATIONS

SECTION	DATE
DATE	12/20/00
COMPILED	DATE
DATE	12/20/00

DESIGN	BY	DATE
SURVEY		
NOTE BOOK		
FIELD		
ASSET		

FINAL	BY	DATE
SURVEY		
NOTE BOOK		
FIELD		
ASSET		



DESIGNED	SCS
CHECKED	SCS
DATE	10/20/10
REVISION	
REVISION	
REVISION	

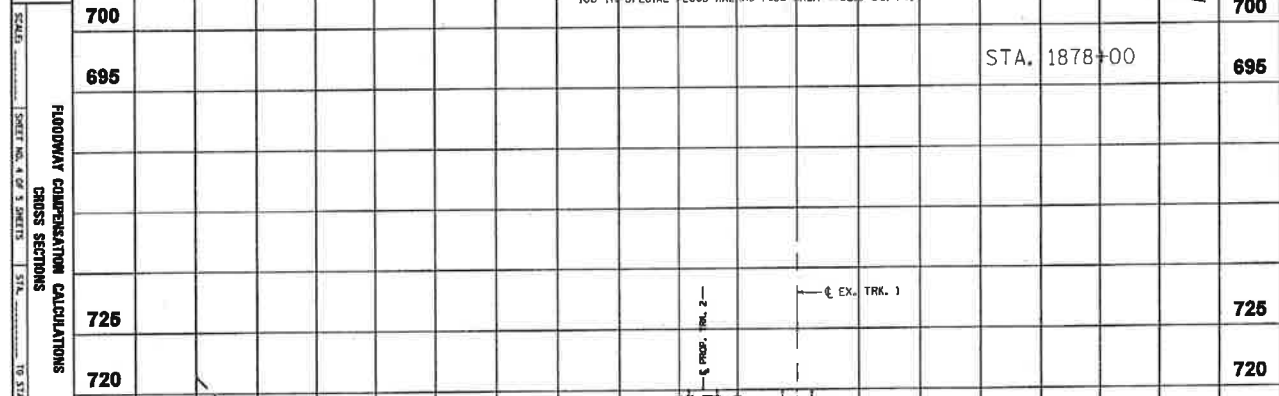
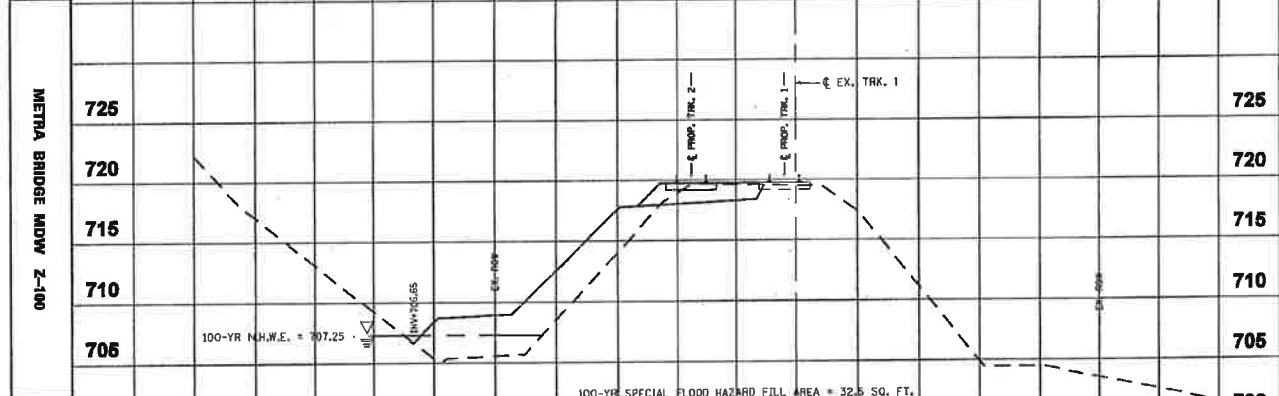
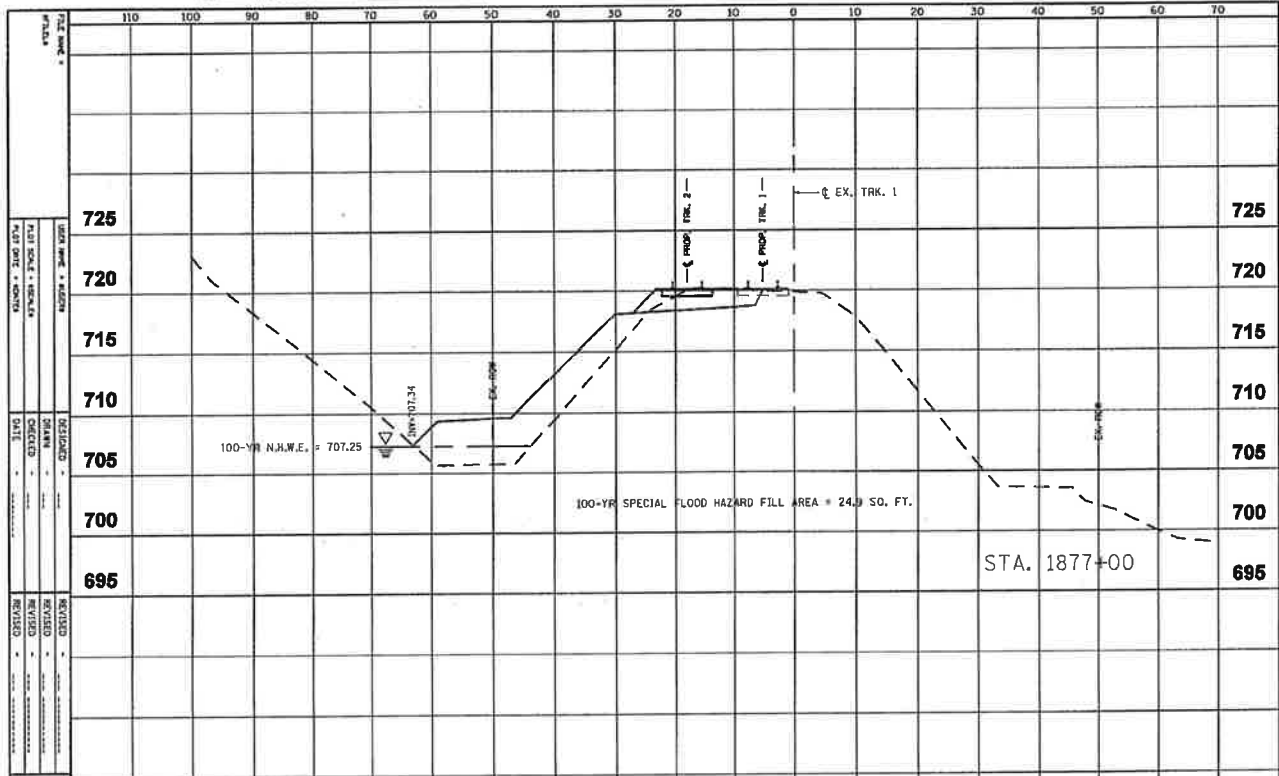
MEHTA BRIDGE MODV Z-100

**FLOODWAY COMPENSATION CALCULATIONS
CROSS SECTIONS**

SECTION	COUNT
DATE	NO.
CONTRACT NO.	

ORIGINAL SURVEY	DRAWN	BY	DATE

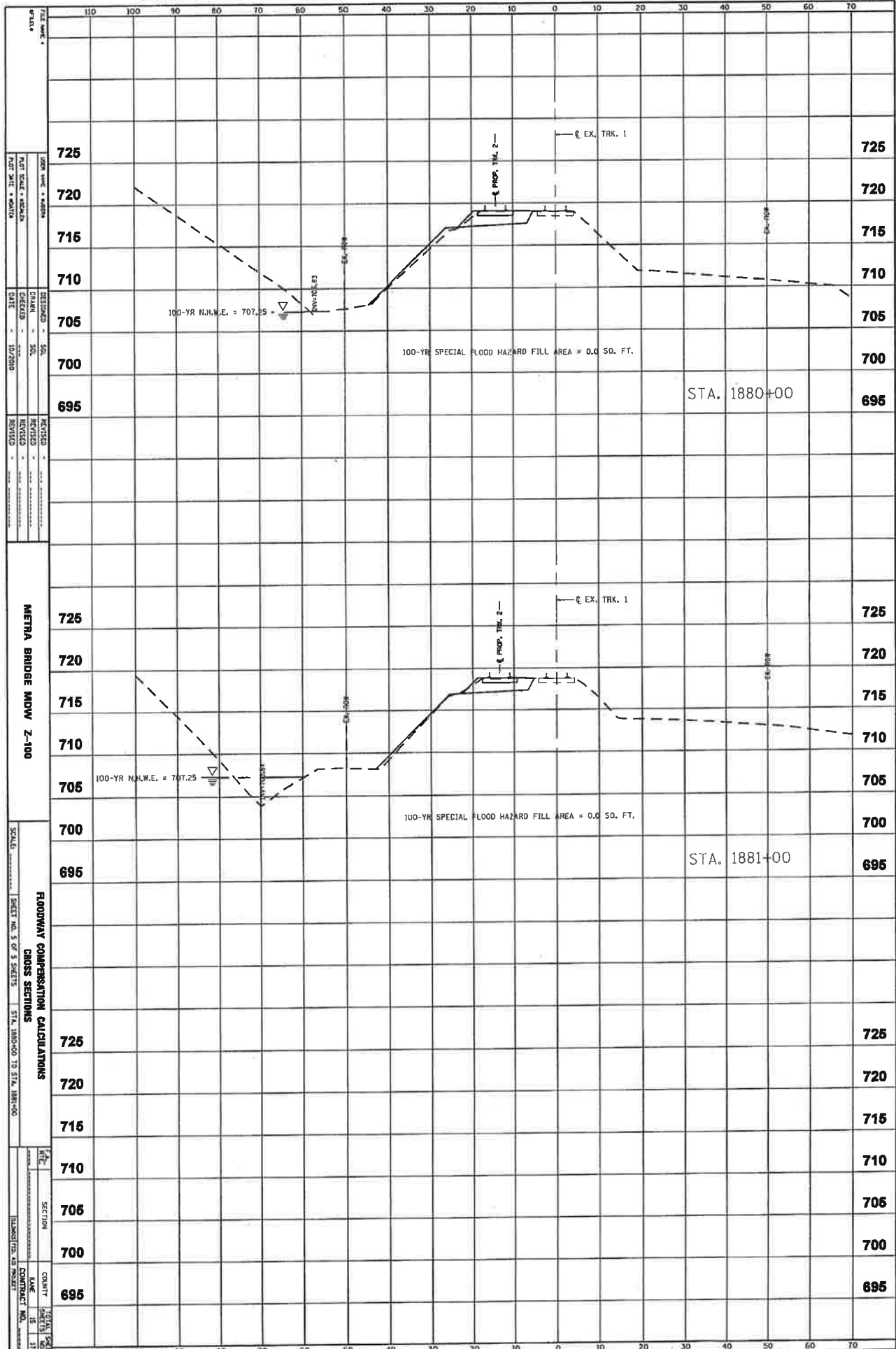
FINAL SURVEY	DRAWN	BY	DATE



SECTION	DATE	BY	CHKD

DESIGN SURVEY	SUBMITTED	BY	DATE
NOTE BOOK	FORWARDED		
NO.	NO.		

FINAL SURVEY	FORWARDED	BY	DATE
NOTE BOOK	FORWARDED		
NO.	NO.		



DESIGNED BY	DATE
CHECKED BY	DATE
REVISIONS	

METRA BRIDGE MDW Z-100

FLOODWAY COMPENSATION CALCULATIONS
CROSS SECTIONS

SECTION	COUNTY	DATE
CONTRACT NO.		

Appendix F Environmental Database Search

FirstSearch Technology Corporation

Environmental FirstSearch™ Report

Target Property:

ELGIN IL 60120

Job Number: ELGIN-RR-TRACK

PREPARED FOR:

**Huff and Huff, Inc.
915 Harger Road, Suite 330
Oak Brook, IL 60523**

08-17-10



Tel: (317) 823-3500

Fax: (317) 823-3535

Environmental FirstSearch Search Summary Report

Target Site:

ELGIN IL 60120

FirstSearch Summary

Database	Sel	Updated	Radius	Site	1/8	1/4	1/2	1/2>	ZIP	TOTALS
NPL	Y	08-01-10	1.25	0	0	0	0	0	1	1
NPL Delisted	Y	08-02-10	0.75	0	0	0	0	0	0	0
CERCLIS	Y	07-02-10	0.75	0	0	0	0	0	1	1
NFRAP	Y	07-02-10	0.75	0	0	1	0	2	0	3
RCRA COR ACT	Y	07-14-10	1.25	0	0	1	0	1	0	2
RCRA TSD	Y	07-14-10	0.75	0	0	1	0	1	0	2
RCRA GEN	Y	07-14-10	0.50	0	0	2	13	-	0	15
RCRA NLR	Y	07-14-10	0.50	0	0	0	7	-	2	9
Federal Brownfield	Y	07-06-10	0.75	0	0	0	0	0	0	0
ERNS	Y	07-23-10	0.25	0	0	0	1	-	6	7
Tribal Lands	Y	12-01-05	1.25	0	0	0	0	0	0	0
State/Tribal Sites	Y	NA	1.25	0	0	0	0	0	0	0
State Spills 90	Y	NA	0.50	0	0	0	0	-	1	1
State Spills 80	Y	NA	0.50	0	0	0	0	-	0	0
State/Tribal SWL	Y	11-01-08	0.75	0	0	0	0	0	2	2
State/Tribal LUST	Y	06-11-10	0.75	0	0	2	6	8	3	19
State/Tribal UST/AST	Y	07-12-10	0.50	0	0	3	13	-	7	23
State/Tribal EC	Y	08-13-10	0.75	0	0	0	0	0	0	0
State/Tribal IC	Y	08-13-10	0.50	0	0	0	1	-	0	1
State/Tribal VCP	Y	08-13-10	0.75	0	0	1	1	3	0	5
State/Tribal Brownfields	Y	02-22-08	0.75	0	0	0	0	0	0	0
DOCKET	Y	01-09-06	0.50	0	0	0	1	-	1	2
Federal Other	Y	01-01-09	0.50	0	0	0	0	-	0	0
State Other	Y	06-02-10	0.25	0	0	0	-	-	0	0
Federal IC/EC	Y	06-02-10	0.75	0	0	0	0	0	2	2
- TOTALS -				0	0	11	43	15	26	95

Notice of Disclaimer

Due to the limitations, constraints, inaccuracies and incompleteness of government information and computer mapping data currently available to FirstSearch Technology Corp., certain conventions have been utilized in preparing the locations of all federal, state and local agency sites residing in FirstSearch Technology Corp.'s databases. All EPA NPL and state landfill sites are depicted by a rectangle approximating their location and size. The boundaries of the rectangles represent the eastern and western most longitudes; the northern and southern most latitudes. As such, the mapped areas may exceed the actual areas and do not represent the actual boundaries of these properties. All other sites are depicted by a point representing their approximate address location and make no attempt to represent the actual areas of the associated property. Actual boundaries and locations of individual properties can be found in the files residing at the agency responsible for such information.

Waiver of Liability

Although FirstSearch Technology Corp. uses its best efforts to research the actual location of each site, FirstSearch Technology Corp. does not and can not warrant the accuracy of these sites with regard to exact location and size. All authorized users of FirstSearch Technology Corp.'s services proceeding are signifying an understanding of FirstSearch Technology Corp.'s searching and mapping conventions, and agree to waive any and all liability claims associated with search and map results showing incomplete and or inaccurate site locations.

**Environmental FirstSearch
Site Information Report**

Request Date: 08-17-10
Requestor Name: Maureen Wunderlich/pt
Standard: AAI

Search Type: COORD
Job Number: ELGIN-RR-TRACK
Filtered Report

Target Site:
 ELGIN IL 60120

Demographics

Sites: 95	Non-Geocoded: 26	Population: NA
Radon: 1.4 - 10.9 PCI/L		

Site Location

	<u>Degrees (Decimal)</u>	<u>Degrees (Min/Sec)</u>	<u>UTMs</u>
Longitude:	-88.275645	-88:16:32	Easting: 394382.626
Latitude:	42.019552	42:1:10	Northing: 4652520.581
Elevation:	701		Zone: 16

Comment

Comment:

Additional Requests/Services

Adjacent ZIP Codes: 1 Mile(s)				Services:		
<u>ZIP Code</u>	<u>City Name</u>	<u>ST</u>	<u>Dist/Dir</u>	<u>Set</u>	<u>Requested?</u>	<u>Date</u>
60123	ELGIN	IL	0.02 NW	Y	Fire Insurance Maps	No
60177	SOUTH ELGIN	IL	0.27 SE	Y	Aerial Photographs	No
60124	Elgin	IL		Y	Historical Topos	No
60123	ELGIN	IL		Y	City Directories	No
60103	BARTLETT	IL	0.83 SE	N	Title Search/Env Liens	No
					Municipal Reports	No
					Online Topos	No

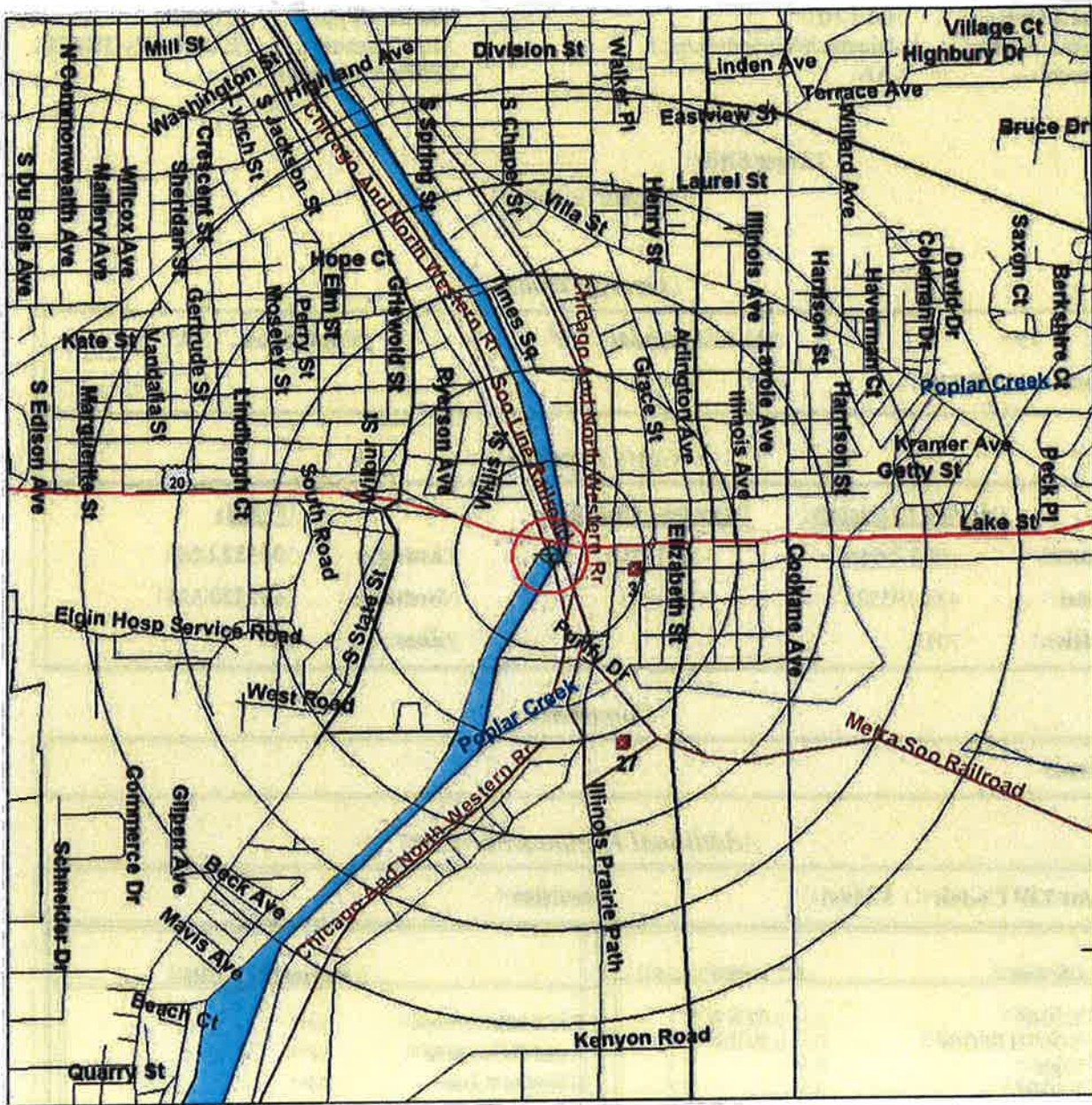


Environmental FirstSearch

1.25 Mile Radius
AAI: NPL, RCRACOR, STATE



, ELGIN IL 60120



Source: 2005 U.S. Census TIGER Files

- Target Site (Latitude: 42.019552 Longitude: -88.275645)
- Identified Site, Multiple Sites, Receptor
- NPL, DELNPL, Brownfield, Solid Waste Landfill (SWL), Hazardous Waste
- Triballand
- Railroads
- Black Rings Represent 1/4 Mile Radius; Red Ring Represents 500 ft. Radius



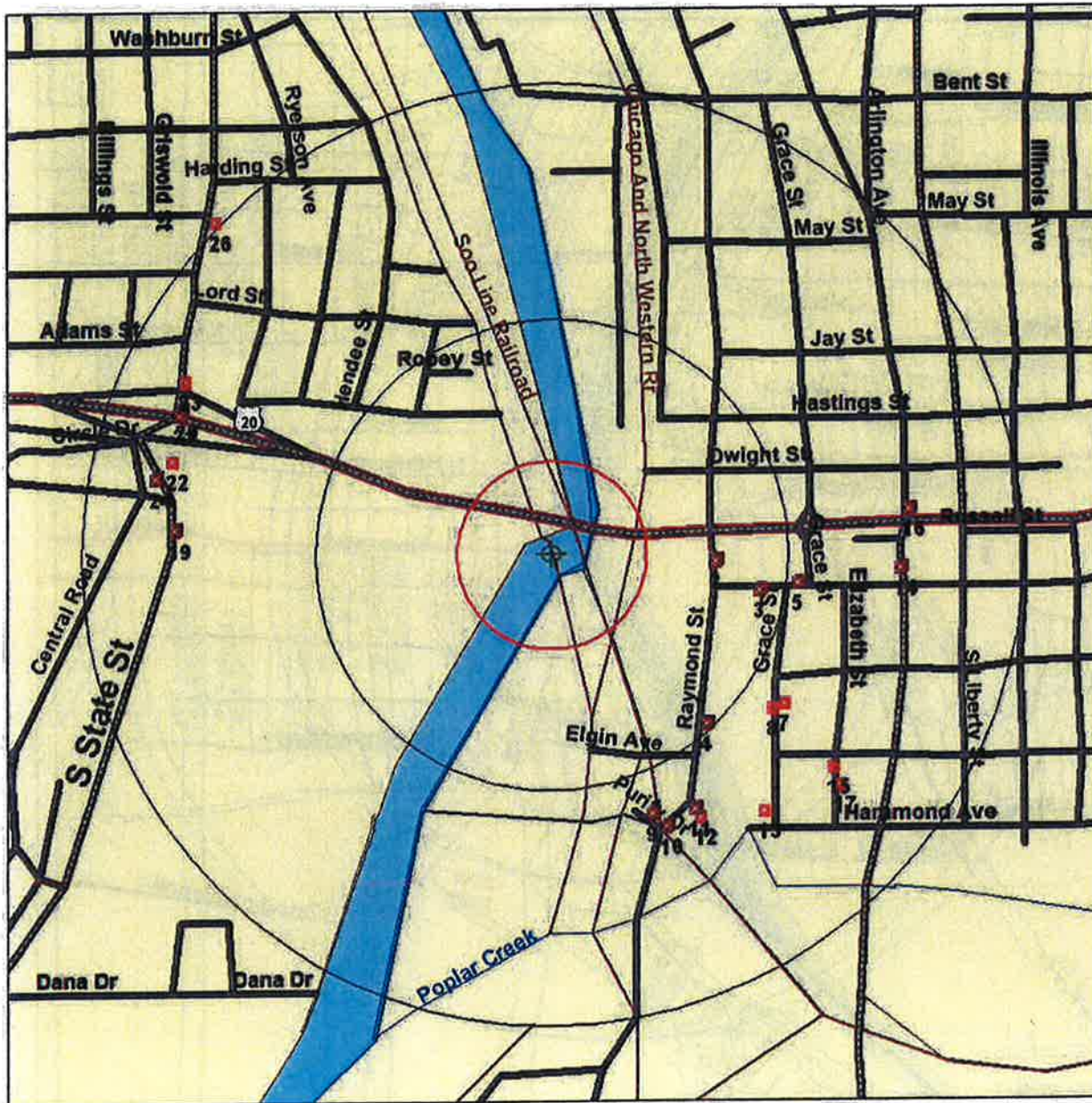


Environmental FirstSearch

.5 Mile Radius
AAI: Multiple Databases



, ELGIN IL 60120



Source: 2005 U.S. Census TIGER Files

- Target Site (Latitude: 42.019552 Longitude: -88.275645)
- Identified Site, Multiple Sites, Receptor
- NPL, DELNPL, Brownfield, Solid Waste Landfill (SWL), Hazardous Waste
- Triballand
- Railroads



Black Rings Represent 1/4 Mile Radius; Red Ring Represents 500 ft. Radius

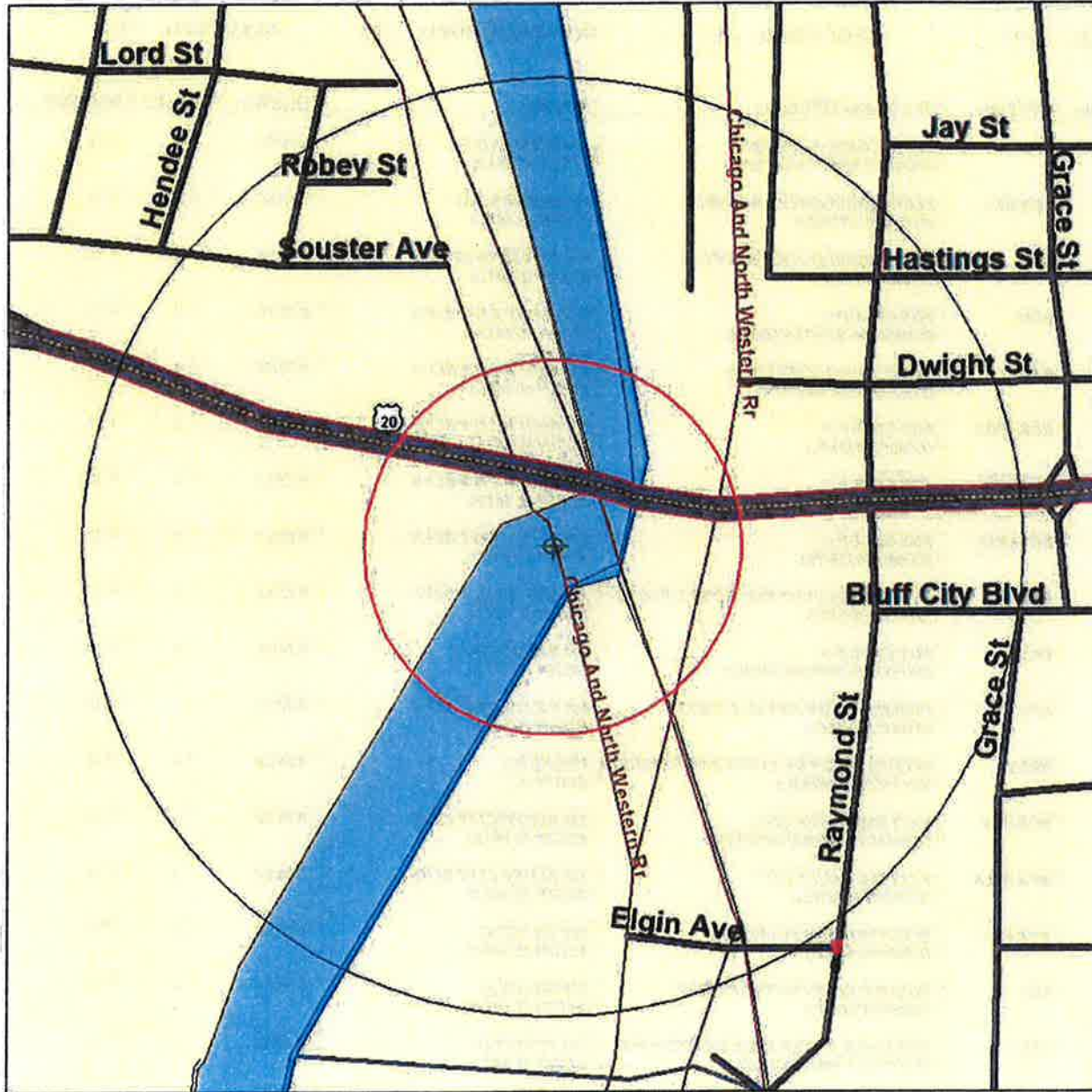


Environmental FirstSearch

.25 Mile Radius
AAI: ERNS, OTHER



, ELGIN IL 60120



Source: 2005 U.S. Census TIGER Files

Target Site (Latitude: 42.019552 Longitude: -88.275645)

Identified Site, Multiple Sites, Receptor

NPL, DELNPL, Brownfield, Solid Waste Landfill (SWL), Hazardous Waste

Triballand.....

Railroads

Black Rings Represent 1/4 Mile Radius; Red Ring Represents 500 ft. Radius



Environmental FirstSearch Sites Summary Report

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

TOTAL: 95 **GEOCODED:** 69 **NON GEOCODED:** 26 **SELECTED:** 95

Page No.	DB Type	Site Name/ID/Status	Address	Dist/Dir	Map ID	ElevDiff
1	VCP	ELGIN CORRUGATED BOX 0894385120/SRP - INACTIVE	824 RAYMOND ST ELGIN IL 60120	0.16 SE	1	+ 36
2	RCRAGN	ELGIN CORRUGATED BOX INC ILD984792077/SGN	824 RAYMOND ST ELGIN IL 60120	0.17 SE	2	+ 36
3	UST	ELGIN CORRUGATED BOX CO 2006255/CLOSED	824 RAYMOND ST ELGIN IL 60120	0.17 SE	2	+ 36
4	LUST	FOX GROUP II 0894380006-991025/CLOSED	363 BLUFF CITY BLVD ELGIN IL 60120	0.22 SE	3	+ 27
6	NFRAP	ELGIN METAL CASKET CO ILD005070529/NFRAP-N	363 BLUFF CITY BLVD ELGIN IL 60120	0.22 SE	3	+ 27
7	RCRACOR	FOX GROUP II ILD005070529/CA	363 BLUFF CITY BLVD ELGIN IL 60120	0.22 SE	3	+ 27
8	RCRAGN	FOX GROUP II ILD005070529/SGN	363 BLUFF CITY BLVD ELGIN IL 60120	0.22 SE	3	+ 27
9	RCRATSD	FOX GROUP II ILD005070529/TSD	363 BLUFF CITY BLVD ELGIN IL 60120	0.22 SE	3	+ 27
10	UST	FORMER ELGIN METAL CASKET FACILITY 2019750/CLOSED	363 BLUFF CITY BLVD ELGIN IL 60120	0.22 SE	3	+ 27
11	LUST	FOX GROUP II 0894385589-991536/CLOSED	901 RAYMOND ST ELGIN IL 60120	0.24 SE	4	+ 19
13	UST	FORMER ELGIN METAL CASKET 2038697/CLOSED	901 S RAYMOND BLVD. ELGIN IL 60120	0.24 SE	4	+ 19
14	ERNS	INTERSECTION OF ELGIN AND RAYMOND NRC-782941/MOBILE	UNKNOWN ELGIN IL	0.26 SE	6	+ 18
17	RCRAGN	KATY IND BLUFF CITY ILD062413570/TRANSPORTER	366 BLUFF CITY BLVD ELGIN IL 60120	0.26 SE	5	+ 29
19	RCRANLR	KATY IND BLUFF CITY ILD062413570/NLR	366 BLUFF CITY BLVD ELGIN IL 60120	0.26 SE	5	+ 29
20	RCRAGN	ELGIN MOLDED PLASTICS ILR000063602/SGN	909 GRACE ST ELGIN IL 60120	0.29 SE	7	+ 16
21	UST	FORMER ELGIN METAL CASKET 2038693/CLOSED	910 GRACE ST ELGIN IL 60120	0.29 SE	8	+ 15
22	LUST	FOX RIVER WATER RECLAMATION DIST. 0894385015-990187/CLOSED	100 PURIFY DR ELGIN IL 60120	0.30 SE	9	+ 15
24	RCRANLR	ELGIN SANITARY DISTRICT CITY OF ILD000672311/NLR	100 PURIFY DR ELGIN IL 60120	0.30 SE	9	+ 15
25	UST	FOX RIVER WATER RECLAMATION PLT 8002129/MERGED	RAYMOND ST and PURIFY DR ELGIN IL 60120	0.30 SE	9	+ 15
26	UST	FKA SANITARY DIST OF ELGIN 2028865/MERGED	RAYMOND ST and PURIFY DR ELGIN IL 60120	0.30 SE	9	+ 15
27	UST	SOUTH REGIONAL WASTE WATER TREATME 2005521/ACTIVE	RAYMOND ST and PURIFY DR ELGIN IL 60120	0.30 SE	9	+ 15

**Environmental FirstSearch
Sites Summary Report**

Target Property:
ELGIN IL 60120

JOB: ELGIN-RR-TRACK

TOTAL: 95 **GEOCODED:** 69 **NON GEOCODED:** 26 **SELECTED:** 95

Page No.	DB Type	Site Name/ID/Status	Address	Dist/Dir	Map ID	ElevDiff
29	RCRAGN	NATIONAL ELECTRO PLATING LTD ILD106928500/LGN	951 RAYMOND ST ELGIN IL 60120	0.31 SE	11	+21
30	RCRAGN	ALPHABET SHOP INC ILD984922203/VGN	300 ELGIN AVE ELGIN IL 60120	0.31 SE	10	+20
32	RCRANLR	NATIONAL ELECTRO PLATING LTD ILD106928500/NLR	951 RAYMOND ST ELGIN IL 60120	0.31 SE	11	+21
33	RCRANLR	FERDON PLASTICS ILD984922203/NLR	300 ELGIN AVE ELGIN IL 60120	0.31 SE	10	+20
34	RCRAGN	FOX GROUP I ILR000065532/SGN	999 RAYMOND ST ELGIN IL 60120	0.32 SE	12	+18
35	UST	FORMER ELGIN METAL CASKET 2038694/ORPILAN FACILITY	1000 GRACE ST ELGIN IL 60120	0.35 SE	13	+9
36	LUST	AMOCO OIL CO. 0894385179-970543/CLOSED	816 SAINT CHARLES RD ELGIN IL 60120	0.37 SE	14	+27
39	RCRAGN	SVENDSEN BROS ILD984802371/VGN	964 ELIZABETH ST ELGIN IL 60120	0.37 SE	15	+19
40	RCRAGN	PRICE RIGHT AMOCO ILD984924613/SGN	816 SAINT CHARLES RD ELGIN IL 60120	0.37 SE	14	+27
41	UST	BP 15478 2010850/ACTIVE	816 CHARLES AND BLUFF CITY ELGIN IL 60120	0.37 SE	14	+27
43	RCRAGN	EMRO MARKETING NO 7095 ILD984781674/LGN	BLUFF CITY/SAINT CHARLES ELGIN IL 60120	0.38 NE	16	+47
44	RCRANLR	EMRO MARKETING NO 7095 ILD984781674/NLR	CHARLES AND BLUFF BLVD ELGIN IL 60120	0.38 NE	16	+47
45	LUST	CHECKER GAS STATION 0894385070-860218	851 ST CHARLES ST ELGIN IL 60120	0.39 SE	18	+26
46	LUST	CHECKER GAS STATION 0894385070-860218/CLOSED	851 ST CHARLES ST ELGIN IL 60120	0.39 SE	18	+26
48	RCRAGN	EASTVIEW MANUFACTURING ILR000014068/VGN	970 ELIZABETH ST ELGIN IL 60120	0.39 SE	17	+19
49	LUST	PACE SUBURBAN BUS 0894385145-980708/CLOSED	975 S STATE ST ELGIN IL 60123	0.40 NW	19	+43
51	RCRAGN	PACE RIVER DIV ILD984787762/SGN	975 S STATE ST ELGIN IL 60123	0.40 NW	19	+43
52	UST	PACE RIVER DIVISION 2025605/ACTIVE	975 S STATE RT 31 ELGIN IL 60123	0.40 NW	19	+43
53	LUST	CMS 0894385079-992177/CLOSED	595 S STATE ST ELGIN IL 60123	0.42 NW	20	+45
55	RCRAGN	ILL DEPT OF TRANS ILD982605990/SGN	595 S STATE ST ELGIN IL 60123	0.42 NW	20	+45

Environmental FirstSearch Sites Summary Report

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

TOTAL: 95 **GEOCODED:** 69 **NON GEOCODED:** 26 **SELECTED:** 95

Page No.	DB Type	Site Name/ID/Status	Address	Dist/Dir	Map ID	ElevDiff
56	RCRAGN	ISP DIST 2 ELGIN HQ ILD984905513/SGN	777 S STATE ST ELGIN IL 60123	0.42 NW	21	+ 47
57	RCRAGN	IL CENTRAL MGMNT SERV DEPT OF VEHI ILD981955271/LGN	595 S STATE ST ELGIN IL 60123	0.42 NW	20	+ 45
58	RCRANLR	IL CENTRAL MGMNT SERV DEPT OF VEHI ILD981955271/NLR	595 S STATE ST ELGIN IL 60123	0.42 NW	20	+ 45
59	RCRANLR	ILL DEPT OF TRANS ILD982605990/NLR	595 S STATE ST ELGIN IL 60123	0.42 NW	20	+ 45
60	UST	ELGIN SANITARY DIST OF 2005523/EXEMPT	875 S STATE ST ELGIN IL 60123	0.42 NW	22	+ 48
61	UST	ELGIN STATE GARAGE 2008417/ACTIVE	595 S STATE ST ELGIN IL 60123	0.42 NW	20	+ 45
62	UST	IL DEPT OF STATE POLICE 2020383/CLOSED	777 S STATE ST ELGIN IL 60123	0.42 NW	21	+ 47
63	DOCKET	ELGIN MENTAL HEALTH CENTER 05-1987-0518/ICIS	750 S STATE ST ELGIN IL 60123	0.43 NW	24	+ 47
64	INSTCONTROL	ELGIN MENTAL HEALTH CENTER 0894385115/SRP-IC	750 S STATE ST ELGIN IL 60123	0.43 NW	24	+ 47
65	UST	PETROLIANCE LLC 2024546/ACTIVE	739 STATE ST ELGIN IL 60123	0.43 NW	23	+ 51
66	UST	VACANT PROPERTY 2043365/CLOSED	750 S STATE ST ELGIN IL 60123	0.43 NW	24	+ 47
67	VCP	RUSS AUTOMOTIVE 0894385694/SRP - INACTIVE	970 LIBERTY ST ELGIN IL 60120	0.49 SE	25	+ 7
68	UST	KATZ and SONS WEST 2018200/CLOSED	438 S STATE ELGIN IL 60123	0.50 NW	26	+ 61
69	NFRAP	DSM DESOTECH INC ILD058587759/NFRAP-N	1122 SAINT CHARLES ST SOUTH ELGIN IL 60177	0.54 SE	27	+ 10
70	RCRACOR	DSM DESOTECH INC ILD058587759/CA	1122 SAINT CHARLES ST SOUTH ELGIN IL 60177	0.54 SE	27	+ 10
71	RCRATSD	DSM DESOTECH INC ILD058587759/TSD	1122 SAINT CHARLES ST SOUTH ELGIN IL 60177	0.54 SE	27	+ 10
74	VCP	DSM DESOTO, INC. 0894380007/SRP - INACTIVE	1122 SAINT CHARLES ST SOUTH ELGIN IL 60177	0.54 SE	27	+ 10
75	VCP	DSM DESOTO, INC. SITE - 285/SRP	1122 SAINT CHARLES ST SOUTH ELGIN IL 60177	0.54 SE	27	+ 10
76	LUST	LEE WARDS CREATIVE CRAFTS 0894385125-921414/CLOSED	1200 ST CHARLES ST SOUTH ELGIN IL 60177	0.61 SE	28	+ 17
79	LUST	BONCOSKY OIL CO. 0894385578-983019/ACTIVE	355 HENDEE ST ELGIN IL 60123	0.62 NW	30	+ 67

**Environmental FirstSearch
Sites Summary Report**

Target Property:

ELGIN IL 60120

JOB: ELGIN-RR-TRACK

TOTAL: 95 **GEOCODED:** 69 **NON GEOCODED:** 26 **SELECTED:** 95

Page No.	DB Type	Site Name/ID/Status	Address	Dist/Dir	Map ID	ElevDiff
82	NFRAP	ILLINOIS TOOL WORKS ILD9990817249/NFRAP-N	1201 ST CHARLES ST ELGIN IL 60120	0.62 SE	29	+ 21
83	LUST	ELGIN MENTAL HEALTH CENTER 0894385115-940022/ACTIVE	750 S STATE ST ELGIN IL 60123	0.68 SW	31	+ 30
85	LUST	ELGIN MENTAL HEALTH CTR. 0894385115-891217/ACTIVE	750 S STATE ST ELGIN IL 60123	0.68 SW	31	+ 30
87	LUST	ELGIN MENTAL HEALTH DEPT. HUMAN SE 0894385115-981344/CLOSED	750 S STATE ST ELGIN IL 60123	0.68 SW	31	+ 30
89	LUST	DHS ELGIN MENTAL HEALTH CTR. 0894385115-981699/ACTIVE	750 S STATE ST ELGIN IL 60123	0.68 SW	31	+ 30
91	VCP	ELGIN MENTAL HEALTH CENTER 0894385115/SRP - INACTIVE	750 S STATE ST ELGIN IL 60123	0.68 SW	31	+ 30
92	LUST	STATE and WALNUT QUIK MART 0314385464-941864/CLOSED	300 S STATE ST ELGIN IL 60123	0.74 NW	33	+ 73
94	LUST	LEWA CO. 0894385446-932527/CLOSED	5 W WALNUT AVE ELGIN IL 60123	0.74 NW	32	+ 22

Environmental FirstSearch Sites Summary Report

Target Property:

ELGIN IL 60120

JOB: ELGIN-RR-TRACK

TOTAL: 95 **GEOCODED:** 69 **NON GEOCODED:** 26 **SELECTED:** 95

Page No.	DB Type	Site Name/ID/Status	Address	Dist/Dir	Map ID	ElevDiff
97	CERCLIS	ELGIN LDFL ILD981960800/PART OF NPL	RT 25 SOUTH ELGIN IL 60177	NON GC		N/A
98	DOCKET	VILLA OLIVIA HQ-2005-8000-388/ICIS	RT. 1 BOX 28 LAKE ST ELGIN IL 60120	NON GC		N/A
99	ERNS	CANADIAN PACIFIC RAILWAY NRC-728904/STORAGE TANK	UNKNOWN ELGIN IL 60120	NON GC		N/A
102	ERNS	CANADIAN PACIFIC RAILYARD MILE POS NRC-650192/RAILROAD	UNKNOWN ELGIN IL 60120	NON GC		N/A
105	ERNS	CANADIAN PACIFIC RAILWAY NRC-796330/RAILROAD	UNKNOWN ELGIN IL	NON GC		N/A
108	ERNS	IN THE RAIL YARD 29 STATE ROAD NRC-912959/RAILROAD NON-RELEASE	IN THE RAIL YARD 29 STATE R ELGIN IL	NON GC		N/A
109	ERNS	NRC-821863/RAILROAD NON-RELEASE	ELGIN and ROUTE 31 ELGIN IL	NON GC		N/A
112	ERNS	MILEPOST 38 NRC-782950/RAILROAD NON-RELEASE	UNKNOWN ELGIN IL	NON GC		N/A
115	FED IC/EC	TRI-COUNTY LANDFILL CO./WASTE MANA ILD048306138-IC/EPA INST CONTROL	STATE ROUTE 25 SOUTH ELGIN IL 60177	NON GC		N/A
118	FED IC/EC	TRI-COUNTY LANDFILL CO./WASTE MANA ILD048306138-EC/EPA ENG CONTROL	STATE ROUTE 25 SOUTH ELGIN IL 60177	NON GC		N/A
124	LUST	CHICAGO GRAVEL CO. 0894385586-991052/CLOSED	SR-25 ELGIN IL 60120	NON GC		N/A
126	LUST	FOX RIVER WATER RECLAMATION DIST. 0894385615-200113/ACTIVE	RT. 31 and DANA ST ELGIN IL 60120	NON GC		N/A
128	LUST	CENTRAL BLACKTOP CO. 0890805023-912099/CLOSED	RT. 31 SOUTH ELGIN IL 60177	NON GC		N/A
130	NPL	TRI-COUNTY LANDFILL/WASTE MGMT ILL ILD048306138/FINAL	RTE 25 SOUTH ELGIN IL 60177	NON GC		N/A
136	RCRANLR	COMMONWEALTH EDISON CO ILD984911412/NLR	W OF FOX RIVER OF STANDISH ELGIN IL 60120	NON GC		N/A
137	RCRANLR	DANA CORP ELGIN PLANT ILD005176375/NLR	SOUTH STATE ST ELGIN IL 60120	NON GC		N/A
138	SPIILLS	20060649	SOUTH LIBERTY and VILLA ELGIN IL	NON GC		N/A
139	SWL	ELGIN COMPOST FACILITY 0894380047/CLOSED	UNKNOWN ELGIN IL 60123	NON GC		N/A
140	SWL	WOODLAND RECYCLING AND DISPOSAL FA 0894830005/CLOSED	UNKNOWN IL 60177	NON GC		N/A
142	UST	PET-AG INC 2035463/EXEMPT	30 W432 ROUTE 20 ELGIN IL 60123	NON GC		N/A

**Environmental FirstSearch
Sites Summary Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

TOTAL: 95 **GEOCODED:** 69 **NON GEOCODED:** 26 **SELECTED:** 95

Page No.	DB Type	Site Name/ID/Status	Address	Dist/Dir	Map ID	ElevDiff
143	UST	LAKE ST BOOSTER STATION 8000785/MERGED	31 W 124 RT 20 ELGIN IL 60120	NON GC		N/A
144	UST	SOUTH ELGIN MARATHON 2042666/ACTIVE	MIDDLE ST/ROUTE 25 SOUTH ELGIN IL 60120	NON GC		N/A
145	UST	WASTE TREATMENT PLANT FOX RIVER WRDANA AND STATE ST 2040582/EXEMPT	ELGIN IL 60120	NON GC		N/A
146	UST	ELGIN PLANT 2018527/CLOSED	RTE 31 PO BOX SOUTH ELGIN IL 60177	NON GC		N/A
147	UST	CHICAGO GRAVEL-ELGIN PIT 2002874/CLOSED	RT 25 ELGIN IL 60123	NON GC		N/A
148	UST	ACE COFFEE BAR, INC. 2002895/CLOSED	30 W 626 ROUTE 20 ELGIN IL 60123	NON GC		N/A

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

VCP

SEARCH ID: 63 **DIST/DIR:** 0.16 SE **ELEVATION:** 737 **MAP ID:** 1

NAME:	ELGIN CORRUGATED BOX	REV:	8/13/10
ADDRESS:	824 RAYMOND ST ELGIN IL 60120 KANE	ID1:	0894385120
CONTACT:	EDWARD CLIFFORD	ID2:	ILD984792077
SOURCE:	IEPA	STATUS:	SRP - INACTIVE
		PHONE:	(317) 879-4484

SITE INFORMATION

DATE ENROLLED: 7/2/2001
ACERAGE: 9.6
SITE TYPE:

REMEDICATION APPLICANT: MR. EDWARD CLIFFORD
4030 VINCENNES ROAD
INDIANAPOLIS, IN 46268-0937
PHONE: (317) 879-4484

CONSULTANT: GREGORY P. VERRET, P.E.
ENVIRON, INTERNATIONAL
156 ST. PETERS CENTRE
ST. PETERS, MO 63376-
PHONE: (636) 498-4447

PROJECT MANAGER: LANDERS
SECTION 4 (Y) LETTER:
NFR LETTER: 6/19/2002
DATE RECORDED: 7/3/2002

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 10 **DIST/DIR:** 0.17 SE **ELEVATION:** 737 **MAP ID:** 2

NAME:	ELGIN CORRUGATED BOX INC	REV:	7/14/10
ADDRESS:	824 RAYMOND ST ELGIN IL 60120 KANE	ID1:	ILD984792077
CONTACT:		ID2:	
SOURCE:	EPA	STATUS:	SGN
		PHONE:	

SITE INFORMATION

CONTACT INFORMATION: BRIAN MCMAHON
824 RAYMOND
ELGIN IL 60120

PHONE: 7087412200

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO

PERMIT WORKLOAD: ---

CLOSURE WORKLOAD: ---

POST CLOSURE WORKLOAD: ---

PERMITTING /CLOSURE/POST-CLOSURE PROGRESS: ---

CORRECTIVE ACTION WORKLOAD: N - NO

GENERATOR STATUS: SQG - SMALL QUANTITY GENERATOR: GENERATES 100 - 1000 KG/MONTH
OF HAZARDOUS WASTE

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

Ignitable waste

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST

SEARCH ID: 30 **DIST/DIR:** 0.17 SE **ELEVATION:** 737 **MAP ID:** 2

NAME: ELGIN CORRUGATED BOX CO
ADDRESS: 824 RAYMOND ST
ELGIN IL 60120

REV: 7/12/10
ID1: 2006255
ID2:
STATUS: CLOSED
PHONE:

CONTACT:
SOURCE: IL FMO

SITE INFORMATION

TOTAL NUMBER OF TANKS: 2

OWNER: ELGIN CORRUGATED BOX CO
824 RAYMOND ST
ELGIN IL 60120

FACILITY TYPE: NONE
GREEN TAG DECAL:
GREEN TAG ISSUED:
GREEN TAG EXPIRED:
SSP INSPECT DATE:
SSP EXPIRATION DATE:

TANK INFORMATION:

TANK NUMBER:	1	CAPACITY:	5000
SUBSTANCE:	DIESEL FUEL	STATUS:	REMOVED
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	3/20/1986		

TANK NUMBER:	2	CAPACITY:	0
SUBSTANCE:		STATUS:	EXEMPT FROM REGISTRATION
LAST USED:	1/1/1950	RED TAG:	
OSFM FIRST NOTIFIED:	2/8/1991		

**Environmental FirstSearch
Site Detail Report**

Target Property:

ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 55 **DIST/DIR:** 0.22 SE **ELEVATION:** 728 **MAP ID:** 3

NAME: FOX GROUP II
ADDRESS: 363 BLUFF CITY BLVD
ELGIN IL 60120

REV: 6/11/10
ID1: 0894380006-991025
ID2: 991025
STATUS: CLOSED
PHONE: 9732566641

CONTACT: LORRAINE KIMBLE
SOURCE: IL EPA

SITE INFORMATION

DATE REPORTED: 4/26/1999
IEMA NUMBER: 991025

CONTENTS/PRODUCT

GASOLINE: NO
UNLEADED GASOLINE: NO
DIESEL FUEL: NO
FUEL OIL: NO
JET FUEL: NO
USED/WASTE OIL: NO
NON-PETROLEUM PRODUCT: NO
PETROLEUM: YES

NON-LUST LETTER SENT:
SEC 57.5G LETTER SENT:
NFR LETTER SENT: 2/29/2000
SITE CLASSIFICATION:
IEPA PROJECT MANAGER: WELLER

IEPA CORRESPONDENCE

DATE: 4/28/1999 **DESCRIPTION:** NOTICE OF RELEASE LETTER SENT
DATE: 11/3/1999 **DESCRIPTION:** PROFESSIONAL ENGINEER CERTIFICATION RECEIVED

TITLE XVI INFORMATION

DOCUMENT: CORRECTIVE
RECEIVED: 11/3/1999
RESPONSE DUE: 3/2/2000
RESPONSE MAILED: 2/29/2000
RESPONSE TYPE: APR

ENGINEERING CONTROLS

BARRIER STRUCTURE: NO **BARRIER PAVEMENT:** NO
BARRIER SOIL: NO **BARRIER OTHER:** NO
BARRIER OTHER DESC:

INSTITUTIONAL CONTROLS

GW USE: NO **INDUST COM:** YES
ORDINANCE: NO **WORKER:** NO
OTHER: NO
OTHER DESC:

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 55 **DIST/DIR:** 0.22 SE **ELEVATION:** 728 **MAP ID:** 3

NAME: FOX GROUP II
ADDRESS: 363 BLUFF CITY BLVD
ELGIN IL 60120

REV: 6/11/10
ID1: 0894380006-991025
ID2: 991025
STATUS: CLOSED
PHONE: 9732566641

CONTACT: LORRAINE KIMBLE
SOURCE: IL EPA

ENVIRONMENTAL LAND USE CONTROLS

GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			

HWY AUTH AGREEMENT: NO
AGREEMENT DESC: No

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

NFRAP			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
2	0.22 SE	728	3
NAME: ELGIN METAL CASKET CO		REV: 7/2/10	
ADDRESS: 363 BLUFF CITY BLVD		ID1: ILD005070529	
ELGIN IL 60120		ID2: 0500140	
CONTACT:		STATUS: NFRAP-N	
SOURCE: EPA		PHONE:	
DESCRIPTION:			
ACTION/QUALITY	AGENCY/RPS	START/RAA	END
ARCHIVE SITE	EPA In-House		10/30/1995
ARCHIVE SITE	EPA In-House		10/30/1995
ARCHIVE SITE	EPA In-House		
DISCOVERY	EPA Fund-Financed		2/1/1984
DISCOVERY	EPA Fund-Financed		2/1/1984
DISCOVERY	EPA Fund-Financed		
HRS PACKAGE	EPA Fund-Financed		
HRS PACKAGE	EPA Fund-Financed		9/29/1985
NFRAP: NO FURTHER REMEDIAL ACTION PLANNED			
HRS PACKAGE	EPA Fund-Financed		9/29/1985
NFRAP: NO FURTHER REMEDIAL ACTION PLANNED			
PRELIMINARY ASSESSMENT	State, Fund Financed		
PRELIMINARY ASSESSMENT	State, Fund Financed		7/1/1984
LOW PRIORITY FOR FURTHER ASSESSMENT			
PRELIMINARY ASSESSMENT	State, Fund Financed		7/1/1984
LOW PRIORITY FOR FURTHER ASSESSMENT			
SITE INSPECTION	EPA Fund-Financed		
SITE INSPECTION	EPA Fund-Financed		10/1/1984
NFRAP: NO FURTHER REMEDIAL ACTION PLANNED			
SITE INSPECTION	EPA Fund-Financed		10/1/1984
NFRAP: NO FURTHER REMEDIAL ACTION PLANNED			

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRACOR	
SEARCH ID: 7	DIST/DIR: 0.22 SE
ELEVATION: 728	MAP ID: 3
NAME: FOX GROUP II ADDRESS: 363 BLUFF CITY BLVD ELGIN IL 60120 KANE CONTACT: SOURCE: EPA	REV: 7/14/10 ID1: ILD005070529 ID2: STATUS: CA PHONE:
 <u>SITE INFORMATION</u>	
CONTACT INFORMATION:	LORRAINE L KIMBLE 101 E MAIN ST LITTLE FALLS NJ 07424
PHONE:	9732566644
 <u>UNIVERSE INFORMATION:</u>	
<u>SUBJECT TO CORRECTIVE ACTION (SUBJCA)</u>	
SUBJCA:	Y - SUBJECT TO CORRECTIVE ACTION
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO
SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	
PERMIT WORKLOAD:	—
CLOSURE WORKLOAD:	—
POST CLOSURE WORKLOAD:	—
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	—S-
CORRECTIVE ACTION WORKLOAD:	N - NO
GENERATOR STATUS:	SQG - SMALL QUANTITY GENERATOR: GENERATES 100 - 1000 KG/MONTH
OF HAZARDOUS WASTE	
INSTITUTIONAL CONTROL:	N
HUMAN EXPOSURE:	
GW CONTROLS:	
LAND TYPE:	P
 <u>NAIC INFORMATION</u>	
 <u>ENFORCEMENT INFORMATION:</u>	
 <u>VIOLATION INFORMATION:</u>	
 <u>CORRECTIVE ACTION INFORMATION</u>	
CA EVENT:	09/01/2009 CA070NO - DETERMINATION OF NEED FOR AN RFI - RFI IS NOT NECESSARY
 <u>HAZARDOUS WASTE INFORMATION:</u>	
F005 - The following spent non-halogenated solvents: toluene, methyl ethyl ketone, carbon disulfide, isobutanol, pyridine, benzene, 2-ethoxyethanol, and 2-nitropropane; all spent solvent mixtures/blends containing, before use, a total of ten percent or more (by volume) of one or more of the above non-halogenated solvents or those solvents listed in F001, F002, or F004; and still bottoms from the recovery of these spent solvents and spent solvent mixtures. F017	

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 14 **DIST/DIR:** 0.22 SE **ELEVATION:** 728 **MAP ID:** 3

NAME:	FOX GROUP II	REV:	12/11/09
ADDRESS:	363 BLUFF CITY BLVD ELGIN IL 60120 KANE	ID1:	ILD005070529
CONTACT:		ID2:	
SOURCE:	EPA	STATUS:	SGN
		PHONE:	

SITE INFORMATION

CONTACT INFORMATION: LORRAINE L KIMBLE
101 E MAIN ST
LITTLE FALLS NJ 07424

PHONE: 9732566644

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	Y - SUBJECT TO CORRECTIVE ACTION
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO
PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---
POST CLOSURE WORKLOAD:	---
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---S-
CORRECTIVE ACTION WORKLOAD:	N - NO
GENERATOR STATUS:	SQG - SMALL QUANTITY GENERATOR: GENERATES 100 - 1000 KG/MONTH
OF HAZARDOUS WASTE	

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

The following spent non-halogenated solvents: toluene, methyl ethyl ketone, carbon disulfide, isobutanol, pyridine, benzene, 2-ethoxyethanol, and 2-nitropropane; all spent solvent mixtures/blends containing, before use, a to F017

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRATSD

SEARCH ID: 5 **DIST/DIR:** 0.22 SE **ELEVATION:** 728 **MAP ID:** 3

NAME: FOX GROUP II
ADDRESS: 363 BLUFF CITY BLVD
ELGIN IL 60120
KANE
CONTACT: LORRAINE L KIMBLE
SOURCE: EPA

REV: 6/8/02
ID1: ILD005070529
ID2:
STATUS: TSD
PHONE: 9732566644

SITE INFORMATION

CONTACT INFORMATION: LORRAINE L KIMBLE
ENVIRON MGR
101 E MAIN ST
LITTLE FALLS NJ 07424

PHONE: 9732566644

CONTACT INFORMATION: THOMAS COX
CHIEF ENGINEER
363 BLUFF CITY BLVD
ELGIN IL 60120

PHONE: 3127424205

UNIVERSE NAME:

TSDS SUBJECT TO CORRECTIVE ACT
DF: LAND DISPOSAL FACILITY
INCINERATOR
ST: STORAGE AND TREATMENT
SUBJECT TO CEI
SUBJECT TO CORRECTIVE ACTION

SIC INFORMATION:

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST

SEARCH ID: 37 **DIST/DIR:** 0.22 SE **ELEVATION:** 728 **MAP ID:** 3

NAME:	FORMER ELGIN METAL CASKET FACILITY	REV:	7/12/10
ADDRESS:	363 BLUFF CITY BLVD ELGIN IL 60120	IDI:	2019750
CONTACT:		IDZ:	
SOURCE:	IL FMO	STATUS:	CLOSED
		PHONE:	

SITE INFORMATION

TOTAL NUMBER OF TANKS: 3

OWNER: GULF and WESTERN PROPERTIES INC
1 GULF and WESTERN PLZ
NEW YORK NY 10023

FACILITY TYPE: NONE

GREEN TAG DECAL:

GREEN TAG ISSUED:

GREEN TAG EXPIRED:

SSP INSPECT DATE:

SSP EXPIRATION DATE:

TANK INFORMATION:

TANK NUMBER:	1	CAPACITY:	99
SUBSTANCE:	HAZARDOUS SUBSTANCE	STATUS:	EXEMPT FROM REGISTRATION
LAST USED:	6/30/1980	RED TAG:	
OSFM FIRST NOTIFIED:	5/8/1986		
TANK NUMBER:	2	CAPACITY:	99
SUBSTANCE:	EMPTY	STATUS:	EXEMPT FROM REGISTRATION
LAST USED:	6/30/1980	RED TAG:	
OSFM FIRST NOTIFIED:	5/8/1986		
TANK NUMBER:	3	CAPACITY:	15000
SUBSTANCE:	HEATING OIL	STATUS:	ABANDONED IN PLACE
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	10/13/1999		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 54 **DIST/DIR:** 0.24 SE **ELEVATION:** 720 **MAP ID:** 4

NAME: FOX GROUP II	REV: 6/11/10
ADDRESS: 901 RAYMOND ST	ID1: 0894385589-991536
ELGIN IL 60120	ID2: 991536
	STATUS: CLOSED
CONTACT: LORRAINE KIMBLE	PHONE: 9732566644
SOURCE: IL EPA	

SITE INFORMATION

DATE REPORTED: 6/24/1999
IEMA NUMBER: 991536

CONTENTS/PRODUCT

GASOLINE: NO
UNLEADED GASOLINE: NO
DIESEL FUEL: YES
FUEL OIL: NO
JET FUEL: NO
USED/WASTE OIL: NO
NON-PETROLEUM PRODUCT: NO
PETROLEUM: NO

NON-LUST LETTER SENT:
SEC 57.5G LETTER SENT:
NFR LETTER SENT: 12/30/1999
SITE CLASSIFICATION:
IEPA PROJECT MANAGER: BLOOME

IEPA CORRESPONDENCE

DATE: 7/13/1999	DESCRIPTION: NOTICE OF RELEASE LETTER SENT
DATE: 11/1/1999	DESCRIPTION: PROFESSIONAL ENGINEER CERTIFICATION RECEIVED

TITLE XVI INFORMATION

DOCUMENT: CORRECTIVE
RECEIVED: 11/1/1999
RESPONSE DUE: 2/29/2000
RESPONSE MAILED: 12/30/1999
RESPONSE TYPE: APR

ENGINEERING CONTROLS

BARRIER STRUCTURE: NO	BARRIER PAVEMENT: NO
BARRIER SOIL: NO	BARRIER OTHER: NO
BARRIER OTHER DESC:	

INSTITUTIONAL CONTROLS

GW USE: NO	INDUST COM: YES
ORDINANCE: NO	WORKER: NO
OTHER: NO	
OTHER DESC:	

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 54 **DIST/DIR:** 0.24 SE **ELEVATION:** 720 **MAP ID:** 4

NAME: FOX GROUP II
ADDRESS: 901 RAYMOND ST
ELGIN IL 60120

REV: 6/11/10
ID1: 0894385589-991536
ID2: 991536
STATUS: CLOSED
PHONE: 9732566644

CONTACT: LORRAINE KIMBLE
SOURCE: IL EPA

ENVIRONMENTAL LAND USE CONTROLS

GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			

HWY AUTH AGREEMENT: NO
AGREEMENT DESC: No

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST			
SEARCH ID: 34	DIST/DIR: 0.24 SE	ELEVATION: 720	MAP ID: 4
NAME: FORMER ELGIN METAL CASKET		REV: 7/12/10	
ADDRESS: 901 S RAYMOND BLVD. ELGIN IL 60120		ID1: 2038697	
		ID2:	
CONTACT:		STATUS: CLOSED	
SOURCE: IL FMO		PHONE:	
<u>SITE INFORMATION</u>			
TOTAL NUMBER OF TANKS:	2		
OWNER:	UNKNOWN UNKNOWN UNKNOWN IL 000000000		
FACILITY TYPE:	NONE		
GREEN TAG DECAL:			
GREEN TAG ISSUED:			
GREEN TAG EXPIRED:			
SSP INSPECT DATE:			
SSP EXPIRATION DATE:			
<u>TANK INFORMATION:</u>			
TANK NUMBER:	1	CAPACITY:	500
SUBSTANCE:	GASOLINE	STATUS:	REMOVED
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	7/21/1999		
TANK NUMBER:	2	CAPACITY:	500
SUBSTANCE:	GASOLINE	STATUS:	REMOVED
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	7/12/1999		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS

SEARCH ID: 77 **DIST/DIR:** 0.26 SE **ELEVATION:** 719 **MAP ID:** 6

NAME: INTERSECTION OF ELGIN AND RAYMOND	REV: 12/31/05
ADDRESS: UNKNOWN ELGIN IL KANE	ID1: NRC-782941
CONTACT: UNKNOWN	ID2:
SOURCE: NRC	STATUS: MOBILE
	PHONE:

SITE INFORMATION

THIS INFORMATION WAS OBTAINED FROM THE NATIONAL RESPONSE CENTER

DATE RECEIVED: 12/19/2005 6:48:17 PM	12/19/2005 6:43:07 PM	DATE COMPLETE:
CALL TAKER: TMM0760	CALL TYPE:	INC

RESPONSIBLE PARTY: UNKNOWN
PHONE 1:
PHONE 2:
PHONE 3:

RESPONSIBLE COMPANY:
ORGANIZATION TYPE: UNKNOWN

ADDRESS: XX

SOURCE: TELEPHONE

INCIDENT INFORMATION

INCIDENT DESCRIPTION: CALLER IS REPORTING A RELEASE OF MATERIALS FROM A TRUCK DUE TO A COLLISION WITH A TRAIN. THE TRAIN WAS A METRO PASSENGER. CALLER DID NOT HAVE ANY OTHER RAILROAD DETAILS.

INCIDENT TYPE: MOBILE	INCIDENT CAUSE: TRANSPORT ACCIDENT
INCIDENT DATE: 12/19/2005 4:49:00 PM	INCIDENT DATE DESC:

OCCURRED	DISTANCE UNITS:
DISTANCE FROM CITY:	LOCATION SECTION:
DIRECTION FROM CITY:	LOCATION RANGE:
LOCATION TOWNSHIP:	

AIRCRAFT TYPE:	AIRCRAFT MODEL:
AIRCRAFT ID:	AIRCRAFT FUEL CAPACITY:
AIRCRAFT FUEL CAPACITY UNITS:	AIRCRAFT FUEL ON BOARD:
AIRCRAFT FUEL ON BOARD UNITS:	AIRCRAFT SPOT NUMBER:
AIRCRAFT HANGER:	AIRCRAFT RUNWAY NUM:
ROAD MILE MARKER:	BUILDING ID:
TYPE OF FIXED OBJECT:	POWER GEN FACILITY: U
GENERATING CAPACITY:	TYPE OF FUEL:
NPDES:	NPDES COMPLIANCE: U
PIPELINE TYPE:	DOT REGULATED: U
PIPELINE ABOVE GROUND: ABOVE	EXPOSED UNDERWATER: N
PIPELINE COVERED: U	GRADE CROSSING: N
LOCATION SUBDIVISION:	RAILROAD MILEPOST:
TYPE VEHICLE INVOLVED:	CROSSING DEVICE TYPE:
DEVICE OPERATIONAL: Y	

DOT CROSSING NUMBER:	BRAKE FAILURE: N
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- Continued on next page -

Environmental FirstSearch Site Detail Report

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS	
SEARCH ID: 77	DIST/DIR: 0.26 SE ELEVATION: 719 MAP ID: 6
NAME: INTERSECTION OF ELGIN AND RAYMOND	REV: 12/31/05
ADDRESS: UNKNOWN	ID1: NRC-782941
ELGIN IL	ID2:
KANE	STATUS: MOBILE
CONTACT: UNKNOWN	PHONE:
SOURCE: NRC	
TANK ABOVE GROUND: ABOVE	TRANSPORTABLE CONTAINER: U
TANK REGULATED: U	TANK REGULATED BY:
TANK ID:	CAPACITY OF TANK:
CAPACITY OF TANK UNITS:	ACTUAL AMOUNT:
ACTUAL AMOUNT UNITS:	PLATFORM RIG NAME:
PLATFORM LETTER:	LOCATION AREA ID:
LOCATION BLOCK ID:	
DESCRIPTION OF TANK:	
OCSP NUMBER:	OCSP NUMBER:
STATE LEASE NUMBER:	PIER DOCK NUMBER:
BERTH SLIP NUMBER:	CONTIN RELEASE TYPE:
INITIAL CONT RELEASE NUM:	CONT RELEASE PERMIT:
ALLISION: N	TYPE OF STRUCTURE:
STRUCTURE NAME:	STRUCT OPERATIONAL: U
AIRBAG DEPLOYED:	DATE NORMAL SERVICE:
SERVICE DISRUPT TIME:	SERVICE DISRUPT UNITS:
TRANSIT BUS FLAG:	CR BEGIN DATE:
CR END DATE:	CR CHANGE DATE:
FIRE INVOLVED: N	FIRE EXTINGUISHED: U
ANY EVACUATIONS: N	NUMBER EVACUATED:
WHO EVACUATED:	RADIUS OF EVACUATION:
ANY INJURIES: N	NUMBER INJURED:
NUMBER HOSPITALIZED:	ANY FATALITIES: Y
NUMBER FATALITIES: 1	ANY DAMAGES: N
DAMAGE AMOUNT:	AIR CORRIDOR CLOSED: N
AIR CORRIDOR DESC:	AIR CLOSURE TIME:
WATERWAY CLOSED: N	WATERWAY DESC:
WATERWAY CLOSURE TIME:	ROAD CLOSED: N
ROAD DESC:	ROAD CLOSURE TIME:
CLOSURE DIRECTION:	MAJOR ARTERY: N
TRACK CLOSED: Y	TRACK DESC: CNW MAIN
TRACK CLOSURE TIME:	MEDIA INTEREST: NONE
MEDIUM DESC: BALLAST	ADDTL MEDIUM INFO:
BODY OF WATER:	TRIBUTARY OF:
NEAREST RIVER MILE MARK:	RELEASE SECURED: U
EST DUR OF RELEASE:	RELEASE RATE:
TRACK CLOSE DIR: ALL	ST AGENCY ON SCENE:
ST AGENCY RPT NUM:	OTHER AGENCY NOTIFIED:
WEATHER CONDITIONS:	AIR TEMPERATURE:
WIND SPEED:	WIND DIRECTION:
WATER SUPPLY CONTAM: U	SHEEN SIZE:
SHEEN COLOR:	DIR OF SHEEN TRAVEL:
SHEEN ODOR DESCRIPTION:	WAVE CONDITION:
CURRENT SPEED:	CURRENT DIRECTION:
WATER TEMPERATURE:	
DESC OF REMEDIAL ACTION:	INVESTIGATION UNDERWAY, POLICE AND FIRE DEPT RESPONDED.
EMPL FATALITY:	PASS FATALITY:

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS

SEARCH ID: 77 **DIST/DIR:** 0.26 SE **ELEVATION:** 719 **MAP ID:** 6

NAME:	INTERSECTION OF ELGIN AND RAYMOND	REV:	12/31/05
ADDRESS:	UNKNOWN ELGIN IL KANE	ID1:	NRC-782941
CONTACT:	UNKNOWN	ID2:	
SOURCE:	NRC	STATUS:	MOBILE
		PHONE:	

COMMUNITY IMPACT:	N	WIND SPEED UNITS:	
EMPLOYEE INJURIES:		PASSENGER INJURIES:	
OCCUPANT FATALITY:	I	CURRENT SPEED UNITS:	
ROAD CLOSURE UNITS:		TRACK CLOSURE UNITS:	
SHEEN SIZE UNITS:		STATE AGENCY NOTIFIED:	NONE
FED AGENCY NOTIFIED:	NONE	NEAREST RIVER MILE MARK:	
SHEEN SIZE LENGTH:		SHEEN SIZE LENGTH UNITS:	
SHEEN SIZE WIDTH:		SHEEN SIZE WIDTH UNITS:	
OFFSHORE:	N	DURATION UNIT:	
RELEASE RATE UNIT:		RELEASE RATE RATE:	

ADDITIONAL INFO: 150 TO 200 PASSENGERS WILL BE TRANSFERRED TO ANOTHER TRAIN.

MATERIAL INFORMATION

CHRIS CODE:	GAS	CASE NUMBER:	000000-00-0
UN NUMBER:		REACHED WATER:	NO
NAME OF MATERIAL:	GASOLINE: AUTOMOTIVE (UNLEADED)		
AMOUNT OF MATERIAL:	0 UNKNOWN AMOUNT		
AMOUNT IN WATER:			

OTHER MATERIAL INFORMATION

VEHICLE NUMBER:	UNKNOWN	TRAILER NUMBER:	
VEHICLE FUEL CAPACITY:		CARGO CAPACITY:	
AMOUNT OF CARGO ON BOARD:		HAZMAT CARRIER:	U
CARRIER LICENSED:	U	NONCOMPLIANCE WITH HAZMAT:	U
MOBILE TYPE:	PASSENGER TRUCK	VEHICLE YEAR:	
VEHICLE MAKE:		VEHICLE MODEL:	

MOBILE DETAILS INFORMATION

TRAIN INFORMATION

VESSEL INFORMATION

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 18 **DIST/DIR:** 0.26 SE **ELEVATION:** 730 **MAP ID:** 5

NAME: KATY IND BLUFF CITY	REV: 7/14/09
ADDRESS: 366 BLUFF CITY BLVD	ID1: ILD062413570
ELGIN IL 60120	ID2:
KANE	STATUS: TRANSPORTER
CONTACT:	PHONE:
SOURCE: EPA	

SITE INFORMATION

CONTACT INFORMATION: RICHARD NELSON
853 DUNDEE AVE
ELGIN IL 60120

PHONE: 7086978900

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO
PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---
POST CLOSURE WORKLOAD:	---
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---
CORRECTIVE ACTION WORKLOAD:	N - NO
GENERATOR STATUS:	N

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

Spent stripping and cleaning bath solutions from electroplating operations in which cyanides are used in the process.
D000
Corrosive waste
Cadmium
Wastewater treatment sludges from electroplating operations except from the following processes: (1) sulfuric acid anodizing of aluminum; (2) tin plating on

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 18 **DIST/DIR:** 0.26 SE **ELEVATION:** 730 **MAP ID:** 5

NAME: KATY IND BLUFF CITY
ADDRESS: 366 BLUFF CITY BLVD
ELGIN IL 60120
KANE

REV: 7/14/09
ID1: ILD062413570
ID2:
STATUS: TRANSPORTER
PHONE:

CONTACT:
SOURCE: EPA

carbon steel; (3) zinc plating (segregated basis) on carbon steel;

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRANLR	
SEARCH ID: 27	DIST/DIR: 0.26 SE
ELEVATION: 730	MAP ID: 5
NAME: KATY IND BLUFF CITY	REV: 7/14/10
ADDRESS: 366 BLUFF CITY BLVD	ID1: ILD062413570
ELGIN IL 60120	ID2:
KANE	STATUS: NLR
CONTACT:	PHONE:
SOURCE: EPA	
 <u>SITE INFORMATION</u>	
CONTACT INFORMATION:	RICHARD NELSON 853 DUNDEE AVE ELGIN IL 60120
PHONE:	7086978900
 <u>UNIVERSE INFORMATION:</u>	
<u>SUBJECT TO CORRECTIVE ACTION (SUBJCA)</u>	
SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO
SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	
PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---
POST CLOSURE WORKLOAD:	---
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---
CORRECTIVE ACTION WORKLOAD:	N - NO
GENERATOR STATUS:	N
INSTITUTIONAL CONTROL:	N
HUMAN EXPOSURE:	
GW CONTROLS:	
LAND TYPE:	E
 <u>NAIC INFORMATION</u>	
 <u>ENFORCEMENT INFORMATION:</u>	
 <u>VIOLATION INFORMATION:</u>	
 <u>HAZARDOUS WASTE INFORMATION:</u>	
D000	
D002 - Corrosive waste	
D006 - Cadmium	
F006 - Wastewater treatment sludges from electroplating operations except from the following processes: (1) sulfuric acid anodizing of aluminum; (2) tin plating on carbon steel; (3) zinc plating (segregated basis) on carbon steel; (4) aluminum or zinc-aluminum plating on carbon steel; (5) cleaning/stripping associated with tin, zinc, and aluminum plating on carbon steel; and (6) chemical etching and milling of aluminum.	
F009 - Spent stripping and cleaning bath solutions from electroplating operations in which cyanides are used in the process.	

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST			
SEARCH ID: 36	DIST/DIR: 0.29 SE	ELEVATION: 716	MAP ID: 8
NAME: FORMER ELGIN METAL CASKET		REV: 7/12/10	
ADDRESS: 910 GRACE ST ELGIN IL 60120		ID1: 2038693	
		ID2:	
CONTACT:		STATUS: CLOSED	
SOURCE: IL FMO		PHONE:	
<u>SITE INFORMATION</u>			
TOTAL NUMBER OF TANKS:	2		
OWNER:	UNKNOWN UNKNOWN UNKNOWN IL 000000000		
FACILITY TYPE:	NONE		
GREEN TAG DECAL:			
GREEN TAG ISSUED:			
GREEN TAG EXPIRED:			
SSP INSPECT DATE:			
SSP EXPIRATION DATE:			
<u>TANK INFORMATION:</u>			
TANK NUMBER:	1	CAPACITY:	3000
SUBSTANCE:	HAZARDOUS SUBSTANCE	STATUS:	REMOVED
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:			
TANK NUMBER:	2	CAPACITY:	3000
SUBSTANCE:	HAZARDOUS SUBSTANCE	STATUS:	REMOVED
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:			

**Environmental FirstSearch
Site Detail Report**

Target Property:

ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 56 **DIST/DIR:** 0.30 SE **ELEVATION:** 716 **MAP ID:** 9

NAME:	FOX RIVER WATER RECLAMATION DIST.	REV:	6/11/10
ADDRESS:	100 PURIFY DR	ID1:	0894385015-990187
	ELGIN IL 60121	ID2:	990187
	KANE	STATUS:	CLOSED
CONTACT:	GREGORY HERGENROEDER	PHONE:	8477242068
SOURCE:	IL EPA		

SITE INFORMATION

DATE REPORTED: 1/28/1999
IEMA NUMBER: 990187

CONTENTS/PRODUCT

GASOLINE: NO
UNLEADED GASOLINE: NO
DIESEL FUEL: YES
FUEL OIL: NO
JET FUEL: NO
USED/WASTE OIL: NO
NON-PETROLEUM PRODUCT: NO
PETROLEUM: NO

NON-LUST LETTER SENT:
SEC 57.5G LETTER SENT:
NFR LETTER SENT: 6/3/1999
SITE CLASSIFICATION:
IEPA PROJECT MANAGER: KAISER

IEPA CORRESPONDENCE

DATE: 6/9/1999	DESCRIPTION:	MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 2/4/1999	DESCRIPTION:	NOTICE OF RELEASE LETTER SENT
DATE: 3/22/1999	DESCRIPTION:	PROFESSIONAL ENGINEER CERTIFICATION RECEIVED

TITLE XVI INFORMATION

DOCUMENT: CORRECTIVE
RECEIVED: 3/22/1999
RESPONSE DUE: 7/20/1999
RESPONSE MAILED: 6/3/1999
RESPONSE TYPE: APR

ENGINEERING CONTROLS

BARRIER STRUCTURE:	NO	BARRIER PAVEMENT:	NO
BARRIER SOIL:	NO	BARRIER OTHER:	NO
BARRIER OTHER DESC:			

INSTITUTIONAL CONTROLS

GW USE:	NO	INDUST COM:	NO
ORDINANCE:	NO	WORKER:	NO
OTHER:	NO		

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST			
SEARCH ID: 56	DIST/DIR: 0.30 SE	ELEVATION: 716	MAP ID: 9
NAME: FOX RIVER WATER RECLAMATION DIST.		REV: 6/11/10	
ADDRESS: 100 PURIFY DR		ID1: 0894385015-990187	
ELGIN IL 60121		ID2: 990187	
KANE		STATUS: CLOSED	
CONTACT: GREGORY HERGENROEDER		PHONE: 8477242068	
SOURCE: IL EPA			
OTHER DESC:			
ENVIRONMENTAL LAND USE CONTROLS			
GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			
HWY AUTH AGREEMENT:	NO		
AGREEMENT DESC:	No		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRANLR

SEARCH ID: 23 **DIST/DIR:** 0.30 SE **ELEVATION:** 716 **MAP ID:** 9

NAME: ELGIN SANITARY DISTRICT CITY OF	REV: 7/14/10
ADDRESS: 100 PURIFY DR	ID1: ILD000672311
ELGIN IL 60120	ID2:
KANE	STATUS: NLR
CONTACT:	PHONE:
SOURCE: EPA	

SITE INFORMATION

CONTACT INFORMATION: ALBIN PAGORSKI
PO BOX 92 RAYMOND ST/PURIFY DR
ELGIN IL 60120

PHONE: 3127422068

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO
PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---
POST CLOSURE WORKLOAD:	---
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---
CORRECTIVE ACTION WORKLOAD:	N - NO
GENERATOR STATUS:	N

NAIC INFORMATION

22132 - SEWAGE TREATMENT FACILITIES

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

Cadmium

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST			
SEARCH ID: 38	DIST/DIR: 0.30 SE	ELEVATION: 716	MAP ID: 9
NAME: FOX RIVER WATER RECLAMATION PLT		REV: 6/17/08	
ADDRESS: RAYMOND ST and PURIFY DR		ID1: 8002129	
ELGIN IL		ID2:	
KANE		STATUS: MERGED	
CONTACT: PAGORSKI ALBIN D		PHONE: (708) 742-2068	
SOURCE: IL FMO			
<u>SITE INFORMATION</u>			
OWNER:	ELGIN SANITARY DISTRICT		
	ELGIN IL 60121		
1998 DECAL:	NONE		
ENFORCE, ORDER:	N		
FEES OWED:			
<u>PERMIT NUMBER</u>	<u>EXPIRATION DATE</u>		
<u>TANK INFORMATION:</u>			

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST

SEARCH ID: 33 **DIST/DIR:** 0.30 SE **ELEVATION:** 716 **MAP ID:** 9

NAME:	FKA SANITARY DIST OF ELGIN	REV:	6/17/08
ADDRESS:	RAYMOND ST and PURIFY DR	ID1:	2028865
	ELGIN IL	ID2:	
	KANE	STATUS:	MERGED
CONTACT:	PAGORSKI ALBIN D	PHONE:	(708) 742-2068
SOURCE:	IL FMO		

SITE INFORMATION

TOTAL NUMBER OF TANKS: 2

OWNER: FOX RIVER WATER RECLAMATION DISTRICT
RAYMOND STREET and PURIFY DRIVE
ELGIN IL 60121

1998 DECAL: NONE
ENFORCE. ORDER: N
FEES OWED: N

<u>PERMIT NUMBER</u>	<u>EXPIRATION DATE</u>
-----------------------------	-------------------------------

TANK INFORMATION:

TANK NUMBER:	1	CAPACITY:	2500
SUBSTANCE:	USED OIL	STATUS:	MOVED
AGE:	7	LAST USED:	
RED TAG:	N		
TANK NUMBER:	2	CAPACITY:	4000
SUBSTANCE:	DIESEL FUEL	STATUS:	MOVED
AGE:	7	LAST USED:	
RED TAG:	N		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST

SEARCH ID: 43 **DIST/DIR:** 0.30 SE **ELEVATION:** 716 **MAP ID:** 9

NAME:	SOUTH REGIONAL WASTE WATER TREATMENT PLA	REV:	7/12/10
ADDRESS:	RAYMOND ST and PURIFY DR	ID1:	2005521
	ELGIN IL 60121	ID2:	
	KANE	STATUS:	ACTIVE
CONTACT:		PHONE:	
SOURCE:	IL FMO		

SITE INFORMATION

TOTAL NUMBER OF TANKS: 7

OWNER: FOX RIVER WATER RECLAMATION DISTRICT
P.O. BOX 328 RAYMOND STREET and PURIFY DRIVE
ELGIN IL 60121

FACILITY TYPE: UTILITY

GREEN TAG DECAL: L000029

GREEN TAG ISSUED: 1/14/2010

GREEN TAG EXPIRED: 12/31/2012

SSP INSPECT DATE:

SSP EXPIRATION DATE:

TANK INFORMATION:

TANK NUMBER:	1	CAPACITY:	4050
SUBSTANCE:	GASOLINE	STATUS:	CURRENTLY IN USE
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	3/26/1986		
TANK NUMBER:	2	CAPACITY:	560
SUBSTANCE:	DIESEL FUEL	STATUS:	REMOVED
LAST USED:	11/12/1998	RED TAG:	
OSFM FIRST NOTIFIED:	3/26/1986		
TANK NUMBER:	3	CAPACITY:	550
SUBSTANCE:	USED OIL	STATUS:	REMOVED
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	3/26/1986		
TANK NUMBER:	4	CAPACITY:	30940
SUBSTANCE:		STATUS:	EXEMPT FROM REGISTRATION
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	3/26/1986		
TANK NUMBER:	5	CAPACITY:	2000
SUBSTANCE:	USED OIL	STATUS:	CURRENTLY IN USE
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	10/29/1992		
TANK NUMBER:	6	CAPACITY:	4000
SUBSTANCE:	DIESEL FUEL	STATUS:	CURRENTLY IN USE
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	10/29/1992		
TANK NUMBER:	7	CAPACITY:	0
SUBSTANCE:	UNKNOWN	STATUS:	DOES NOT EXIST
LAST USED:		RED TAG:	

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST

SEARCH ID: 43 **DIST/DIR:** 0.30 SE **ELEVATION:** 716 **MAP ID:** 9

NAME: SOUTH REGIONAL WASTE WATER TREATMENT PLA
ADDRESS: RAYMOND ST and PURIFY DR
ELGIN IL 60121
KANE

REV: 7/12/10
ID1: 2005521
ID2:
STATUS: ACTIVE
PHONE:

CONTACT:
SOURCE: IL FMO

OSFM FIRST NOTIFIED:

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 19 **DIST/DIR:** 0.31 SE **ELEVATION:** 722 **MAP ID:** 11

NAME:	NATIONAL ELECTRO PLATING LTD	REV:	6/6/06
ADDRESS:	951 RAYMOND ST ELGIN IL 60120 KANE	ID1:	ILD106928500
CONTACT:	ALLEN SKJOLDAGER	ID2:	
SOURCE:	EPA	STATUS:	LGN
		PHONE:	3127415946

SITE INFORMATION

CONTACT INFORMATION: ALLEN SKJOLDAGER
1482 MAPLE LN
ELGIN IL 60120

PHONE: 3127415946

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO
PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---
POST CLOSURE WORKLOAD:	---
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---
CORRECTIVE ACTION WORKLOAD:	N - NO
GENERATOR STATUS:	LQG - LARGE QUANTITY GENERATORS: GENERATES MORE THAN 1000
KG/MONTH OF HAZARDOUS WASTE	

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

Corrosive waste
Wastewater treatment sludges from electroplating operations except from the following processes: (1) sulfuric acid anodizing of aluminum; (2) tin plating on carbon steel; (3) zinc plating (segregated basis) on carbon steel;

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 8 **DIST/DIR:** 0.31 SE **ELEVATION:** 721 **MAP ID:** 10

NAME: ALPHABET SHOP INC	REV: 7/14/10
ADDRESS: 300 ELGIN AVE	ID1: ILD984922203
ELGIN IL 60120	ID2:
KANE	STATUS: VGN
CONTACT:	PHONE:
SOURCE: EPA	

SITE INFORMATION

CONTACT INFORMATION: SHELDON BERNSTEIN
300 E ELGIN AVE
ELGIN IL 60120

PHONE: 8478883150

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO

PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---
POST CLOSURE WORKLOAD:	---
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---
CORRECTIVE ACTION WORKLOAD:	N - NO

GENERATOR STATUS: CEG - CONDITIONALLY EXEMPT SMALL QUANTITY GENERATORS: GENERATES LESS THAN 100 KG/MONTH OF HAZA

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

The following spent non-halogenated solvents: toluene, methyl ethyl ketone, carbon disulfide, isobutanol, pyridine, benzene, 2-ethoxyethanol, and 2-nitropropane; all spent solvent mixtures/blends containing, before use, a to

Methyl ethyl ketone

Ignitable waste

The following spent non-halogenated solvents: Xylene, acetone, ethyl acetate, ethyl benzene, ethyl ether, methyl isobutyl ketone, n-butyl alcohol, cyclohexanone,

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 8 **DIST/DIR:** 0.31 SE **ELEVATION:** 721 **MAP ID:** 10

NAME: ALPHABET SHOP INC
ADDRESS: 300 ELGIN AVE
ELGIN IL 60120
KANE

REV: 7/14/10
ID1: ILD984922203
ID2:
STATUS: VGN
PHONE:

CONTACT:
SOURCE: EPA

and methanol; all spent solvent mixtures/ blends containing, b

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRANLR

SEARCH ID: 28 **DIST/DIR:** 0.31 SE **ELEVATION:** 722 **MAP ID:** 11

NAME:	NATIONAL ELECTRO PLATING LTD	REV:	7/14/10
ADDRESS:	951 RAYMOND ST	ID1:	ILD106928500
	ELGIN IL 60120	ID2:	
	KANE	STATUS:	NLR
CONTACT:		PHONE:	
SOURCE:	EPA		

CONTACT INFORMATION:

ENV COORDINATOR
8477415946

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA CA BASELINE UNIVERSE: NO

GPRA CA 2008: NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA: NO

SUBJCA TSD 3004: NO

SUBJCA NON TSD: NO

SUBJCA TSD DISCRETION: NO

PERMIT WORKLOAD: —

CLOSURE WORKLOAD: —

POST CLOSURE WORKLOAD: —

PERMITTING /CLOSURE/POST-CLOSURE PROGRESS: —

CORRECTIVE ACTION WORKLOAD: NO

GENERATOR STATUS: NO

TRANSPORTER: NO

UNIVERSAL WASTE: NO

RECYCLER: NO

USED OIL: NO

IMPORTER: NO

MIXED WASTE GENERATOR: N

ONSITE BURNER EXEMPT: NO

FURNACE EXEMPTION: NO

UNDERGROUND INJECTION: NO

NAIC 1:

NAIC 2:

NAIC 3:

NAIC 4:

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRANLR

SEARCH ID: 24 **DIST/DIR:** 0.31 SE **ELEVATION:** 721 **MAP ID:** 10

NAME: FERDON PLASTICS
ADDRESS: 300 ELGIN AVE
ELGIN IL 60120
KANE
CONTACT:
SOURCE: EPA

REV: 8/8/01
ID1: ILD984922203
ID2:
STATUS: NLR
PHONE:

SITE INFORMATION

UNIVERSE NAME:

NO LONGER REGULATED

SIC INFORMATION:

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 13 **DIST/DIR:** 0.32 SE **ELEVATION:** 719 **MAP ID:** 12

NAME:	FOX GROUP I	REV:	7/14/10
ADDRESS:	999 RAYMOND ST ELGIN IL 60120 KANE	ID1:	ILR000065532
CONTACT:		ID2:	
SOURCE:	EPA	STATUS:	SGN
		PHONE:	

SITE INFORMATION

CONTACT INFORMATION: LORRAINE KIMBLE
101 E MAIN ST
LITTLE FALLS NJ 07424

PHONE: 9732566644

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO

PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---

POST CLOSURE WORKLOAD:	---
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---

CORRECTIVE ACTION WORKLOAD:	N - NO
GENERATOR STATUS:	SQG - SMALL QUANTITY GENERATOR: GENERATES 100 - 1000 KG/MONTH
OF HAZARDOUS WASTE	

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST			
SEARCH ID: 45	DIST/DIR: 0.37 SE	ELEVATION: 728	MAP ID: 14
NAME: AMOCO OIL CO. ADDRESS: 816 SAINT CHARLES RD ELGIN IL 60120	REV: 6/11/10 ID1: 0894385179-970543 ID2: 970543 STATUS: CLOSED PHONE: 6308366374		
CONTACT: LYLE BRUCE SOURCE: IL EPA			
RESPONSE TYPE:	APR		
DOCUMENT:	GROUNDWATE		
RECEIVED:	12/2/2003		
RESPONSE DUE:	3/31/2004		
RESPONSE MAILED:	3/19/2004		
RESPONSE TYPE:	DEN		
DOCUMENT:	LOW PRIORI		
RECEIVED:	11/8/2000		
RESPONSE DUE:	3/8/2001		
RESPONSE MAILED:	1/24/2001		
RESPONSE TYPE:	APR		
DOCUMENT:	GROUNDWATE		
RECEIVED:	7/30/2004		
RESPONSE DUE:	11/27/2004		
RESPONSE MAILED:	10/13/2004		
RESPONSE TYPE:	APR		
DOCUMENT:	SITE CLASS		
RECEIVED:	3/31/1998		
RESPONSE DUE:	7/29/1998		
RESPONSE MAILED:	5/4/1998		
RESPONSE TYPE:	DEN		
DOCUMENT:	GROUNDWATE		
RECEIVED:	1/29/2003		
RESPONSE DUE:	5/29/2003		
RESPONSE MAILED:	2/18/2003		
RESPONSE TYPE:	APR		
DOCUMENT:	GROUNDWATE		
RECEIVED:	2/11/2002		
RESPONSE DUE:	6/11/2002		
RESPONSE MAILED:	2/26/2002		
RESPONSE TYPE:	APR		
<u>ENGINEERING CONTROLS</u>			
BARRIER STRUCTURE:	NO	BARRIER PAVEMENT:	NO
BARRIER SOIL:	NO	BARRIER OTHER:	NO
BARRIER OTHER DESC:			
<u>INSTITUTIONAL CONTROLS</u>			
GW USE:	NO	INDUST COM:	NO
ORDINANCE:	NO	WORKER:	NO
OTHER:	NO		
OTHER DESC:			

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 45 **DIST/DIR:** 0.37 SE **ELEVATION:** 728 **MAP ID:** 14

NAME: AMOCO OIL CO.
ADDRESS: 816 SAINT CHARLES RD
ELGIN IL 60120

REV: 6/11/10
ID1: 0894385179-970543
ID2: 970543
STATUS: CLOSED
PHONE: 6308366374

CONTACT: LYLE BRUCE
SOURCE: IL EPA

ENVIRONMENTAL LAND USE CONTROLS

GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			

HWY AUTH AGREEMENT: NO
AGREEMENT DESC: No

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 22 **DIST/DIR:** 0.37 SE **ELEVATION:** 720 **MAP ID:** 15

NAME: SVENDSEN BROS	REV: 7/14/10
ADDRESS: 964 ELIZABETH ST ELGIN IL 60120 KANE	ID1: ILD984802371
CONTACT:	ID2:
SOURCE: EPA	STATUS: VGN
	PHONE:

SITE INFORMATION

CONTACT INFORMATION: MORRIS SVENDSEN
964 ELIZABETH ST
ELGIN IL 60120

PHONE: 7087424622

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO
PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---
POST CLOSURE WORKLOAD:	---
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---
CORRECTIVE ACTION WORKLOAD:	N - NO

GENERATOR STATUS: CEG - CONDITIONALLY EXEMPT SMALL QUANTITY GENERATORS: GENERATES LESS THAN 100 KG/MONTH OF HAZA

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

The following spent halogenated solvents used in degreasing: Tetrachloroethylen, trichlorethylene, methylene chloride, 1,1,1-trichloroethane, carbon tetrachloride and chlorinated fluorocarbons; all spent solvent mixtures/bl

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 21 **DIST/DIR:** 0.37 SE **ELEVATION:** 728 **MAP ID:** 14

NAME: PRICE RIGHT AMOCO	REV: 7/14/10
ADDRESS: 816 SAINT CHARLES RD	ID1: ILD984924613
ELGIN IL 60120	ID2:
KANE	STATUS: SGN
CONTACT:	PHONE:
SOURCE: EPA	

SITE INFORMATION

CONTACT INFORMATION: MIKE HINES
816 ST CHARLES
ELGIN IL 60120

PHONE: 8476970480

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO

PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---
POST CLOSURE WORKLOAD:	---

PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---
CORRECTIVE ACTION WORKLOAD:	N - NO

GENERATOR STATUS: SQG - SMALL QUANTITY GENERATOR: GENERATES 100 - 1000 KG/MONTH
OF HAZARDOUS WASTE

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

Benzene
Ignitable waste

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST			
SEARCH ID: 29	DIST/DIR: 0.37 SE	ELEVATION: 728	MAP ID: 14
NAME: BP 15478	ADDRESS: 816 CHARLES AND BLUFF CITY ST ELGIN IL 60120	REV: 7/12/10	ID1: 2010850
CONTACT:		ID2:	STATUS: ACTIVE
SOURCE: IL FMO		PHONE:	
<u>SITE INFORMATION</u>			
TOTAL NUMBER OF TANKS:	9		
OWNER:	BP PRODUCTS NORTH AMERICA, INC. P. O. BOX 6038 ENVIRONMENTAL COMPLIANCE DEPARTMENT ARTESIA CA 90702		
FACILITY TYPE:	SELF-SERVICE STATION		
GREEN TAG DECAL:	K000530		
GREEN TAG ISSUED:	6/5/2009		
GREEN TAG EXPIRED:	12/31/2011		
SSP INSPECT DATE:	7/1/2009		
<u>TANK INFORMATION:</u>			
TANK NUMBER:	1	CAPACITY:	10000
SUBSTANCE:	GASOLINE	STATUS:	REMOVED
LAST USED:	6/27/1997	RED TAG:	
OSFM FIRST NOTIFIED:	9/28/1987		
TANK NUMBER:	2	CAPACITY:	10000
SUBSTANCE:	GASOLINE	STATUS:	REMOVED
LAST USED:	6/27/1997	RED TAG:	
OSFM FIRST NOTIFIED:	9/28/1987		
TANK NUMBER:	3	CAPACITY:	12000
SUBSTANCE:	GASOLINE	STATUS:	REMOVED
LAST USED:	6/27/1997	RED TAG:	
OSFM FIRST NOTIFIED:	9/28/1987		
TANK NUMBER:	4	CAPACITY:	10000
SUBSTANCE:	GASOLINE	STATUS:	CURRENTLY IN USE
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	1/13/1998		
TANK NUMBER:	5	CAPACITY:	10000
SUBSTANCE:	GASOLINE	STATUS:	CURRENTLY IN USE
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	1/13/1998		
TANK NUMBER:	6	CAPACITY:	10000
SUBSTANCE:	GASOLINE	STATUS:	CURRENTLY IN USE
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	1/13/1998		
TANK NUMBER:	7	CAPACITY:	4000
SUBSTANCE:	GASOLINE	STATUS:	EXEMPT FROM REGISTRATION
LAST USED:	1/1/1987	RED TAG:	

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST

SEARCH ID: 29 **DIST/DIR:** 0.37 SE **ELEVATION:** 728 **MAP ID:** 14

NAME: BP 15478
ADDRESS: 816 CHARLES AND BLUFF CITY ST
ELGIN IL 60120

REV: 7/12/10
ID1: 2010850
ID2:
STATUS: ACTIVE
PHONE:

CONTACT:
SOURCE: IL FMO

OSFM FIRST NOTIFIED: 2/4/1999

TANK NUMBER: 8
SUBSTANCE: GASOLINE
LAST USED: 1/1/1987
OSFM FIRST NOTIFIED: 2/4/1999

CAPACITY: 4000
STATUS: EXEMPT FROM REGISTRATION
RED TAG:

TANK NUMBER: 9
SUBSTANCE: GASOLINE
LAST USED: 1/1/1987
OSFM FIRST NOTIFIED: 2/4/1999

CAPACITY: 6000
STATUS: EXEMPT FROM REGISTRATION
RED TAG:

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 12 **DIST/DIR:** 0.38 NE **ELEVATION:** 748 **MAP ID:** 16

NAME: EMRO MARKETING NO 7095	REV: 4/9/05
ADDRESS: BLUFF CITY/SAINT CHARLES ELGIN IL 60120 KANE	ID1: ILD984781674
CONTACT: SID GLENN	ID2:
SOURCE: EPA	STATUS: LGN
	PHONE: 7083350600

SITE INFORMATION

CONTACT INFORMATION: SID GLENN
174TH AND DIXIE HWY
EAST HAZEL CREST IL 60429

PHONE: 7083350600

UNIVERSE INFORMATION:

SNC:	N - NO
BOYSNC:	N - NO
GPRA PERMIT:	N - NO
GPRA POSTCLOSURE:	N - NO
GPRA CA:	N - NO
GPRA CME:	N - NO
PERM PROG:	---

SUBJCA NON TSD:	N - NO
CA WRKLD:	N - NO
GEN STATUS:	N

PREM WRKLD:	---
CLOSURE WRKLD:	---
P C WRKLD:	---
SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

Ignitable waste

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRANLR

SEARCH ID: 72 **DIST/DIR:** 0.38 NE **ELEVATION:** 748 **MAP ID:** 16

NAME: EMRO MARKETING NO 7095	REV: 7/14/10
ADDRESS: CHARLES AND BLUFF BLVD	IDI: ILD984781674
ELGIN IL 60120	ID2:
KANE	STATUS: NLR
CONTACT:	PHONE:
SOURCE: EPA	

SITE INFORMATION

CONTACT INFORMATION: SID GLENN
174TH AND DIXIE HWY
EAST HAZEL CREST IL 60429

PHONE: 7083350600

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO
SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO
PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---
POST CLOSURE WORKLOAD:	---
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---
CORRECTIVE ACTION WORKLOAD:	N - NO
GENERATOR STATUS:	N

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

Ignitable waste

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 47 **DIST/DIR:** 0.39 SE- **ELEVATION:** 727 **MAP ID:** 18

NAME: CHECKER GAS STATION	REV: 6/21/02
ADDRESS: 851 ST CHARLES ST	ID1: 0894385070-860218
ELGIN IL 60120	ID2:
KANE	STATUS:
CONTACT: R.G. SCHUMANN	PHONE:
SOURCE: EPA	

SITE INFORMATION

DATE REPORTED: 2/18/1986
IEMA NUMBER: 860218
LPC NUMBER: 0894385070

OWNER/OPERATOR: R.G. SCHUMANN
EMRO MARKETING
P.O. BOX 162
EAST HAZEL CREST IL 60429-0162

PHONE

TANK CONTENTS/PRODUCT: GASOLINE
NON-LUST LETTER SENT:
SEC 57.5G LETTER SENT:
NFR LETTER SENT:
SITE CLASSIFICATION:
IEPA PROJECT MANAGER: PUTRICH

IEPA CORRESPONDENCE

DATE: 12/5/1996	DESCRIPTION: 20 DAY REPORT RECEIVED
DATE: 5/29/2002	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 12/8/1994	DESCRIPTION: RESPONSE LETTER RECEIVED
DATE: 12/1/2000	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 9/20/1994	DESCRIPTION: REVIEW LETTER SENT
DATE: 4/25/2002	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 3/7/2002	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 6/14/2001	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 12/5/1996	DESCRIPTION: 45 DAY REPORT RECEIVED

TITLE XVI INFORMATION

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 48 **DIST/DIR:** 0.39 SE **ELEVATION:** 727 **MAP ID:** 18

NAME: CHECKER GAS STATION	REV: 6/11/10
ADDRESS: 851 ST CHARLES ST	ID1: 0894385070-860218A
ELGIN IL 60120	ID2: 860218A
KANE	STATUS: CLOSED
CONTACT: GENE POOLE	PHONE: 8476974026
SOURCE: IL EPA	

SITE INFORMATION

DATE REPORTED: 2/18/1986
IEMA NUMBER: 860218A

CONTENTS/PRODUCT

GASOLINE:	YES
UNLEADED GASOLINE:	NO
DIESEL FUEL:	NO
FUEL OIL:	NO
JET FUEL:	NO
USED/WASTE OIL:	NO
NON-PETROLEUM PRODUCT:	NO
PETROLEUM:	NO

NON-LUST LETTER SENT:
SEC 57.5G LETTER SENT:
NFR LETTER SENT: 7/26/2006
SITE CLASSIFICATION:
IEPA PROJECT MANAGER: PUTRICH

IEPA CORRESPONDENCE

DATE: 5/29/2002	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 12/1/2000	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 12/16/2002	DESCRIPTION: CORRECTIVE ACTION PLAN RECEIVED
DATE: 6/26/2006	DESCRIPTION: PROFESSIONAL ENGINEER CERTIFICATION RECEIVED
DATE: 7/5/2005	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 10/11/2005	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 4/25/2002	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 2/3/2003	DESCRIPTION: APPROVED PLAN LETTER SENT
DATE: 7/19/2004	DESCRIPTION: STATUS REPORT RECEIVED
DATE: 3/7/2002	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 9/20/1994	DESCRIPTION: REVIEW LETTER SENT
DATE: 12/8/1994	DESCRIPTION: RESPONSE LETTER RECEIVED
DATE: 6/14/2001	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST			
SEARCH ID: 48	DIST/DIR: 0.39 SE	ELEVATION: 727	MAP ID: 18
NAME: CHECKER GAS STATION		REV: 6/11/10	
ADDRESS: 851 ST CHARLES ST ELGIN IL 60120 KANE		ID1: 0894385070-860218A	
CONTACT: GENE POOLE		ID2: 860218A	
SOURCE: IL EPA		STATUS: CLOSED	
		PHONE: 8476974026	
<u>TITLE XVI INFORMATION</u>			
DOCUMENT:	CORRECTIVE		
RECEIVED:	6/26/2006		
RESPONSE DUE:	10/24/2006		
RESPONSE MAILED:	7/26/2006		
RESPONSE TYPE:	APR		
DOCUMENT:	CORRECTIVE		
RECEIVED:	4/21/2006		
RESPONSE DUE:	8/19/2006		
RESPONSE MAILED:	5/12/2006		
RESPONSE TYPE:	DEN		
DOCUMENT:	CORRECTIVE		
RECEIVED:	11/10/2005		
RESPONSE DUE:	3/10/2006		
RESPONSE MAILED:	3/9/2006		
RESPONSE TYPE:	DEN		
DOCUMENT:	CORRECTIVE		
RECEIVED:	5/17/2005		
RESPONSE DUE:	9/14/2005		
RESPONSE MAILED:	6/24/2005		
RESPONSE TYPE:	DEN		
<u>ENGINEERING CONTROLS</u>			
BARRIER STRUCTURE:	NO	BARRIER PAVEMENT:	NO
BARRIER SOIL:	NO	BARRIER OTHER:	NO
BARRIER OTHER DESC:			
<u>INSTITUTIONAL CONTROLS</u>			
GW USE:	YES	INDUST COM:	YES
ORDINANCE:	NO	WORKER:	NO
OTHER:	NO		
OTHER DESC:			
<u>ENVIRONMENTAL LAND USE CONTROLS</u>			
GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			
HWY AUTH AGREEMENT:	YES		
AGREEMENT DESC:	No		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 9 **DIST/DIR:** 0.39 SE **ELEVATION:** 720 **MAP ID:** 17

NAME: EASTVIEW MANUFACTURING	REV: 7/14/10
ADDRESS: 970 ELIZABETH ST	ID1: ILR000014068
ELGIN IL 60120	ID2:
KANE	STATUS: VGN
CONTACT:	PHONE:
SOURCE: EPA	

SITE INFORMATION

CONTACT INFORMATION: JOHN MCHURNEY
970 ELIZABETH ST
ELGIN IL 60120

PHONE: 7087412514

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO
PERMIT WORKLOAD:	----
CLOSURE WORKLOAD:	----
POST CLOSURE WORKLOAD:	----
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	----
CORRECTIVE ACTION WORKLOAD:	N - NO

GENERATOR STATUS: CEG - CONDITIONALLY EXEMPT SMALL QUANTITY GENERATORS; GENERATES LESS THAN 100 KG/MONTH OF HAZA

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

Ignitable waste

**Environmental FirstSearch
Site Detail Report**

Target Property:

ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 59 **DIST/DIR:** 0.40 NW **ELEVATION:** 744 **MAP ID:** 19

NAME: PACE SUBURBAN BUS
ADDRESS: 975 S STATE ST
ELGIN IL 60123

REV: 6/11/10
ID1: 0894385145-980708
ID2: 980708
STATUS: CLOSED
PHONE: 8479316745

CONTACT: MELINDA METZGER
SOURCE: IL EPA

SITE INFORMATION

DATE REPORTED: 4/1/1998
HEMA NUMBER: 980708

CONTENTS/PRODUCT

GASOLINE: NO
UNLEADED GASOLINE: NO
DIESEL FUEL: NO
FUEL OIL: NO
JET FUEL: NO
USED/WASTE OIL: NO
NON-PETROLEUM PRODUCT: YES
PETROLEUM: NO

NON-LUST LETTER SENT:
SEC 57.5G LETTER SENT:
NFR LETTER SENT: 8/2/1999
SITE CLASSIFICATION:
IEPA PROJECT MANAGER: GEBHARDT

IEPA CORRESPONDENCE

DATE: 6/14/1999	DESCRIPTION: CORRECTIVE ACTION COMPLETION REPORT RECEIVED
DATE: 7/21/1999	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 6/14/1999	DESCRIPTION: PROFESSIONAL ENGINEER CERTIFICATION RECEIVED
DATE: 8/2/1999	DESCRIPTION: APPROVED PLAN LETTER SENT
DATE: 4/8/1998	DESCRIPTION: NOTICE OF RELEASE LETTER SENT

TITLE XVI INFORMATION

ENGINEERING CONTROLS

BARRIER STRUCTURE: NO	BARRIER PAVEMENT: YES
BARRIER SOIL: NO	BARRIER OTHER: NO
BARRIER OTHER DESC:	

INSTITUTIONAL CONTROLS

GW USE: YES	INDUST COM: NO
ORDINANCE: NO	WORKER: NO
OTHER: NO	
OTHER DESC:	

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 59 **DIST/DIR:** 0.40 NW **ELEVATION:** 744 **MAP ID:** 19

NAME: PACE SUBURBAN BUS
ADDRESS: 975 S STATE ST
ELGIN IL 60123

REV: 6/11/10
ID1: 0894385145-980708
ID2: 980708
STATUS: CLOSED
PHONE: 8479316745

CONTACT: MELINDA METZGER
SOURCE: IL EPA

ENVIRONMENTAL LAND USE CONTROLS

GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			

HWY AUTH AGREEMENT: NO
AGREEMENT DESC: No

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 20 **DIST/DIR:** 0.40 NW **ELEVATION:** 744 **MAP ID:** 19

NAME: PACE RIVER DIV	REV: 7/14/10
ADDRESS: 975 S STATE ST	ID1: ILD984787762
ELGIN IL 60123	ID2:
KANE	STATUS: SGN
CONTACT:	PHONE:
SOURCE: EPA	

SITE INFORMATION

CONTACT INFORMATION: JOHN CALAME
975 S STATE ST
ELGIN IL 60123

PHONE: 7086956500

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO
PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---
POST CLOSURE WORKLOAD:	---
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---
CORRECTIVE ACTION WORKLOAD:	N - NO
GENERATOR STATUS: OF HAZARDOUS WASTE	SQG - SMALL QUANTITY GENERATOR: GENERATES 100 - 1000 KG/MONTH

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

Ignitable waste

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST

SEARCH ID: 41 **DIST/DIR:** 0.40 NW **ELEVATION:** 744 **MAP ID:** 19

NAME: PACE RIVER DIVISION	REV: 7/12/10
ADDRESS: 975 S STATE RT 31 ELGIN IL 60123	ID1: 2025605
	ID2:
CONTACT:	STATUS: ACTIVE
SOURCE: IL FMO	PHONE:

SITE INFORMATION

TOTAL NUMBER OF TANKS: 5

OWNER: PACE SUBURBAN BUS
550 W. ALGONQUIN ROAD
ARLINGTON HEIGHTS IL 60005

FACILITY TYPE: STATE

GREEN TAG DECAL: L000028

GREEN TAG ISSUED: 1/14/2010

GREEN TAG EXPIRED: 12/31/2012

SSP INSPECT DATE:

SSP EXPIRATION DATE:

TANK INFORMATION:

TANK NUMBER: 1	CAPACITY: 20000
SUBSTANCE: ULTRA LOW SULFUR DIESEL	STATUS: CURRENTLY IN USE
LAST USED:	RED TAG:
OSFM FIRST NOTIFIED: 4/30/1990	
TANK NUMBER: 2	CAPACITY: 1000
SUBSTANCE: AUTOMATIC TRANSMISSION FLUID	STATUS: CURRENTLY IN USE
LAST USED:	RED TAG:
OSFM FIRST NOTIFIED: 4/30/1990	
TANK NUMBER: 3	CAPACITY: 1000
SUBSTANCE: MOTOR OIL	STATUS: CURRENTLY IN USE
LAST USED:	RED TAG:
OSFM FIRST NOTIFIED: 4/30/1990	
TANK NUMBER: 4	CAPACITY: 550
SUBSTANCE: HAZARDOUS SUBSTANCE	STATUS: ABANDONED IN PLACE
LAST USED:	RED TAG:
OSFM FIRST NOTIFIED: 4/30/1990	
TANK NUMBER: 5	CAPACITY: 1000
SUBSTANCE: USED OIL	STATUS: CURRENTLY IN USE
LAST USED:	RED TAG:
OSFM FIRST NOTIFIED: 4/30/1990	

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST			
SEARCH ID: 49	DIST/DIR: 0.42 NW	ELEVATION: 746	MAP ID: 20
NAME: CMS		REV: 6/11/10	
ADDRESS: 595 S STATE ST ELGIN IL 60123		ID1: 0894385079-992177	
		ID2: 992177	
CONTACT: STEVE HALL		STATUS: CLOSED	
SOURCE: IL EPA		PHONE: 2177822535	
<u>SITE INFORMATION</u>			
DATE REPORTED:	9/21/1999		
IEMA NUMBER:	992177		
<u>CONTENTS/PRODUCT</u>			
GASOLINE:	YES		
UNLEADED GASOLINE:	NO		
DIESEL FUEL:	YES		
FUEL OIL:	NO		
JET FUEL:	NO		
USED/WASTE OIL:	NO		
NON-PETROLEUM PRODUCT:	NO		
PETROLEUM:	NO		
NON-LUST LETTER SENT:			
SEC 57.5G LETTER SENT:			
NFR LETTER SENT:	11/30/1999		
SITE CLASSIFICATION:			
IEPA PROJECT MANAGER:	ROMINGER		
<u>IEPA CORRESPONDENCE</u>			
DATE: 11/12/1999	DESCRIPTION:	PROFESSIONAL ENGINEER CERTIFICATION RECEIVED	
DATE: 9/23/1999	DESCRIPTION:	NOTICE OF RELEASE LETTER SENT	
<u>TITLE XVI INFORMATION</u>			
DOCUMENT:	CORRECTIVE		
RECEIVED:	11/12/1999		
RESPONSE DUE:	3/11/2000		
RESPONSE MAILED:	11/30/1999		
RESPONSE TYPE:	APR		
<u>ENGINEERING CONTROLS</u>			
BARRIER STRUCTURE:	NO	BARRIER PAVEMENT:	NO
BARRIER SOIL:	NO	BARRIER OTHER:	NO
BARRIER OTHER DESC:			
<u>INSTITUTIONAL CONTROLS</u>			
GW USE:	NO	INDUST COM:	NO
ORDINANCE:	NO	WORKER:	NO
OTHER:	NO		
OTHER DESC:			

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 49 **DIST/DIR:** 0.42 NW **ELEVATION:** 746 **MAP ID:** 20

NAME: CMS
ADDRESS: 595 S STATE ST
ELGIN IL 60123

REV: 6/11/10
ID1: 0894385079-992177
ID2: 992177
STATUS: CLOSED
PHONE: 2177822535

CONTACT: STEVE HALL
SOURCE: IL EPA

ENVIRONMENTAL LAND USE CONTROLS

GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			

HWY AUTH AGREEMENT: NO
AGREEMENT DESC: No

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 16 **DIST/DIR:** 0.42 NW **ELEVATION:** 746 **MAP ID:** 20

NAME:	ILL DEPT OF TRANS	REV:	6/6/06
ADDRESS:	595 S STATE ST ELGIN IL 60123 KANE	ID1:	ILD982605990
CONTACT:	RONALD CONLEY	ID2:	
SOURCE:	EPA	STATUS:	SGN
		PHONE:	3127415302

SITE INFORMATION

CONTACT INFORMATION: RONALD CONLEY
595 S STATE ST
ELGIN IL 60123

PHONE: 3127415302

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO
PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---
POST CLOSURE WORKLOAD:	---
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---
CORRECTIVE ACTION WORKLOAD:	N - NO
GENERATOR STATUS:	SQG - SMALL QUANTITY GENERATOR: GENERATES 100 - 1000 KG/MONTH
OF HAZARDOUS WASTE	

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

The following spent non-halogenated solvents: Xylene, acetone, ethyl acetate, ethyl benzene, ethyl ether, methyl isobutyl ketone, n-butyl alcohol, cyclohexanone, and methanol; all spent solvent mixtures/ blends containing, b Ignitable waste

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 17 **DIST/DIR:** 0.42 NW **ELEVATION:** 748 **MAP ID:** 21

NAME:	ISP DIST 2 ELGIN HQ	REV:	7/14/10
ADDRESS:	777 S STATE ST	ID1:	ILD984905513
	ELGIN IL 60123	ID2:	
	KANE	STATUS:	SGN
CONTACT:		PHONE:	
SOURCE:	EPA		

SITE INFORMATION

CONTACT INFORMATION: MIKE GLEASON
PO BOX 19461 300 ARMORY
SPRINGFIELD IL 62794

PHONE: 2177824467

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO
PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---
POST CLOSURE WORKLOAD:	---
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---
CORRECTIVE ACTION WORKLOAD:	N - NO
GENERATOR STATUS:	SQG - SMALL QUANTITY GENERATOR: GENERATES 100 - 1000 KG/MONTH
OF HAZARDOUS WASTE	

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

Ignitable waste

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRAGN

SEARCH ID: 15 **DIST/DIR:** 0.42 NW **ELEVATION:** 746 **MAP ID:** 20

NAME:	IL CENTRAL MGMNT SERV DEPT OF VEHICLES	REV:	6/6/06
ADDRESS:	595 S STATE ST ELGIN IL 60123 KANE	ID1:	ILD981955271
CONTACT:	STEVE HALL	ID2:	
SOURCE:	EPA	STATUS:	LGN
		PHONE:	2177822535

SITE INFORMATION

CONTACT INFORMATION: STEVE HALL
595 S STATE ST
ELGIN IL 60123

PHONE: 2177822535

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC): N - NO

BEGINNING OF THE YEAR SNC: N - NO

PERMIT WORKLOAD: —

CLOSURE WORKLOAD: —

POST CLOSURE WORKLOAD: —

PERMITTING /CLOSURE/POST-CLOSURE PROGRESS: —

CORRECTIVE ACTION WORKLOAD: N - NO

GENERATOR STATUS: LQG - LARGE QUANTITY GENERATORS: GENERATES MORE THAN 1000
KG/MONTH OF HAZARDOUS WASTE

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

The following spent halogenated solvents used in degreasing: Tetrachloroethylene, trichlorethylene, methylene chloride, 1,1,1-trichloroethane, carbon tetrachloride and chlorinated fluorocarbons; all spent solvent mixtures/bl

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRANLR			
SEARCH ID: 25	DIST/DIR: 0.42 NW	ELEVATION: 746	MAP ID: 20
NAME: IL CENTRAL MGMNT SERV DEPT OF VEHICLES ADDRESS: 595 S STATE ST ELGIN IL 60120 KANE CONTACT: SOURCE: EPA	REV: 7/14/10 ID1: ILD981955271 ID2: STATUS: NLR PHONE:		
CONTACT INFORMATION:			
ENV COORDINATOR 2177822535			
UNIVERSE INFORMATION:			
GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)			
GPRA CA BASELINE UNIVERSE: NO			
GPRA CA 2008: NO			
SUBJECT TO CORRECTIVE ACTION (SUBJCA)			
SUBJCA: NO			
SUBJCA TSD 3004: NO			
SUBJCA NON TSD: NO			
SUBJCA TSD DISCRETION: NO			
PERMIT WORKLOAD: —			
CLOSURE WORKLOAD: —			
POST CLOSURE WORKLOAD: —			
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS: —			
CORRECTIVE ACTION WORKLOAD: NO			
GENERATOR STATUS: NO			
TRANSPORTER: NO			
UNIVERSAL WASTE: NO			
RECYCLER: NO			
USED OIL: NO			
IMPORTER: NO			
MIXED WASTE GENERATOR: N			
ONSITE BURNER EXEMPT: NO			
FURNACE EXEMPTION: NO			
UNDERGROUND INJECTION: NO			
NAIC 1:			
NAIC 2:			
NAIC 3:			
NAIC 4:			

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST

SEARCH ID: 31 **DIST/DIR:** 0.42 NW **ELEVATION:** 749 **MAP ID:** 22

NAME: ELGIN SANITARY DIST OF
ADDRESS: 875 S STATE ST
ELGIN IL 60120

REV: 7/12/10
ID1: 2005523
ID2:
STATUS: EXEMPT
PHONE:

CONTACT:
SOURCE: IL FMO

SITE INFORMATION

TOTAL NUMBER OF TANKS: 1

OWNER: ELGIN SANITARY DISTRICT
PO BOX 92
ELGIN IL 60121

FACILITY TYPE: NONE

GREEN TAG DECAL:
GREEN TAG ISSUED:
GREEN TAG EXPIRED:
SSP INSPECT DATE:
SSP EXPIRATION DATE:

TANK INFORMATION:

TANK NUMBER: 1 **CAPACITY:** 6000
SUBSTANCE: **STATUS:** EXEMPT FROM REGISTRATION
LAST USED: **RED TAG:**
OSFM FIRST NOTIFIED: 3/26/1986

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST			
SEARCH ID: 32	DIST/DIR: 0.42 NW	ELEVATION: 746	MAP ID: 20
NAME: ELGIN STATE GARAGE		REV: 7/12/10	
ADDRESS: 595 S STATE ST ELGIN IL 60123		ID1: 2008417	
CONTACT:		ID2:	
SOURCE: IL FMO		STATUS: ACTIVE	
PHONE:			
<u>SITE INFORMATION</u>			
TOTAL NUMBER OF TANKS: 4			
OWNER:		IL DEPT OF CENTRAL MANAGEMENT SERVICES 200 EAST ASH STREET SPRINGFIELD IL 62704	
FACILITY TYPE: STATE			
GREEN TAG DECAL: L000027			
GREEN TAG ISSUED: 1/14/2010			
GREEN TAG EXPIRED: 12/31/2012			
SSP INSPECT DATE:			
SSP EXPIRATION DATE:			
<u>TANK INFORMATION:</u>			
TANK NUMBER: 1	CAPACITY: 10000		
SUBSTANCE: DIESEL FUEL	STATUS: REMOVED		
LAST USED: 12/22/1998	RED TAG:		
OSFM FIRST NOTIFIED: 4/25/1986			
TANK NUMBER: 2	CAPACITY: 10000		
SUBSTANCE: GASOLINE	STATUS: REMOVED		
LAST USED: 12/22/1998	RED TAG:		
OSFM FIRST NOTIFIED: 4/25/1986			
TANK NUMBER: 3	CAPACITY: 10000		
SUBSTANCE: GASOLINE	STATUS: CURRENTLY IN USE		
LAST USED: 11/22/1999	RED TAG:		
OSFM FIRST NOTIFIED: 11/22/1999			
TANK NUMBER: 4	CAPACITY: 10000		
SUBSTANCE: DIESEL FUEL	STATUS: CURRENTLY IN USE		
LAST USED: 11/22/1999	RED TAG:		
OSFM FIRST NOTIFIED: 11/22/1999			

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST

SEARCH ID: 39 **DIST/DIR:** 0.42 NW **ELEVATION:** 748 **MAP ID:** 21

NAME: IL DEPT OF STATE POLICE
ADDRESS: 777 S STATE ST
ELGIN IL 60120

REV: 7/12/10
ID1: 2020383
ID2:
STATUS: CLOSED
PHONE:

CONTACT:
SOURCE: IL FMO

SITE INFORMATION

TOTAL NUMBER OF TANKS: 1

OWNER: IL DEPT OF STATE POLICE
401 ARMORY BLDG
SPRINGFIELD IL 62706

FACILITY TYPE: NONE
GREEN TAG DECAL:
GREEN TAG ISSUED:
GREEN TAG EXPIRED:
SSP INSPECT DATE:
SSP EXPIRATION DATE:

TANK INFORMATION:

TANK NUMBER:	1	CAPACITY:	0
SUBSTANCE:	GASOLINE	STATUS:	REMOVED
LAST USED:	1/1/1980	RED TAG:	
OSFM FIRST NOTIFIED:	4/18/1986		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

DOCKET

SEARCH ID: 67 **DIST/DIR:** 0.43 NW **ELEVATION:** 748 **MAP ID:** 24

NAME: ELGIN MENTAL HEALTH CENTER	REV: 1/30/07
ADDRESS: 750 S STATE ST ELGIN IL 60123	ID1: 05-1987-0518
	ID2: 05-1987-0518
CONTACT:	STATUS: ICIS
SOURCE: EPA	PHONE:

SITE INFORMATION

ICIS = INTEGRATED COMPLIANCE INFORMATION SYSTEM

ENFORCEMENT NUMBER: 05-1987-0518
ACTION TYPE: ADMINISTRATIVE - FORMAL
ACTIVITY STATUS: CLOSED
COURT DOCKET NUM: V-C-006-88
HQ DIVISION: TOX
ENFORCEMENT OUTCOME: LITIGATED WITH PENALTY
BRANCH:
VOL SELF DISCLOSURE:

ENFORCEMENT TYPE: TSCA 16 AO FOR COMPLIANCE AND/OR PENALTY
VIOLATION TYPE: VIOLATION OF PCB RULES
RELIEF REQUESTED:

DEFENDANT NAME: ELGIN MENTAL HEALTH CENTER
NAMED IN COMPLAINT: YES
NAMED IN SETTLEMENT: YES

CONCLUSION ID: 17212
CONCLUSION NAME: ELGIN MENTAL HEALTH CENTER
SETTLE LODGE DATE: 10/30/1987
SETTLE ENTERED DATE: 07/18/1988
PENALTY SOUGHT AMT: \$3,200
FED PENALTY ACCESSED AMT: \$3,200
LOCAL PENALTY AMT:
TOTAL SEP AMT(DERIVED):
COMP ACTION AMT:
COST REC AWARDED AMT:

CASE SUMMARY:

CENTER IS IN VIOLATION OF PCB REGULATIONS.

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

INSTCONTROL

SEARCH ID: 66 **DIST/DIR:** 0.43 NW **ELEVATION:** 748 **MAP ID:** 24

NAME: ELGIN MENTAL HEALTH CENTER	REV: 8/13/10
ADDRESS: 750 S STATE ST	ID1: 0894385115
ELGIN IL 60123	ID2: ILLD082050543
COOK	STATUS: SRP-IC
CONTACT: JAMES NICKETTA	PHONE: (847) 593-2300
SOURCE: IEPA	

SITE INFORMATION

TYPE OF SITE:	INDUSTRIAL/COMMERCIAL
COMP/FOCUSED:	COMPREHENSIVE
INSTITUTIONAL CONTROL:	GROUNDWATER USE RESTRICTION
ENG CONTROLS(BARRIERS):	
WORKER CAUTION:	0
ACRES:	87.81

DATE ENROLLED:	11/28/2005
ACERAGE:	87.81
SITE TYPE:	

REMEDIATION APPLICANT:	MR. JAMES NICKETTA 2299 BUSSE ROAD ELK GROVE VILLAGE, IL 60007-
PHONE:	(847) 593-2300

CONSULTANT:	DAKOTA PRENTICE PIONEER ENGINEERING and ENVIRONMENTAL SERVICES, INC. 700 NORTH SACRAMENTO BOULEVARD SUITE 101 CHICAGO, IL 60612-
PHONE:	(773) 722-9200

PROJECT MANAGER:	CATLIN
SECTION 4 (Y) LETTER:	
NFR LETTER:	4/25/2007
DATE RECORDED:	6/5/2007

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST

SEARCH ID: 42 **DIST/DIR:** 0.43 NW **ELEVATION:** 752 **MAP ID:** 23

NAME: PETROLIANCE LLC	REV: 7/12/10
ADDRESS: 739 STATE ST ELGIN IL 60120	IDI: 2024546
	ID2:
CONTACT:	STATUS: ACTIVE
SOURCE: IL FMO	PHONE:

SITE INFORMATION

TOTAL NUMBER OF TANKS: 6

OWNER: PETROLIANCE LLC
P. O. BOX 1145
CARY NC 27512

FACILITY TYPE: SELF-SERVICE STATION

GREEN TAG DECAL: L000160
GREEN TAG ISSUED: 5/3/2010
GREEN TAG EXPIRED: 12/31/2012
SSP INSPECT DATE: 5/3/2010

TANK INFORMATION:

TANK NUMBER: 1	CAPACITY: 4000
SUBSTANCE: GASOLINE	STATUS: CURRENTLY IN USE
LAST USED:	RED TAG:
OSFM FIRST NOTIFIED: 12/30/1988	
TANK NUMBER: 2	CAPACITY: 4000
SUBSTANCE: GASOLINE	STATUS: CURRENTLY IN USE
LAST USED:	RED TAG:
OSFM FIRST NOTIFIED: 12/30/1988	
TANK NUMBER: 3	CAPACITY: 6000
SUBSTANCE: GASOLINE	STATUS: CURRENTLY IN USE
LAST USED:	RED TAG:
OSFM FIRST NOTIFIED: 12/30/1988	
TANK NUMBER: 4	CAPACITY: 10000
SUBSTANCE: DIESEL FUEL	STATUS: CURRENTLY IN USE
LAST USED:	RED TAG:
OSFM FIRST NOTIFIED: 12/30/1988	
TANK NUMBER: 5	CAPACITY: 2000
SUBSTANCE: USED OIL	STATUS: CURRENTLY IN USE
LAST USED:	RED TAG:
OSFM FIRST NOTIFIED: 7/26/1991	
TANK NUMBER: 6	CAPACITY: 2000
SUBSTANCE: USED OIL	STATUS: CURRENTLY IN USE
LAST USED:	RED TAG:
OSFM FIRST NOTIFIED: 7/26/1991	

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST			
SEARCH ID: 44	DIST/DIR: 0.43 NW	ELEVATION: 748	MAP ID: 24
NAME: VACANT PROPERTY ADDRESS: 750 S STATE ST ELGIN IL 60123		REV: 7/12/10 ID1: 2043365 ID2: STATUS: CLOSED PHONE:	
CONTACT: SOURCE: IL FMO			
<u>SITE INFORMATION</u>			
TOTAL NUMBER OF TANKS:	2		
OWNER:	JOHN B. SANFILIPPO and SON, INC. 2299 BUSSE ROAD ELK GROVE VILLAGE IL 60007		
FACILITY TYPE:	NONE		
GREEN TAG DECAL:			
GREEN TAG ISSUED:			
GREEN TAG EXPIRED:			
SSP INSPECT DATE:			
SSP EXPIRATION DATE:			
<u>TANK INFORMATION:</u>			
TANK NUMBER:	1	CAPACITY:	1000
SUBSTANCE:	DIESEL FUEL	STATUS:	REMOVED
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	8/17/1993		
TANK NUMBER:	2	CAPACITY:	1000
SUBSTANCE:	GASOLINE	STATUS:	REMOVED
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	8/17/1993		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

VCP

SEARCH ID: 65 **DIST/DIR:** 0.49 SE **ELEVATION:** 708 **MAP ID:** 25

NAME: RUSS AUTOMOTIVE	REV: 8/13/10
ADDRESS: 970 LIBERTY ST	ID1: 0894385694
ELGIN IL 60120	ID2:
KANE	STATUS: SRP - INACTIVE
CONTACT: RICHARD GARLITZ	PHONE: (412) 882-4050
SOURCE: IEPA	

SITE INFORMATION

DATE ENROLLED: 7/5/2005
ACERAGE: 25
SITE TYPE:

REMEDICATION APPLICANT: MR. RICHARD GARLITZ
1641 SAW MILL RUN BOULEVARD
PITTSBURGH, PA 15210-
PHONE: (412) 882-4050

CONSULTANT: RICHARD GARLITZ
BRADBURNE, BRILLER and JOHNSON, LLC
1641 SAW MILL RUN BOULEVARD
PITTSBURGH, PA 15210-
PHONE: (412) 882-4050

PROJECT MANAGER: SMITH
SECTION 4 (Y) LETTER:
NFR LETTER:
DATE RECORDED:

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST			
SEARCH ID: 40	DIST/DIR: 0.50 NW	ELEVATION: 762	MAP ID: 26
NAME: KATZ and SONS WEST		REV: 7/12/10	
ADDRESS: 438 S STATE ELGIN IL 60120		ID1: 2018200	
		ID2:	
		STATUS: CLOSED	
CONTACT:		PHONE:	
SOURCE: IL FMO			
<u>SITE INFORMATION</u>			
TOTAL NUMBER OF TANKS:	5		
OWNER:	KATZ RICHARD C 514 ALGONA ELGIN IL 60120		
FACILITY TYPE:	NONE		
GREEN TAG DECAL:			
GREEN TAG ISSUED:			
GREEN TAG EXPIRED:			
SSP INSPECT DATE:			
SSP EXPIRATION DATE:			
<u>TANK INFORMATION:</u>			
TANK NUMBER:	1	CAPACITY:	6000
SUBSTANCE:	GASOLINE	STATUS:	REMOVED
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	4/7/1986		
TANK NUMBER:	2	CAPACITY:	3000
SUBSTANCE:	GASOLINE	STATUS:	REMOVED
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	4/7/1986		
TANK NUMBER:	3	CAPACITY:	1000
SUBSTANCE:	GASOLINE	STATUS:	REMOVED
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	4/7/1986		
TANK NUMBER:	4	CAPACITY:	1000
SUBSTANCE:	GASOLINE	STATUS:	REMOVED
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	4/7/1986		
TANK NUMBER:	5	CAPACITY:	275
SUBSTANCE:	USED OIL	STATUS:	REMOVED
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	4/7/1986		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

NFRAP

SEARCH ID: 1 **DIST/DIR:** 0.54 SE **ELEVATION:** 711 **MAP ID:** 27

NAME:	DSM DESOTECH INC	REV:	7/2/10
ADDRESS:	1122 SAINT CHARLES ST ELGIN IL 60120 KANE	ID1:	ILD058587759
CONTACT:		ID2:	0507090
SOURCE:	EPA	STATUS:	NFRAP-N
		PHONE:	

DESCRIPTION:

ACTION/QUALITY	AGENCY/RPS	START/RAA	END
ARCHIVE SITE	EPA In-House		12/1/1995
ARCHIVE SITE	EPA In-House		12/1/1995
ARCHIVE SITE	EPA In-House		
DISCOVERY	EPA Fund-Financed		9/3/1992
DISCOVERY	EPA Fund-Financed		
DISCOVERY	EPA Fund-Financed		9/3/1992
PRELIMINARY ASSESSMENT DEFERRED TO RCRA (SUBTITLE C)	EPA Fund-Financed		2/18/1993
PRELIMINARY ASSESSMENT	EPA Fund-Financed		
PRELIMINARY ASSESSMENT DEFERRED TO RCRA (SUBTITLE C)	EPA Fund-Financed		2/18/1993

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRACOR

SEARCH ID: 6 **DIST/DIR:** 0.54 SE **ELEVATION:** 711 **MAP ID:** 27

NAME: DSM DESOTECH INC	REV: 7/14/10
ADDRESS: 1122 SAINT CHARLES ST	ID1: ILD058587759
ELGIN IL 60120	ID2:
KANE	STATUS: CA
CONTACT:	PHONE:
SOURCE: EPA	

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	Y - NON TSDFS WHERE CORRECTIVE ACTION HAS BEEN IMPOSED

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO
PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---
POST CLOSURE WORKLOAD:	---
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---
CORRECTIVE ACTION WORKLOAD:	N - NO
GENERATOR STATUS:	LQG - LARGE QUANTITY GENERATORS: GENERATES MORE THAN 1000
KG/MONTH OF HAZARDOUS WASTE	

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRATSD			
SEARCH ID: 4	DIST/DIR: 0.54 SE	ELEVATION: 711	MAP ID: 27
NAME: DSM DESOTECH INC	REV: 12/9/02		
ADDRESS: 1122 SAINT CHARLES ST ELGIN IL 60120 KANE	ID1: ILD058587759		
CONTACT:	ID2:		
SOURCE: EPA	STATUS: TSD		
PHONE:	PHONE:		
<u>SITE INFORMATION</u>			
CONTACT INFORMATION:	FRITZ EPPERLY		
	1122 ST CHARLES ST ELGIN IL 60120		
PHONE:	7086970400		
<u>UNIVERSE TYPE:</u>			
LQG - LARGE QUANTITY GENERATORS: GENERATES MORE THAN 1000 KG/MONTH OF HAZARDOUS WASTE			
<u>SIC INFORMATION:</u>			
2851 - MANUFACTURING - PAINTS AND ALLIED PRODUCTS			
<u>RAATS INFORMATION:</u>			
DOCKET NUMBER:	86 R-38	INITIAL DATE:	5301986
DATE RECEIVED:	10301986	AMOUNT:	3550.00
ORDER TYPE:	3008(A)	FACILITY:	PRIVATELY HELD FACILITY
COMMENTS:			
<u>ENFORCEMENT INFORMATION:</u>			
AGENCY:	S - STATE	DATE:	16-JUN-98
TYPE:	121 - VIOLATION NOTICE (VN)		
AGENCY:	E - EPA	DATE:	30-MAY-86
TYPE:	210 - INITIAL 3008(A) COMPLIANCE ORDER		
AGENCY:	E - EPA	DATE:	11-FEB-88
TYPE:	120 - WRITTEN INFORMAL		
AGENCY:	S - STATE	DATE:	05-APR-89
TYPE:	120 - WRITTEN INFORMAL		
AGENCY:	S - STATE	DATE:	21-JAN-93
TYPE:	120 - WRITTEN INFORMAL		
AGENCY:	S - STATE	DATE:	30-JAN-92
TYPE:	120 - WRITTEN INFORMAL		
<u>VIOLATION INFORMATION:</u>			
VIOLATION NUMBER:	0001	RESPONSIBLE:	E - EPA
DETERMINED:	14-SEP-87	DETERMINED BY:	E - EPA

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRATSD			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
4	0.54 SE	711	27
NAME: DSM DESOTECH INC ADDRESS: 1122 SAINT CHARLES ST ELGIN IL 60120 KANE CONTACT: SOURCE: EPA		REV: 12/9/02 ID1: ILD058587759 ID2: STATUS: TSD PHONE:	
CITATION:		RESOLVED:	10-MAR-88
TYPE:	GLB - GENERATOR LAND BAN REQUIREMENTS		
VIOLATION NUMBER:	0001	RESPONSIBLE:	S - STATE
DETERMINED:	29-MAR-89	DETERMINED BY:	S - STATE
CITATION:		RESOLVED:	05-MAY-89
TYPE:	DFR - TSD FINANCIAL RESPONSIBILITY REQUIREMENTS		
VIOLATION NUMBER:	0002	RESPONSIBLE:	E - EPA
DETERMINED:	14-SEP-87	DETERMINED BY:	E - EPA
CITATION:		RESOLVED:	10-MAR-88
TYPE:	DLB - TSD LAND BAN REQUIREMENTS		
VIOLATION NUMBER:	0003	RESPONSIBLE:	E - EPA
DETERMINED:	09-OCT-85	DETERMINED BY:	E - EPA
CITATION:		RESOLVED:	23-NOV-86
TYPE:	DPP - TSD PREPAREDNESS/PREVENTION REQUIREMENTS		
VIOLATION NUMBER:	0004	RESPONSIBLE:	E - EPA
DETERMINED:	09-OCT-85	DETERMINED BY:	E - EPA
CITATION:		RESOLVED:	23-NOV-86
TYPE:	DPB - TSD PART B PERMIT APPLICATION		
VIOLATION NUMBER:	0004	RESPONSIBLE:	S - STATE
DETERMINED:	12-NOV-91	DETERMINED BY:	S - STATE
CITATION:	724.152(d)	RESOLVED:	13-APR-92
TYPE:	DCP - TSD CONTINGENCY PLAN REQUIREMENTS		
VIOLATION NUMBER:	0005	RESPONSIBLE:	S - STATE
DETERMINED:	12-NOV-91	DETERMINED BY:	S - STATE
CITATION:	724.152(e)	RESOLVED:	14-APR-92
TYPE:	DCP - TSD CONTINGENCY PLAN REQUIREMENTS		
VIOLATION NUMBER:	0006	RESPONSIBLE:	S - STATE
DETERMINED:	12-NOV-91	DETERMINED BY:	S - STATE
CITATION:	724.152(f)	RESOLVED:	13-APR-92
TYPE:	DCP - TSD CONTINGENCY PLAN REQUIREMENTS		
VIOLATION NUMBER:	0007	RESPONSIBLE:	S - STATE
DETERMINED:	12-NOV-91	DETERMINED BY:	S - STATE
CITATION:	I.D.1	RESOLVED:	20-APR-92
TYPE:	DPB - TSD PART B PERMIT APPLICATION		
VIOLATION NUMBER:	0008	RESPONSIBLE:	S - STATE
DETERMINED:	12-NOV-91	DETERMINED BY:	S - STATE
CITATION:	ILE	RESOLVED:	19-MAR-92
TYPE:	DPB - TSD PART B PERMIT APPLICATION		
VIOLATION NUMBER:	0009	RESPONSIBLE:	S - STATE
DETERMINED:	12-NOV-91	DETERMINED BY:	S - STATE
CITATION:	ILF	RESOLVED:	20-APR-92
TYPE:	DPB - TSD PART B PERMIT APPLICATION		

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**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRATSD			
SEARCH ID: 4	DIST/DIR: 0.54 SE	ELEVATION: 711	MAP ID: 27
NAME: DSM DESOTECH INC		REV: 12/9/02	
ADDRESS: 1122 SAINT CHARLES ST ELGIN IL 60120 KANE		ID1: ILD058587759	
CONTACT:		ID2:	
SOURCE: EPA		STATUS: TSD	
		PHONE:	
VIOLATION NUMBER:	0010	RESPONSIBLE:	S - STATE
DETERMINED:	12-NOV-91	DETERMINED BY:	S - STATE
CITATION:	ILL3	RESOLVED:	13-APR-92
TYPE:	DPB - TSD PART B PERMIT APPLICATION		
VIOLATION NUMBER:	0011	RESPONSIBLE:	S - STATE
DETERMINED:	12-NOV-91	DETERMINED BY:	S - STATE
CITATION:	ILLJ	RESOLVED:	19-MAR-92
TYPE:	DPB - TSD PART B PERMIT APPLICATION		
VIOLATION NUMBER:	0012	RESPONSIBLE:	S - STATE
DETERMINED:	07-DEC-92	DETERMINED BY:	S - STATE
CITATION:	725.243	RESOLVED:	24-AUG-93
TYPE:	DFR - TSD FINANCIAL RESPONSIBILITY REQUIREMENTS		
VIOLATION NUMBER:	0013	RESPONSIBLE:	S - STATE
DETERMINED:	07-DEC-92	DETERMINED BY:	S - STATE
CITATION:	725.242(b)	RESOLVED:	24-AUG-93
TYPE:	DFR - TSD FINANCIAL RESPONSIBILITY REQUIREMENTS		
VIOLATION NUMBER:	0014	RESPONSIBLE:	S - STATE
DETERMINED:	03-JUN-98	DETERMINED BY:	S - STATE
CITATION:	722.141(a)	RESOLVED:	13-AUG-98
TYPE:	GER - GENERATOR ALL REQUIREMENTS		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

VCP

SEARCH ID: 61 **DIST/DIR:** 0.54 SE **ELEVATION:** 711 **MAP ID:** 27

NAME: DSM DESOTO, INC.	REV: 8/13/10
ADDRESS: 1122 SAINT CHARLES ST ELGIN IL 60120 KANE	ID1: 0894380007
CONTACT: BARBARA STURM	ID2: ILLD058587759
SOURCE: IEPA	STATUS: SRP - INACTIVE
	PHONE: (847) 697-0400

SITE INFORMATION

DATE ENROLLED: 8/31/1995
ACERAGE: 13.048
SITE TYPE:

REMEDICATION APPLICANT: MS. BARBARA STURM
1122 ST. CHARLES STREET
ELGIN, IL 601208498
PHONE: (847) 697-0400

CONSULTANT: PAUL BURNSTEIN
HARDING LAWSON ASSOCIATES
1420 KENSINGTON ROAD SUITE 213
OAK BROOK, IL 60521-
PHONE: (630) 571-2162

PROJECT MANAGER: SALCH, JIM
SECTION 4 (Y) LETTER:
NFR LETTER: 8/20/1998
DATE RECORDED: 9/17/1998

DATE ENROLLED: 8/31/1995
ACERAGE: 13.048
SITE TYPE:

REMEDICATION APPLICANT: MS. BARBARA STURM
1122 ST. CHARLES STREET
ELGIN, IL 601208498
PHONE: (847) 697-0400

CONSULTANT: PAUL BURNSTEIN
HARDING LAWSON ASSOCIATES
1420 KENSINGTON ROAD SUITE 213
OAK BROOK, IL 60521-
PHONE: (630) 571-2162

PROJECT MANAGER: SALCH, JIM
SECTION 4 (Y) LETTER:
NFR LETTER: 2/3/2000
DATE RECORDED: 3/20/2000

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

VCP			
SEARCH ID: 62	DIST/DIR: 0.54 SE	ELEVATION: 711	MAP ID: 27
NAME: DSM DESOTO, INC.	REV: 07/23/97		
ADDRESS: 1122 SAINT CHARLES ST	ID1: SITE - 285		
ELGIN IL 60120	ID2:		
KANE	STATUS: SRP		
CONTACT:	PHONE:		
SOURCE:			
TYPE: SITE REMEDIATION PROGRAM			
NAME: DSM DESOTO, INC.			
ADDRESS: 1122 ST CHARLES ST			
CITY: ELGIN	COUNTY: KANE		
INVENTORY No.: 0894380007	NOTE:		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 57 **DIST/DIR:** 0.61 SE **ELEVATION:** 718 **MAP ID:** 28

NAME:	LEE WARDS CREATIVE CRAFTS	REV:	6/11/10
ADDRESS:	1200 ST CHARLES ST ELGIN IL 60120 KANE	ID1:	0894385125-921414
CONTACT:	GORDON MELLER	ID2:	921414
SOURCE:	IL EPA	STATUS:	CLOSED
		PHONE:	8478885791

SITE INFORMATION

DATE REPORTED: 5/27/1992
IEMA NUMBER: 921414

CONTENTS/PRODUCT

GASOLINE: YES
UNLEADED GASOLINE: NO
DIESEL FUEL: NO
FUEL OIL: YES
JET FUEL: NO
USED/WASTE OIL: NO
NON-PETROLEUM PRODUCT: NO
PETROLEUM: NO

NON-LUST LETTER SENT:
SEC 57.5G LETTER SENT:
NFR LETTER SENT: 9/3/1998
SITE CLASSIFICATION: HIGH
IEPA PROJECT MANAGER: PUTRICH

IEPA CORRESPONDENCE

DATE: 6/2/1992	DESCRIPTION:	NOTICE OF RELEASE LETTER SENT
DATE: 4/30/1993	DESCRIPTION:	REVIEW LETTER SENT
DATE: 10/29/1992	DESCRIPTION:	CORRECTIVE ACTION PLAN RECEIVED
DATE: 10/8/1993	DESCRIPTION:	REVIEW LETTER SENT
DATE: 12/30/1993	DESCRIPTION:	REVIEW LETTER SENT
DATE: 6/22/1998	DESCRIPTION:	PROFESSIONAL ENGINEER CERTIFICATION RECEIVED
DATE: 5/15/1995	DESCRIPTION:	PROFESSIONAL ENGINEER CERTIFICATION RECEIVED

TITLE XVI INFORMATION

DOCUMENT: SITE CLASS
RECEIVED: 5/31/1994
RESPONSE DUE: 9/28/1994
RESPONSE MAILED: 7/20/1994
RESPONSE TYPE: APR

DOCUMENT: SITE CLASS
RECEIVED: 4/11/1994
RESPONSE DUE: 8/9/1994
RESPONSE MAILED: 7/20/1994

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 57 **DIST/DIR:** 0.61 SE **ELEVATION:** 718 **MAP ID:** 28

NAME:	LEE WARDS CREATIVE CRAFTS	REV:	6/11/10
ADDRESS:	1200 ST CHARLES ST ELGIN IL 60120 KANE	ID1:	0894385125-921414
CONTACT:	GORDON MELLER	ID2:	921414
SOURCE:	IL EPA	STATUS:	CLOSED
		PHONE:	8478885791

RESPONSE TYPE: DEN

DOCUMENT: SITE CLASS
RECEIVED: 1/3/1995
RESPONSE DUE: 5/3/1995
RESPONSE MAILED: 5/2/1995
RESPONSE TYPE: DEN

DOCUMENT: SITE CLASS
RECEIVED: 11/21/1995
RESPONSE DUE: 3/20/1996
RESPONSE MAILED: 3/20/1996
RESPONSE TYPE: MOD

DOCUMENT: SITE CLASS
RECEIVED: 2/9/1994
RESPONSE DUE: 6/9/1994
RESPONSE MAILED: 4/29/1994
RESPONSE TYPE: DEN

DOCUMENT: CORRECTIVE
RECEIVED: 9/25/1997
RESPONSE DUE: 1/23/1998
RESPONSE MAILED: 10/10/1997
RESPONSE TYPE: MOD

DOCUMENT: HIGH PRIOR
RECEIVED: 7/17/1997
RESPONSE DUE: 11/14/1997
RESPONSE MAILED: 7/25/1997
RESPONSE TYPE: MOD

DOCUMENT: CORRECTIVE
RECEIVED: 7/17/1997
RESPONSE DUE: 11/14/1997
RESPONSE MAILED: 7/25/1997
RESPONSE TYPE: APR

DOCUMENT: SITE CLASS
RECEIVED: 9/11/1996
RESPONSE DUE: 1/9/1997
RESPONSE MAILED: 10/2/1996
RESPONSE TYPE: APR

DOCUMENT: SITE CLASS
RECEIVED: 11/21/1995
RESPONSE DUE: 3/20/1996
RESPONSE MAILED: 3/20/1996
RESPONSE TYPE: MOD

DOCUMENT: CORRECTIVE
RECEIVED: 5/15/1995
RESPONSE DUE: 9/12/1995

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**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 57 **DIST/DIR:** 0.61 SE **ELEVATION:** 718 **MAP ID:** 28

NAME:	LEE WARDS CREATIVE CRAFTS	REV:	6/11/10
ADDRESS:	1200 ST CHARLES ST ELGIN IL 60120 KANE	ID1:	0894385125-921414
CONTACT:	GORDON MELLER	ID2:	921414
SOURCE:	IL EPA	STATUS:	CLOSED
		PHONE:	8478885791

RESPONSE MAILED: 9/11/1995
RESPONSE TYPE: DEN

DOCUMENT: HIGH PRIOR
RECEIVED: 6/22/1998
RESPONSE DUE: 10/20/1998
RESPONSE MAILED: 9/3/1998
RESPONSE TYPE: APR

ENGINEERING CONTROLS

BARRIER STRUCTURE:	NO	BARRIER PAVEMENT:	NO
BARRIER SOIL:	NO	BARRIER OTHER:	NO
BARRIER OTHER DESC:			

INSTITUTIONAL CONTROLS

GW USE:	NO	INDUST COM:	NO
ORDINANCE:	NO	WORKER:	NO
OTHER:	NO		
OTHER DESC:			

ENVIRONMENTAL LAND USE CONTROLS

GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			

HWY AUTH AGREEMENT: NO
AGREEMENT DESC: No

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST			
SEARCH ID: 46	DIST/DIR: 0.62 NW	ELEVATION: 768	MAP ID: 30
NAME: BONCOSKY OIL CO. ADDRESS: 355 HENDEE ST ELGIN IL 60123	REV: 6/11/10 ID1: 0894385578-983019 ID2: 983019 STATUS: ACTIVE PHONE: 8477412577		
CONTACT: MIKE HAVENGA SOURCE: IL EPA			
<u>SITE INFORMATION</u>			
DATE REPORTED:	12/9/1998		
IEMA NUMBER:	983019		
<u>CONTENTS/PRODUCT</u>			
GASOLINE:	NO		
UNLEADED GASOLINE:	NO		
DIESEL FUEL:	YES		
FUEL OIL:	NO		
JET FUEL:	NO		
USED/WASTE OIL:	NO		
NON-PETROLEUM PRODUCT:	NO		
PETROLEUM:	NO		
NON-LUST LETTER SENT:			
SEC 57.5G LETTER SENT:			
NFR LETTER SENT:			
SITE CLASSIFICATION:	HIGH		
IEPA PROJECT MANAGER:	KAISER		
<u>IEPA CORRESPONDENCE</u>			
DATE: 12/18/1998	DESCRIPTION:	NOTICE OF RELEASE LETTER SENT	
<u>TITLE XVI INFORMATION</u>			
DOCUMENT:	SITE CLASS		
RECEIVED:	4/7/1999		
RESPONSE DUE:	8/5/1999		
RESPONSE MAILED:	7/13/1999		
RESPONSE TYPE:	DEN		
DOCUMENT:	HIGH PRIOR		
RECEIVED:	8/9/2005		
RESPONSE DUE:	12/7/2005		
RESPONSE MAILED:	9/15/2005		
RESPONSE TYPE:	APR		
DOCUMENT:	HIGH PRIOR		
RECEIVED:	4/14/2009		
RESPONSE DUE:	8/12/2009		
RESPONSE MAILED:	6/15/2009		
RESPONSE TYPE:	DEN		
DOCUMENT:	HIGH PRIOR		
RECEIVED:	1/14/2008		
RESPONSE DUE:	5/13/2008		
RESPONSE MAILED:	3/7/2008		

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**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 46 **DIST/DIR:** 0.62 NW **ELEVATION:** 768 **MAP ID:** 30

NAME: BONCOSKY OIL CO.
ADDRESS: 355 HENDEE ST
ELGIN IL 60123

REV: 6/11/10
ID1: 0894385578-983019
ID2: 983019
STATUS: ACTIVE
PHONE: 8477412577

CONTACT: MIKE HAVENGA
SOURCE: IL EPA

RESPONSE TYPE: APR

DOCUMENT: HIGH PRIOR
RECEIVED: 8/9/2005
RESPONSE DUE: 12/7/2005
RESPONSE MAILED: 9/15/2005
RESPONSE TYPE: APR

DOCUMENT: SITE CLASS
RECEIVED: 1/2/2001
RESPONSE DUE: 5/2/2001
RESPONSE MAILED: 3/27/2001
RESPONSE TYPE: APR

DOCUMENT: HIGH PRIOR
RECEIVED: 1/14/2008
RESPONSE DUE: 5/13/2008
RESPONSE MAILED: 3/7/2008
RESPONSE TYPE: APR

DOCUMENT: SITE CLASS
RECEIVED: 4/19/1999
RESPONSE DUE: 8/17/1999
RESPONSE MAILED: 7/13/1999
RESPONSE TYPE: DEN

DOCUMENT: SITE CLASS
RECEIVED: 4/7/1999
RESPONSE DUE: 8/5/1999
RESPONSE MAILED: 7/13/1999
RESPONSE TYPE: DEN

DOCUMENT: SITE CLASS
RECEIVED: 8/27/1999
RESPONSE DUE: 12/25/1999
RESPONSE MAILED: 10/29/1999
RESPONSE TYPE: APR

DOCUMENT: AMENDED SI
RECEIVED: 8/30/1999
RESPONSE DUE: 12/28/1999
RESPONSE MAILED: 10/29/1999
RESPONSE TYPE: APR

DOCUMENT: AMENDED SI
RECEIVED: 8/30/1999
RESPONSE DUE: 12/28/1999
RESPONSE MAILED: 10/29/1999
RESPONSE TYPE: APR

DOCUMENT: SITE CLASS
RECEIVED: 8/27/1999
RESPONSE DUE: 12/25/1999

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**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
46	0.62 NW	768	30
NAME:	BONCOSKY OIL CO.	REV:	6/11/10
ADDRESS:	355 HENDEE ST ELGIN IL 60123	ID1:	0894385578-983019
		ID2:	983019
CONTACT:	MIKE HAVENGA	STATUS:	ACTIVE
SOURCE:	IL EPA	PHONE:	8477412577
RESPONSE MAILED:	10/29/1999		
RESPONSE TYPE:	APR		
<u>ENGINEERING CONTROLS</u>			
BARRIER STRUCTURE:	NO	BARRIER PAVEMENT:	NO
BARRIER SOIL:	NO	BARRIER OTHER:	NO
BARRIER OTHER DESC:			
<u>INSTITUTIONAL CONTROLS</u>			
GW USE:	NO	INDUST COM:	NO
ORDINANCE:	NO	WORKER:	NO
OTHER:	NO		
OTHER DESC:			
<u>ENVIRONMENTAL LAND USE CONTROLS</u>			
GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			
HWY AUTH AGREEMENT:	NO		
AGREEMENT DESC:	No		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

NFRAP

SEARCH ID: 3 **DIST/DIR:** 0.62 SE **ELEVATION:** 722 **MAP ID:** 29

NAME: ILLINOIS TOOL WORKS	REV: 7/2/10
ADDRESS: 1201 ST CHARLES ST	ID1: ILD990817249
ELGIN IL 60120	ID2: 0501153
KANE	STATUS: NFRAP-N
CONTACT:	PHONE:
SOURCE: EPA	

DESCRIPTION:

ACTION/QUALITY	AGENCY/RPS	START/RAA	END
ARCHIVE SITE	EPA In-House		
ARCHIVE SITE	EPA In-House		1/8/1996
ARCHIVE SITE	EPA In-House		1/8/1996
DISCOVERY	EPA Fund-Financed		2/1/1984
DISCOVERY	EPA Fund-Financed		2/1/1984
DISCOVERY	EPA Fund-Financed		
HRS PACKAGE	EPA Fund-Financed		
HRS PACKAGE NFRAP: NO FURTHER REMEDIAL ACTION PLANNED	EPA Fund-Financed		12/20/1985
HRS PACKAGE NFRAP: NO FURTHER REMEDIAL ACTION PLANNED	EPA Fund-Financed		12/20/1985
PRELIMINARY ASSESSMENT LOW PRIORITY FOR FURTHER ASSESSMENT	State, Fund Financed		7/1/1984
PRELIMINARY ASSESSMENT LOW PRIORITY FOR FURTHER ASSESSMENT	State, Fund Financed		7/1/1984
PRELIMINARY ASSESSMENT	State, Fund Financed		
SITE INSPECTION	EPA Fund-Financed		
SITE INSPECTION NFRAP: NO FURTHER REMEDIAL ACTION PLANNED	EPA Fund-Financed		9/24/1985
SITE INSPECTION NFRAP: NO FURTHER REMEDIAL ACTION PLANNED	EPA Fund-Financed		9/24/1985

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
51	0.68 SW	731	31
NAME: ELGIN MENTAL HEALTH CENTER		REV: 6/11/10	
ADDRESS: 750 S STATE ST ELGIN IL 60123		ID1: 0894385115-940022	
		ID2: 940022	
CONTACT: WILLIAM CLANTIN		STATUS: ACTIVE	
SOURCE: IL BPA		PHONE: 8477421040	
<u>SITE INFORMATION</u>			
DATE REPORTED:	1/4/1994		
IEMA NUMBER:	940022		
<u>CONTENTS/PRODUCT</u>			
GASOLINE:	NO		
UNLEADED GASOLINE:	NO		
DIESEL FUEL:	NO		
FUEL OIL:	NO		
JET FUEL:	NO		
USED/WASTE OIL:	NO		
NON-PETROLEUM PRODUCT:	NO		
PETROLEUM:	NO		
NON-LUST LETTER SENT:			
SEC 57.5G LETTER SENT:	12/27/1994		
NFR LETTER SENT:			
SITE CLASSIFICATION:			
IEPA PROJECT MANAGER:	BAUER		
<u>IEPA CORRESPONDENCE</u>			
DATE:	1/10/1994	DESCRIPTION:	NOTICE OF RELEASE LETTER SENT
<u>TITLE XVI INFORMATION</u>			
<u>ENGINEERING CONTROLS</u>			
BARRIER STRUCTURE:	NO	BARRIER PAVEMENT:	NO
BARRIER SOIL:	NO	BARRIER OTHER:	NO
BARRIER OTHER DESC:			
<u>INSTITUTIONAL CONTROLS</u>			
GW USE:	NO	INDUST COM:	NO
ORDINANCE:	NO	WORKER:	NO
OTHER:	NO		
OTHER DESC:			
<u>ENVIRONMENTAL LAND USE CONTROLS</u>			
GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			

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**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 51 **DIST/DIR:** 0.68 SW **ELEVATION:** 731 **MAP ID:** 31

NAME: ELGIN MENTAL HEALTH CENTER
ADDRESS: 750 S STATE ST
ELGIN IL 60123

REV: 6/11/10
ID1: 0894385115-940022
ID2: 940022
STATUS: ACTIVE
PHONE: 8477421040

CONTACT: WILLIAM CLANIN
SOURCE: IL EPA

HWY AUTH AGREEMENT: NO
AGREEMENT DESC: No

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
52	0.68 SW	731	31
NAME: ELGIN MENTAL HEALTH CTR. ADDRESS: 750 S STATE ST ELGIN IL 60123		REV: 6/11/10 ID1: 0894385115-891217 ID2: 891217 STATUS: ACTIVE PHONE: 8477421040	
CONTACT: WILLIAM CLANIN SOURCE: IL EPA			
<u>SITE INFORMATION</u>			
DATE REPORTED:	7/12/1989		
IEPA NUMBER:	891217		
<u>CONTENTS/PRODUCT</u>			
GASOLINE:	YES		
UNLEADED GASOLINE:	NO		
DIESEL FUEL:	NO		
FUEL OIL:	NO		
JET FUEL:	NO		
USED/WASTE OIL:	NO		
NON-PETROLEUM PRODUCT:	NO		
PETROLEUM:	NO		
NON-LUST LETTER SENT:			
SEC 57.5G LETTER SENT:			
NFR LETTER SENT:			
SITE CLASSIFICATION:			
IEPA PROJECT MANAGER:	NOT ASSIGNED		
<u>IEPA CORRESPONDENCE</u>			
DATE:	8/4/1989	DESCRIPTION:	RESPONSE LETTER RECEIVED
DATE:	7/26/1989	DESCRIPTION:	NOTICE OF RELEASE LETTER SENT
<u>TITLE XVI INFORMATION</u>			
<u>ENGINEERING CONTROLS</u>			
BARRIER STRUCTURE:	NO	BARRIER PAVEMENT:	NO
BARRIER SOIL:	NO	BARRIER OTHER:	NO
BARRIER OTHER DESC:			
<u>INSTITUTIONAL CONTROLS</u>			
GW USE:	NO	INDUST COM:	NO
ORDINANCE:	NO	WORKER:	NO
OTHER:	NO		
OTHER DESC:			
<u>ENVIRONMENTAL LAND USE CONTROLS</u>			
GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 52 **DIST/DIR:** 0.68 SW **ELEVATION:** 731 **MAP ID:** 31

NAME: ELGIN MENTAL HEALTH CTR.
ADDRESS: 750 S STATE ST
ELGIN IL 60123

REV: 6/11/10
ID1: 0894385115-891217
ID2: 891217
STATUS: ACTIVE
PHONE: 8477421040

CONTACT: WILLIAM CLANIN
SOURCE: IL EPA

OTHER DESC:

HWY AUTH AGREEMENT: NO
AGREEMENT DESC: No

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST	
SEARCH ID: 53	DIST/DIR: 0.68 SW
ELEVATION: 731	MAP ID: 31
NAME: ELGIN MENTAL HEALTH DEPT. HUMAN SERVICES	REV: 6/11/10
ADDRESS: 750 S STATE ST ELGIN IL 60123	ID1: 0894385115-981344
	ID2: 981344
CONTACT: WILLIAM CLANIN	STATUS: CLOSED
SOURCE: IL EPA	PHONE: 8477421040
<u>SITE INFORMATION</u>	
DATE REPORTED:	6/5/1998
IEMA NUMBER:	981344
<u>CONTENTS/PRODUCT</u>	
GASOLINE:	NO
UNLEADED GASOLINE:	NO
DIESEL FUEL:	YES
FUEL OIL:	NO
JET FUEL:	NO
USED/WASTE OIL:	NO
NON-PETROLEUM PRODUCT:	NO
PETROLEUM:	NO
NON-LUST LETTER SENT:	
SEC 57.5G LETTER SENT:	
NFR LETTER SENT:	3/22/1999
SITE CLASSIFICATION:	
IEPA PROJECT MANAGER:	INGOLD
<u>IEPA CORRESPONDENCE</u>	
DATE: 3/12/1999	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 11/23/1998	DESCRIPTION: PROFESSIONAL ENGINEER CERTIFICATION RECEIVED
DATE: 3/26/1999	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 6/11/1998	DESCRIPTION: NOTICE OF RELEASE LETTER SENT
DATE: 3/16/1999	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 3/1/1999	DESCRIPTION: PROFESSIONAL ENGINEER CERTIFICATION RECEIVED
<u>TITLE XVI INFORMATION</u>	
DOCUMENT:	CORRECTIVE
RECEIVED:	11/23/1998
RESPONSE DUE:	3/23/1999
RESPONSE MAILED:	3/22/1999
RESPONSE TYPE:	APR
DOCUMENT:	CORRECTIVE
RECEIVED:	3/1/1999
RESPONSE DUE:	6/29/1999
RESPONSE MAILED:	3/22/1999
RESPONSE TYPE:	APR

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
53	0.68 SW	731	31
NAME: ELGIN MENTAL HEALTH DEPT. HUMAN SERVICES		REV:	6/11/10
ADDRESS: 750 S STATE ST ELGIN IL 60123		ID1:	0894385115-981344
		ID2:	981344
CONTACT: WILLIAM CLANIN		STATUS:	CLOSED
SOURCE: IL EPA		PHONE:	8477421040
<u>ENGINEERING CONTROLS</u>			
BARRIER STRUCTURE:	NO	BARRIER PAVEMENT:	NO
BARRIER SOIL:	NO	BARRIER OTHER:	NO
BARRIER OTHER DESC:			
<u>INSTITUTIONAL CONTROLS</u>			
GW USE:	NO	INDUST COM:	NO
ORDINANCE:	NO	WORKER:	NO
OTHER:	NO		
OTHER DESC:			
<u>ENVIRONMENTAL LAND USE CONTROLS</u>			
GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			
HWY AUTH AGREEMENT:	NO		
AGREEMENT DESC:	No		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 50 **DIST/DIR:** 0.68 SW **ELEVATION:** 731 **MAP ID:** 31

NAME: DHS ELGIN MENTAL HEALTH CTR.	REV: 6/11/10
ADDRESS: 750 S STATE ST ELGIN IL 60123	ID1: 0894385115-981699
	ID2: 981699
CONTACT: WILLIAM CLANIN	STATUS: ACTIVE
SOURCE: IL EPA	PHONE: 8477421040

SITE INFORMATION

DATE REPORTED: 7/13/1998
IEMA NUMBER: 981699

CONTENTS/PRODUCT

GASOLINE: NO
UNLEADED GASOLINE: NO
DIESEL FUEL: YES
FUEL OIL: NO
JET FUEL: NO
USED/WASTE OIL: NO
NON-PETROLEUM PRODUCT: NO
PETROLEUM: NO

NON-LUST LETTER SENT: 11/30/1998
SEC 57.5G LETTER SENT:
NFR LETTER SENT:
SITE CLASSIFICATION:
IEPA PROJECT MANAGER: INGOLD

IEPA CORRESPONDENCE

DATE: 7/17/1998	DESCRIPTION: NOTICE OF RELEASE LETTER SENT
DATE: 8/14/1998	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 10/9/1998	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED

TITLE XVI INFORMATION

ENGINEERING CONTROLS

BARRIER STRUCTURE: NO	BARRIER PAVEMENT: NO
BARRIER SOIL: NO	BARRIER OTHER: NO
BARRIER OTHER DESC:	

INSTITUTIONAL CONTROLS

GW USE: NO	INDUST COM: NO
ORDINANCE: NO	WORKER: NO
OTHER: NO	
OTHER DESC:	

ENVIRONMENTAL LAND USE CONTROLS

GW USE: NO	LAND USE: NO
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- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST			
SEARCH ID: 50	DIST/DIR: 0.68 SW	ELEVATION: 731	MAP ID: 31
NAME: DHS ELGIN MENTAL HEALTH CTR.		REV: 6/11/10	
ADDRESS: 750 S STATE ST ELGIN IL 60123		ID1: 0894385115-981699	
		ID2: 981699	
CONTACT: WILLIAM CLANIN		STATUS: ACTIVE	
SOURCE: IL EPA		PHONE: 8477421040	
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			
HWY AUTH AGREEMENT:	NO		
AGREEMENT DESC:	No		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

VCP

SEARCH ID: 64 **DIST/DIR:** 0.68 SW **ELEVATION:** 731 **MAP ID:** 31

NAME: ELGIN MENTAL HEALTH CENTER
ADDRESS: 750 S STATE ST
ELGIN IL 60123
KANE
CONTACT: JAMES NICKETTA
SOURCE: IEPA

REV: 8/13/10
ID1: 0894385115
ID2: ILD082050543
STATUS: SRP - INACTIVE
PHONE: (847) 593-2300

SITE INFORMATION

DATE ENROLLED: 11/28/2005
ACERAGE: 87.81
SITE TYPE:

REMEDATION APPLICANT: MR. JAMES NICKETTA
2299 BUSSE ROAD
ELK GROVE VILLAGE, IL 60007-
PHONE: (847) 593-2300

CONSULTANT: DAKOTA PRENTICE
PIONEER ENGINEERING and ENVIRONMENTAL SERVICES, INC.
700 NORTH SACRAMENTO BOULEVARD SUITE 101
CHICAGO, IL 60612-
PHONE: (773) 722-9200

PROJECT MANAGER: CATLIN
SECTION 4 (Y) LETTER:
NFR LETTER: 4/25/2007
DATE RECORDED: 6/5/2007

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 60 **DIST/DIR:** 0.74 NW **ELEVATION:** 774 **MAP ID:** 33

NAME: STATE and WALNUT QUIK MART	REV: 6/11/10
ADDRESS: 300 S STATE ST ELGIN IL 60123	ID1: 0314385464-941864
	ID2: 941864
CONTACT: WILLIAM LUSSON	STATUS: CLOSED
SOURCE: IL EPA	PHONE: 8473816570

SITE INFORMATION

DATE REPORTED: 8/17/1994
IEMA NUMBER: 941864

CONTENTS/PRODUCT

GASOLINE: NO
UNLEADED GASOLINE: NO
DIESEL FUEL: NO
FUEL OIL: YES
JET FUEL: NO
USED/WASTE OIL: NO
NON-PETROLEUM PRODUCT: NO
PETROLEUM: NO

NON-LUST LETTER SENT:
SEC 57.5G LETTER SENT:
NFR LETTER SENT: 3/9/2006
SITE CLASSIFICATION:
IEPA PROJECT MANAGER: MCGILL

IEPA CORRESPONDENCE

DATE: 10/13/2006	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 8/23/1994	DESCRIPTION: NOTICE OF RELEASE LETTER SENT
DATE: 2/28/2006	DESCRIPTION: PROFESSIONAL ENGINEER CERTIFICATION RECEIVED

TITLE XVI INFORMATION

DOCUMENT: CORRECTIVE
RECEIVED: 2/28/2006
RESPONSE DUE: 6/28/2006
RESPONSE MAILED: 3/9/2006
RESPONSE TYPE: APR

DOCUMENT: CORRECTIVE
RECEIVED: 8/26/2005
RESPONSE DUE: 12/24/2005
RESPONSE MAILED: 9/15/2005
RESPONSE TYPE: DEN

ENGINEERING CONTROLS

BARRIER STRUCTURE: NO	BARRIER PAVEMENT: NO
BARRIER SOIL: NO	BARRIER OTHER: NO
BARRIER OTHER DESC:	

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST			
SEARCH ID: 60	DIST/DIR: 0.74 NW	ELEVATION: 774	MAP ID: 33
NAME: STATE and WALNUT QUIK MART		REV: 6/11/10	
ADDRESS: 300 S STATE ST ELGIN IL 60123		ID1: 0314385464-941864	
		ID2: 941864	
CONTACT: WILLIAM LUSSON		STATUS: CLOSED	
SOURCE: IL EPA		PHONE: 8473816570	
<u>INSTITUTIONAL CONTROLS</u>			
GW USE:	NO	INDUST COM:	NO
ORDINANCE:	NO	WORKER:	NO
OTHER:	NO		
OTHER DESC:			
<u>ENVIRONMENTAL LAND USE CONTROLS</u>			
GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			
HWY AUTH AGREEMENT:	NO		
AGREEMENT DESC:	No		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 58 **DIST/DIR:** 0.74 NW **ELEVATION:** 723 **MAP ID:** 32

NAME: LEWA CO.
ADDRESS: 5 W WALNUT AVE
ELGIN IL 60123

REV: 6/11/10
IDI: 0894385446-932527
ID2: 932527
STATUS: CLOSED
PHONE: 8479403934

CONTACT: BERNIE SCHEBLER
SOURCE: IL EPA

SITE INFORMATION

DATE REPORTED: 9/21/1993
IEMA NUMBER: 932527

CONTENTS/PRODUCT

GASOLINE: NO
UNLEADED GASOLINE: NO
DIESEL FUEL: NO
FUEL OIL: YES
JET FUEL: NO
USED/WASTE OIL: NO
NON-PETROLEUM PRODUCT: NO
PETROLEUM: NO

NON-LUST LETTER SENT:
SEC 57.5G LETTER SENT:
NFR LETTER SENT: 5/18/1999
SITE CLASSIFICATION:
IEPA PROJECT MANAGER: PUTRICH

IEPA CORRESPONDENCE

DATE: 7/20/1994	DESCRIPTION: REVIEW LETTER SENT
DATE: 6/7/2004	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 4/30/1999	DESCRIPTION: PROFESSIONAL ENGINEER CERTIFICATION RECEIVED
DATE: 1/28/2000	DESCRIPTION: MISCELLANEOUS REPORT RECEIVED
DATE: 11/9/1998	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 8/11/1994	DESCRIPTION: REVIEW LETTER SENT
DATE: 9/28/1993	DESCRIPTION: NOTICE OF RELEASE LETTER SENT
DATE: 3/3/2000	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 9/25/1998	DESCRIPTION: REVIEW LETTER SENT

TITLE XVI INFORMATION

DOCUMENT: CORRECTIVE
RECEIVED: 8/4/1997
RESPONSE DUE: 12/2/1997
RESPONSE MAILED: 8/15/1997
RESPONSE TYPE: DEN

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**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 58 **DIST/DIR:** 0.74 NW **ELEVATION:** 723 **MAP ID:** 32

NAME: LEWA CO.
ADDRESS: 5 W WALNUT AVE
ELGIN IL 60123

REV: 6/11/10
ID1: 0894385446-932527
ID2: 932527
STATUS: CLOSED
PHONE: 8479403934

CONTACT: BERNIE SCHEBLER
SOURCE: IL EPA

DOCUMENT: CORRECTIVE
RECEIVED: 12/22/1998
RESPONSE DUE: 4/21/1999
RESPONSE MAILED: 3/22/1999
RESPONSE TYPE: DEN

DOCUMENT: CORRECTIVE
RECEIVED: 4/2/1999
RESPONSE DUE: 7/31/1999
RESPONSE MAILED: 5/18/1999
RESPONSE TYPE: APR

DOCUMENT: CORRECTIVE
RECEIVED: 11/25/1997
RESPONSE DUE: 3/25/1998
RESPONSE MAILED: 1/23/1998
RESPONSE TYPE: DEN

DOCUMENT: CORRECTIVE
RECEIVED: 4/30/1999
RESPONSE DUE: 8/28/1999
RESPONSE MAILED: 5/18/1999
RESPONSE TYPE: APR

DOCUMENT: CORRECTIVE
RECEIVED: 8/6/1998
RESPONSE DUE: 12/4/1998
RESPONSE MAILED: 9/25/1998
RESPONSE TYPE: DEN

ENGINEERING CONTROLS

BARRIER STRUCTURE:	NO	BARRIER PAVEMENT:	NO
BARRIER SOIL:	NO	BARRIER OTHER:	NO
BARRIER OTHER DESC:			

INSTITUTIONAL CONTROLS

GW USE:	YES	INDUST COM:	NO
ORDINANCE:	NO	WORKER:	NO
OTHER:	NO		
OTHER DESC:			

ENVIRONMENTAL LAND USE CONTROLS

GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			

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**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 58 **DIST/DIR:** 0.74 NW **ELEVATION:** 723 **MAP ID:** 32

NAME: LEWA CO.
ADDRESS: 5 W WALNUT AVE
ELGIN IL 60123

REV: 6/11/10
ID1: 0894385446-932527
ID2: 932527
STATUS: CLOSED
PHONE: 8479403934

CONTACT: BERNIE SCHEBLER
SOURCE: IL EPA

HWY AUTH AGREEMENT: NO
AGREEMENT DESC: No

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

CERCLIS

SEARCH ID: 69 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME: ELGIN LDFL	REV: 7/2/10
ADDRESS: RT 25	ID1: ILD981960800
SOUTH ELGIN IL 60177	ID2: 0505269
KANE	STATUS: PART OF NPL
CONTACT: JOHN FAGIOLO	PHONE: 3128860800
SOURCE: EPA	

ACTION/QUALITY	AGENCY/RPS	START/RAA	END
non-national priorities list potentially responsible party search		Federal Enforcement	12/31/2001
discovery	EPA Fund-Financed		8/1/1987
preliminary assessment Higher priority for further assessment	State, Fund Financed		9/30/1988
site inspection Higher priority for further assessment	State, Fund Financed		11/3/1989

DESCRIPTION:

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

DOCKET			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME: VILLA OLIVIA ADDRESS: RT. 1 BOX 28 LAKE ST ELGIN IL 60120 COOK CONTACT: SOURCE: EPA		REV: 1/30/07 ID1: HQ-2005-8000-388 ID2: HQ-2005-8000 STATUS: ICIS PHONE:	
<u>SITE INFORMATION</u>			
ICIS = INTEGRATED COMPLIANCE INFORMATION SYSTEM			
<u>CASE SUMMARY:</u>			

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
74	NON GC		
NAME: CANADIAN PACIFIC RAILWAY		REV: 12/31/04	
ADDRESS: UNKNOWN BENSONVILLE IL 60120 DU PAGE		ID1: NRC-728904	
CONTACT: UNKNOWN		ID2:	
SOURCE: NRC		STATUS: STORAGE TANK	
		PHONE:	
<u>SITE INFORMATION</u>			
THIS INFORMATION WAS OBTAINED FROM THE NATIONAL RESPONSE CENTER			
DATE RECEIVED: 7/20/2004 6:08:40 PM	7/20/2004 6:03:08 PM	DATE COMPLETE:	
CALL TAKER: 7/20/2004 6:08:40 PM	REC7955	CALL TYPE:	INC
RESPONSIBLE PARTY:	UNKNOWN		
PHONE 1:			
PHONE 2:			
PHONE 3:			
RESPONSIBLE COMPANY:			
ORGANIZATION TYPE:	UNKNOWN		
ADDRESS:	XX		
SOURCE:	TELEPHONE		
<u>INCIDENT INFORMATION</u>			
INCIDENT DESCRIPTION: NON HAZARDOUS PAINT HAS LEAKED ONTO THE SOIL FROM AN INTERMODAL CONTAINER IN A RAIL YARD.			
INCIDENT TYPE:	STORAGE TANK	INCIDENT CAUSE:	EQUIPMENT FAILURE
INCIDENT DATE:	7/20/2004 3:50:00 PM	INCIDENT DATE DESC:	
OCCURRED			
DISTANCE FROM CITY:		DISTANCE UNITS:	
DIRECTION FROM CITY:		LOCATION SECTION:	
LOCATION TOWNSHIP:		LOCATION RANGE:	
AIRCRAFT TYPE:		AIRCRAFT MODEL:	
AIRCRAFT ID:		AIRCRAFT FUEL CAPACITY:	
AIRCRAFT FUEL CAPACITY UNITS:		AIRCRAFT FUEL ON BOARD:	
AIRCRAFT FUEL ON BOARD UNITS:		AIRCRAFT SPOT NUMBER:	
AIRCRAFT HANGER:		AIRCRAFT RUNWAY NUM:	
ROAD MILE MARKER:		BUILDING ID:	
TYPE OF FIXED OBJECT:		POWER GEN FACILITY:	U
GENERATING CAPACITY:		TYPE OF FUEL:	
NPDES:		NPDES COMPLIANCE:	U
PIPELINE TYPE:		DOT REGULATED:	U
PIPELINE ABOVE GROUND:	ABOVE	EXPOSED UNDERWATER:	N
PIPELINE COVERED:	U	GRADE CROSSING:	N
LOCATION SUBDIVISION:		RAILROAD MILEPOST:	
TYPE VEHICLE INVOLVED:		CROSSING DEVICE TYPE:	
DEVICE OPERATIONAL:	Y		
DOT CROSSING NUMBER:		BRAKE FAILURE:	N

- Continued on next page -

Environmental FirstSearch Site Detail Report

Target Property:

ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS			
SEARCH ID: 74	DIST/DIR: NON GC	ELEVATION:	MAP ID:
NAME: CANADIAN PACIFIC RAILWAY ADDRESS: UNKNOWN BENSONVILLE IL 60120 DU PAGE CONTACT: UNKNOWN SOURCE: NRC		REV: 12/31/04 ID1: NRC-728904 ID2: STATUS: STORAGE TANK PHONE:	
TANK ABOVE GROUND: TANK REGULATED: TANK ID: CAPACITY OF TANK UNITS: ACTUAL AMOUNT UNITS: PLATFORM LETTER: LOCATION BLOCK ID:	ABOVE U	TRANSPORTABLE CONTAINER: TANK REGULATED BY: CAPACITY OF TANK: ACTUAL AMOUNT: PLATFORM RIG NAME: LOCATION AREA ID:	U
DESCRIPTION OF TANK:	INTERMODAL CONTAINER		
OCSG NUMBER: STATE LEASE NUMBER: BERTH SLIP NUMBER: INITIAL CONT RELEASE NUM: ALLISION: STRUCTURE NAME: AIRBAG DEPLOYED: SERVICE DISRUPT TIME: TRANSIT BUS FLAG: CR END DATE:	N	OCSP NUMBER: PIER DOCK NUMBER: CONTIN RELEASE TYPE: CONT RELEASE PERMIT: TYPE OF STRUCTURE: STRUCT OPERATIONAL: DATE NORMAL SERVICE: SERVICE DISRUPT UNITS: CR BEGIN DATE: CR CHANGE DATE:	U
FIRE INVOLVED: ANY EVACUATIONS: WHO EVACUATED: ANY INJURIES: NUMBER HOSPITALIZED: NUMBER FATALITIES: DAMAGE AMOUNT: AIR CORRIDOR DESC: WATERWAY CLOSED: WATERWAY CLOSURE TIME: ROAD DESC: CLOSURE DIRECTION:	N N N N N N N N N N N N	FIRE EXTINGUISHED: NUMBER EVACUATED: RADIUS OF EVACUATION: NUMBER INJURED: ANY FATALITIES: ANY DAMAGES: AIR CORRIDOR CLOSED: AIR CLOSURE TIME: WATERWAY DESC: ROAD CLOSED: ROAD CLOSURE TIME: MAJOR ARTERY:	U N N N N N N N
TRACK CLOSED: TRACK CLOSURE TIME: MEDIUM DESC: BODY OF WATER: NEAREST RIVER MILE MARK: EST DUR OF RELEASE: TRACK CLOSE DIR: ST AGENCY RPT NUM: WEATHER CONDITIONS: WIND SPEED: WATER SUPPLY CONTAM: SHEEN COLOR: SHEEN ODOR DESCRIPTION: CURRENT SPEED: WATER TEMPERATURE:	N LAND CLEAR U	TRACK DESC: MEDIA INTEREST: ADDTL MEDIUM INFO: TRIBUTARY OF: RELEASE SECURED: RELEASE RATE: ST AGENCY ON SCENE: OTHER AGENCY NOTIFIED: AIR TEMPERATURE: WIND DIRECTION: SHEEN SIZE: DIR OF SHEEN TRAVEL: WAVE CONDITION: CURRENT DIRECTION:	NONE SOIL Y
DESC OF REMEDIAL ACTION:	CLEAN UP UNDERWAY		
EMPL FATALITY:	PASS FATALITY:		

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME:	CANADIAN PACIFIC RAILWAY	REV:	12/31/04
ADDRESS:	UNKNOWN BENSONVILLE IL 60120 DU PAGE	ID1:	NRC-728904
CONTACT:	UNKNOWN	ID2:	
SOURCE:	NRC	STATUS:	STORAGE TANK
PHONE:			
COMMUNITY IMPACT:	N	WIND SPEED UNITS:	
EMPLOYEE INJURIES:		PASSENGER INJURIES:	
OCCUPANT FATALITY:		CURRENT SPEED UNITS:	
ROAD CLOSURE UNITS:		TRACK CLOSURE UNITS:	
SHEEN SIZE UNITS:		STATE AGENCY NOTIFIED:	
FED AGENCY NOTIFIED:		NEAREST RIVER MILE MARK:	
SHEEN SIZE LENGTH:		SHEEN SIZE LENGTH UNITS:	
SHEEN SIZE WIDTH:		SHEEN SIZE WIDTH UNITS:	
OFFSHORE:	N	DURATION UNIT:	
RELEASE RATE UNIT:		RELEASE RATE RATE:	
ADDITIONAL INFO:	NONE		
<u>MATERIAL INFORMATION</u>			
CHRIS CODE:	NCC	CASE NUMBER:	000000-00-0
UN NUMBER:		REACHED WATER:	NO
NAME OF MATERIAL:	NON HAZARDOUS PAINT		
AMOUNT OF MATERIAL:	1 GALLON(S)		
AMOUNT IN WATER:			
<u>OTHER MATERIAL INFORMATION</u>			
<u>MOBILE DETAILS INFORMATION</u>			
<u>TRAIN INFORMATION</u>			
<u>VESSEL INFORMATION</u>			

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS

SEARCH ID: 75 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME:	CANADIAN PACIFIC RAILYARD MILE POST 15.5	REV:	12/31/03
ADDRESS:	UNKNOWN BENSONVILLE IL 60120 DU PAGE	ID1:	NRC-650192
CONTACT:	ERIC JONASSON	ID2:	
SOURCE:	NRC	STATUS:	RAILROAD
		PHONE:	6129045836

SITE INFORMATION

THIS INFORMATION WAS OBTAINED FROM THE NATIONAL RESPONSE CENTER

DATE RECEIVED:	7/7/2003 7:17:34 PM	DATE COMPLETE:	7/7/2003
7:21:09 PM			
CALL TAKER:	REC7955 CALL TYPE:	INC	
RESPONSIBLE PARTY:	ERIC JONASSON		
PHONE 1:	6129045836 PRIMARY		
PHONE 2:			
PHONE 3:			
RESPONSIBLE COMPANY:	CANADIAN PACIFIC RAILROAD		
ORGANIZATION TYPE:	PRIVATE ENTERPRISE		
ADDRESS:	501 MARQUETTE STREET MINNEAPOLIS MN 55440		
SOURCE:	TELEPHONE		

INCIDENT INFORMATION

INCIDENT DESCRIPTION: DUE TO AN UNDETERMINED CAUSE NON-HAZARDOUS OIL SEDIMENT SPILLED ONTO THE RAIL BALLAST FROM A TANK CAR IN A RAIL YARD.

INCIDENT TYPE:	RAILROAD	INCIDENT CAUSE:	UNKNOWN
INCIDENT DATE:	7/7/2003 5:45:00 PM	INCIDENT DATE DESC:	
OCCURRED			
DISTANCE FROM CITY:		DISTANCE UNITS:	
DIRECTION FROM CITY:		LOCATION SECTION:	
LOCATION TOWNSHIP:		LOCATION RANGE:	
AIRCRAFT TYPE:		AIRCRAFT MODEL:	
AIRCRAFT ID:		AIRCRAFT FUEL CAPACITY:	
AIRCRAFT FUEL CAPACITY UNITS:		AIRCRAFT FUEL ON BOARD:	
AIRCRAFT FUEL ON BOARD UNITS:		AIRCRAFT SPOT NUMBER:	
AIRCRAFT HANGER:		AIRCRAFT RUNWAY NUM:	
ROAD MILE MARKER:		BUILDING ID:	
TYPE OF FIXED OBJECT:		POWER GEN FACILITY:	U
GENERATING CAPACITY:		TYPE OF FUEL:	
NPDES:		NPDES COMPLIANCE:	U
PIPELINE TYPE:		DOT REGULATED:	U
PIPELINE ABOVE GROUND:	ABOVE	EXPOSED UNDERWATER:	N
PIPELINE COVERED:	U	GRADE CROSSING:	N
LOCATION SUBDIVISION:	ELGIN	RAILROAD MILEPOST:	15.5
TYPE VEHICLE INVOLVED:		CROSSING DEVICE TYPE:	
DEVICE OPERATIONAL:	Y		
DOT CROSSING NUMBER:		BRAKE FAILURE:	N

- Continued on next page -

Environmental FirstSearch Site Detail Report

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME: CANADIAN PACIFIC RAILYARD MILE POST 15.5		REV: 12/31/03	
ADDRESS: UNKNOWN BENSONVILLE IL 60120 DU PAGE		ID1: NRC-650192	
CONTACT: ERIC JONASSON		ID2:	
SOURCE: NRC		STATUS: RAILROAD	
		PHONE: 6129045836	
TANK ABOVE GROUND:	ABOVE	TRANSPORTABLE CONTAINER:	U
TANK REGULATED:	U	TANK REGULATED BY:	
TANK ID:		CAPACITY OF TANK:	
CAPACITY OF TANK UNITS:		ACTUAL AMOUNT:	
ACTUAL AMOUNT UNITS:		PLATFORM RIG NAME:	
PLATFORM LETTER:		LOCATION AREA ID:	
LOCATION BLOCK ID:			
DESCRIPTION OF TANK:			
OCSG NUMBER:		OCSF NUMBER:	
STATE LEASE NUMBER:		PIER DOCK NUMBER:	
BERTH SLIP NUMBER:		CONTIN RELEASE TYPE:	
INITIAL CONT RELEASE NUM:		CONT RELEASE PERMIT:	
ALLISION:	N	TYPE OF STRUCTURE:	
STRUCTURE NAME:		STRUCT OPERATIONAL:	U
AIRBAG DEPLOYED:		DATE NORMAL SERVICE:	
SERVICE DISRUPT TIME:		SERVICE DISRUPT UNITS:	
TRANSIT BUS FLAG:		CR BEGIN DATE:	
CR END DATE:		CR CHANGE DATE:	
FIRE INVOLVED:	N	FIRE EXTINGUISHED:	U
ANY EVACUATIONS:	N	NUMBER EVACUATED:	
WHO EVACUATED:		RADIUS OF EVACUATION:	
ANY INJURIES:	N	NUMBER INJURED:	
NUMBER HOSPITALIZED:		ANY FATALITIES:	N
NUMBER FATALITIES:		ANY DAMAGES:	N
DAMAGE AMOUNT:		AIR CORRIDOR CLOSED:	N
AIR CORRIDOR DESC:		AIR CLOSURE TIME:	
WATERWAY CLOSED:	N	WATERWAY DESC:	
WATERWAY CLOSURE TIME:		ROAD CLOSED:	N
ROAD DESC:		ROAD CLOSURE TIME:	
CLOSURE DIRECTION:		MAJOR ARTERY:	N
TRACK CLOSED:	N	TRACK DESC:	
TRACK CLOSURE TIME:		MEDIA INTEREST:	NONE
MEDIUM DESC:	LAND	ADDTL MEDIUM INFO:	RAIL BALLAST
BODY OF WATER:		TRIBUTARY OF:	
NEAREST RIVER MILE MARK:		RELEASE SECURED:	Y
EST DUR OF RELEASE:		RELEASE RATE:	
TRACK CLOSE DIR:		ST AGENCY ON SCENE:	
ST AGENCY RPT NUM:	H20030992	OTHER AGENCY NOTIFIED:	
WEATHER CONDITIONS:	CLEAR	AIR TEMPERATURE:	
WIND SPEED:		WIND DIRECTION:	
WATER SUPPLY CONTAM:	U	SHEEN SIZE:	
SHEEN COLOR:		DIR OF SHEEN TRAVEL:	
SHEEN ODOR DESCRIPTION:		WAVE CONDITION:	
CURRENT SPEED:		CURRENT DIRECTION:	
WATER TEMPERATURE:			
DESC OF REMEDIAL ACTION:	CONTRACTOR HAS BEEN HIRED		
EMPL FATALITY:		PASS FATALITY:	

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS

SEARCH ID: 75 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME:	CANADIAN PACIFIC RAILYARD MILE POST 15.5	REV:	12/31/03
ADDRESS:	UNKNOWN	ID1:	NRC-650192
	BENSONVILLE IL 60120	ID2:	
	DU PAGE	STATUS:	RAILROAD
CONTACT:	ERIC JONASSON	PHONE:	6129045836
SOURCE:	NRC		

COMMUNITY IMPACT:	N	WIND SPEED UNITS:	
EMPLOYEE INJURIES:		PASSENGER INJURIES:	
OCCUPANT FATALITY:		CURRENT SPEED UNITS:	
ROAD CLOSURE UNITS:		TRACK CLOSURE UNITS:	
SHEEN SIZE UNITS:		STATE AGENCY NOTIFIED:	STATE DUTY OFFICER
FED AGENCY NOTIFIED:		NEAREST RIVER MILE MARK:	
SHEEN SIZE LENGTH:		SHEEN SIZE LENGTH UNITS:	
SHEEN SIZE WIDTH:		SHEEN SIZE WIDTH UNITS:	
OFFSHORE:	N	DURATION UNIT:	
RELEASE RATE UNIT:		RELEASE RATE RATE:	

ADDITIONAL INFO: NONE

MATERIAL INFORMATION

CHRIS CODE:	OTH	CASE NUMBER:	000000-00-0
UN NUMBER:		REACHED WATER:	NO

NAME OF MATERIAL:	NON-HAZARDOUS OIL SEDIMENT
AMOUNT OF MATERIAL:	2 GALLON(S)
AMOUNT IN WATER:	

OTHER MATERIAL INFORMATION

MOBILE DETAILS INFORMATION

TRAIN INFORMATION

TRAIN NAME/NUMBER:	NATX76511	RAILROAD NAME:	CANADIAN PACIFIC RAILROAD
TRAIN TYPE:	RAIL CAR	TRACK SPEED:	
TRAIN SPEED:		TRAIN DIRECTION:	
NUMBER OF LOCOMOTIVES:		NUMBER OF CARS:	
NUMBER DERAILED:		NON COMPLIANCE WITH HAZMAT:	N

VESSEL INFORMATION

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

BRNS

SEARCH ID: 73 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME: CANADIAN PACIFIC RAILWAY	REV: 12/31/06
ADDRESS: UNKNOWN	ID1: NRC-796330
ELGIN IL	ID2:
COOK	STATUS: RAILROAD
CONTACT:	PHONE:
SOURCE: NRC	

SITE INFORMATION

THIS INFORMATION WAS OBTAINED FROM THE NATIONAL RESPONSE CENTER

DATE RECEIVED: 5/6/2006 8:22:01 PM	DATE COMPLETE: 5/6/2006
8:26:49 PM	
CALL TAKER:	CALL TYPE: INC

RESPONSIBLE PARTY:
PHONE 1:
PHONE 2:
PHONE 3:

RESPONSIBLE COMPANY:
ORGANIZATION TYPE: UNKNOWN

ADDRESS:
XX

SOURCE: TELEPHONE

INCIDENT INFORMATION

INCIDENT DESCRIPTION: DIESEL FUEL SPILLED ONTO THE RAIL BALLAST FROM A LEAKING FUEL TANK ON AN UNKNOWN LOCOMOTIVE. THE SPILL OCCURRED OVER A 30 MILE STRETCH OF TRACK.

INCIDENT TYPE: RAILROAD	INCIDENT CAUSE: UNKNOWN
INCIDENT DATE: 5/6/2006 1:00:00 PM	INCIDENT DATE DESC:
DISCOVERED	
DISTANCE FROM CITY:	DISTANCE UNITS:
DIRECTION FROM CITY:	LOCATION SECTION:
LOCATION TOWNSHIP:	LOCATION RANGE:

AIRCRAFT TYPE:	AIRCRAFT MODEL:
AIRCRAFT ID:	AIRCRAFT FUEL CAPACITY:
AIRCRAFT FUEL CAPACITY UNITS:	AIRCRAFT FUEL ON BOARD:
AIRCRAFT FUEL ON BOARD UNITS:	AIRCRAFT SPOT NUMBER:
AIRCRAFT HANGER:	AIRCRAFT RUNWAY NUM:
ROAD MILE MARKER:	BUILDING ID:
TYPE OF FIXED OBJECT:	POWER GEN FACILITY: UNKNOWN
GENERATING CAPACITY:	TYPE OF FUEL:
NPDES:	NPDES COMPLIANCE: UNKNOWN
PIPELINE TYPE:	DOT REGULATED: UNKNOWN
PIPELINE ABOVE GROUND: ABOVE	EXPOSED UNDERWATER: NO
PIPELINE COVERED: UNKNOWN	GRADE CROSSING: NO
LOCATION SUBDIVISION: ELGIN	RAILROAD MILEPOST: 11 TO 37
TYPE VEHICLE INVOLVED:	CROSSING DEVICE TYPE:
DEVICE OPERATIONAL: YES	

DOT CROSSING NUMBER:	BRAKE FAILURE: NO
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- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME: CANADIAN PACIFIC RAILWAY		REV: 12/31/06	
ADDRESS: UNKNOWN		ID1: NRC-796330	
ELGIN IL		ID2:	
COOK		STATUS: RAILROAD	
CONTACT:		PHONE:	
SOURCE: NRC			
TANK ABOVE GROUND:	ABOVE	TRANSPORTABLE CONTAINER:	UNKNOWN
TANK REGULATED:	UNKNOWN	TANK REGULATED BY:	
TANK ID:		CAPACITY OF TANK:	
CAPACITY OF TANK UNITS:		ACTUAL AMOUNT:	
ACTUAL AMOUNT UNITS:		PLATFORM RIG NAME:	
PLATFORM LETTER:		LOCATION AREA ID:	
LOCATION BLOCK ID:			
DESCRIPTION OF TANK:			
OCSG NUMBER:		OCSF NUMBER:	
STATE LEASE NUMBER:		PIER DOCK NUMBER:	
BERTH SLIP NUMBER:		CONTIN RELEASE TYPE:	
INITIAL CONT RELEASE NUM:		CONT RELEASE PERMIT:	
ALLISION:	NO	TYPE OF STRUCTURE:	
STRUCTURE NAME:		STRUCT OPERATIONAL:	UNKNOWN
AIRBAG DEPLOYED:		DATE NORMAL SERVICE:	
SERVICE DISRUPT TIME:		SERVICE DISRUPT UNITS:	
TRANSIT BUS FLAG:		CR BEGIN DATE:	
CR END DATE:		CR CHANGE DATE:	
FIRE INVOLVED:	NO	FIRE EXTINGUISHED:	UNKNOWN
ANY EVACUATIONS:	NO	NUMBER EVACUATED:	
WHO EVACUATED:		RADIUS OF EVACUATION:	
ANY INJURIES:	NO	NUMBER INJURED:	
NUMBER HOSPITALIZED:		ANY FATALITIES:	NO
NUMBER FATALITIES:		ANY DAMAGES:	NO
DAMAGE AMOUNT:		AIR CORRIDOR CLOSED:	NO
AIR CORRIDOR DESC:		AIR CLOSURE TIME:	
WATERWAY CLOSED:	NO	WATERWAY DESC:	
WATERWAY CLOSURE TIME:		ROAD CLOSED:	NO
ROAD DESC:		ROAD CLOSURE TIME:	
CLOSURE DIRECTION:		MAJOR ARTERY:	NO
TRACK CLOSED:	NO	TRACK DESC:	
TRACK CLOSURE TIME:		MEDIA INTEREST:	NONE
MEDIUM DESC:	LAND	ADDTL MEDIUM INFO:	RAIL BALLAST
BODY OF WATER:		TRIBUTARY OF:	
NEAREST RIVER MILE MARK:		RELEASE SECURED:	UNKNOWN
EST DUR OF RELEASE:		RELEASE RATE:	
TRACK CLOSE DIR:		ST AGENCY ON SCENE:	
ST AGENCY RPT NUM:		OTHER AGENCY NOTIFIED:	
WEATHER CONDITIONS:	UNKNOWN	AIR TEMPERATURE:	
WIND SPEED:		WIND DIRECTION:	
WATER SUPPLY CONTAM:	UNKNOWN	SHEEN SIZE:	
SHEEN COLOR:		DIR OF SHEEN TRAVEL:	
SHEEN ODOR DESCRIPTION:		WAVE CONDITION:	
CURRENT SPEED:		CURRENT DIRECTION:	
WATER TEMPERATURE:			
DESC OF REMEDIAL ACTION:	MATERIAL IS UNRECOVERABLE		
EMPL FATALITY:		PASS FATALITY:	

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME: CANADIAN PACIFIC RAILWAY		REV: 12/31/06	
ADDRESS: UNKNOWN		ID1: NRC-796330	
ELGIN IL		ID2:	
COOK		STATUS: RAILROAD	
CONTACT:		PHONE:	
SOURCE: NRC			
COMMUNITY IMPACT:	NO	WIND SPEED UNITS:	
EMPLOYEE INJURIES:		PASSENGER INJURIES:	
OCCUPANT FATALITY:		CURRENT SPEED UNITS:	
ROAD CLOSURE UNITS:		TRACK CLOSURE UNITS:	
SHEEN SIZE UNITS:		STATE AGENCY NOTIFIED:	
FED AGENCY NOTIFIED:		NEAREST RIVER MILE MARK:	
SHEEN SIZE LENGTH:		SHEEN SIZE LENGTH UNITS:	
SHEEN SIZE WIDTH:		SHEEN SIZE WIDTH UNITS:	
OFFSHORE:	N	DURATION UNIT:	
RELEASE RATE UNIT:		RELEASE RATE RATE:	
ADDITIONAL INFO:	NONE		
<u>MATERIAL INFORMATION</u>			
CHRIS CODE:	ODS	CASE NUMBER:	000000-00-0
UN NUMBER:		REACHED WATER:	NO
NAME OF MATERIAL:	OIL: DIESEL		
AMOUNT OF MATERIAL:	0 UNKNOWN AMOUNT		
AMOUNT IN WATER:			
<u>OTHER MATERIAL INFORMATION</u>			
<u>MOBILE DETAILS INFORMATION</u>			
<u>TRAIN INFORMATION</u>			
TRAIN NAME/NUMBER:	N/A	RAILROAD NAME:	CANADIAN PACIFIC RAILWAY
TRAIN TYPE:	UNKNOWN	TRACK SPEED:	
TRAIN SPEED:		TRAIN DIRECTION:	
NUMBER OF LOCOMOTIVES:		NUMBER OF CARS:	
NUMBER DERAILED:		NON COMPLIANCE WITH HAZMAT:	N
<u>VESSEL INFORMATION</u>			

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME: IN THE RAIL YARD 29 STATE ROAD ADDRESS: IN THE RAIL YARD 29 STATE RD ELGIN IL KANE CONTACT: SOURCE: NRC		REV: 9/13/09 ID1: NRC-912959 ID2: STATUS: RAILROAD NON-RELEASE PHONE:	
<u>SITE INFORMATION</u>			
THIS INFORMATION WAS OBTAINED FROM THE NATIONAL RESPONSE CENTER			
INCIDENT DATE: REPORTED DATE: TYPE OF INCIDENT: CAUSE OF INCIDENT: MEDIUM AFFECTED: MATERIAL NAME: LOCATION: SUSPECTED COMPANY:	28-JUL-2009 05:30 28-JUL-2009 08:44 RAILROAD NON-RELEASE DERAILMENT RAIL REPORT (N/A) IN THE RAIL YARD 29 STATE ROAD METRA RAILROAD		
DESCRIPTION:	CALLER STATED THERE WAS A COMMUTER TRAIN DERAILMENT INVOLVING THREE CARS. THERE ARE NO INJURIES REPORTED DUE TO THIS INCIDENT.		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS

SEARCH ID: 79 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME:		REV:	12/31/06
ADDRESS:	ELGIN and ROUTE 31	ID1:	NRC-821863
	ELGIN IL	ID2:	
	KANE	STATUS:	RAILROAD NON-RELEASE
CONTACT:		PHONE:	
SOURCE:	NRC		

SITE INFORMATION

THIS INFORMATION WAS OBTAINED FROM THE NATIONAL RESPONSE CENTER

DATE RECEIVED:	12/22/2006 5:12:10 PM	DATE COMPLETE:	
12/22/2006 5:17:51 PM			
CALL TAKER:	CALL TYPE:		INC

RESPONSIBLE PARTY:
PHONE 1:
PHONE 2:
PHONE 3:

RESPONSIBLE COMPANY: METRA RAILROAD
ORGANIZATION TYPE: PRIVATE ENTERPRISE

ADDRESS: CHICAGO IL 60661

SOURCE: TELEPHONE

INCIDENT INFORMATION

INCIDENT DESCRIPTION: THE CALLER IS REPORTING A TRAIN STRIKING A VEHICLE AT A GRADE CROSSING RESULTING IN NO INJURIES OR FATALITY AT THIS TIME.

INCIDENT TYPE:	RAILROAD NON-RELEASE	INCIDENT CAUSE:	UNKNOWN
INCIDENT DATE:	12/22/2006 3:40:00 PM	INCIDENT DATE DESC:	

OCCURRED		DISTANCE UNITS:	
DISTANCE FROM CITY:		LOCATION SECTION:	
DIRECTION FROM CITY:		LOCATION RANGE:	
LOCATION TOWNSHIP:			

AIRCRAFT TYPE:		AIRCRAFT MODEL:	
AIRCRAFT ID:		AIRCRAFT FUEL CAPACITY:	
AIRCRAFT FUEL CAPACITY UNITS:		AIRCRAFT FUEL ON BOARD:	
AIRCRAFT FUEL ON BOARD UNITS:		AIRCRAFT SPOT NUMBER:	
AIRCRAFT HANGER:		AIRCRAFT RUNWAY NUM:	
ROAD MILE MARKER:		BUILDING ID:	
TYPE OF FIXED OBJECT:		POWER GEN FACILITY:	UNKNOWN
GENERATING CAPACITY:		TYPE OF FUEL:	
NPDES:		NPDES COMPLIANCE:	UNKNOWN
PIPELINE TYPE:		DOT REGULATED:	UNKNOWN
PIPELINE ABOVE GROUND:	ABOVE	EXPOSED UNDERWATER:	NO
PIPELINE COVERED:	UNKNOWN	GRADE CROSSING:	YES
LOCATION SUBDIVISION:		RAILROAD MILEPOST:	
TYPE VEHICLE INVOLVED:	UNKNOWN	CROSSING DEVICE TYPE:	GATES
DEVICE OPERATIONAL:	YES		

DOT CROSSING NUMBER:		BRAKE FAILURE:	UNKNOWN
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- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS

SEARCH ID: 79 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME:		REV:	12/31/06
ADDRESS:	ELGIN and ROUTE 31	ID1:	NRC-821863
	ELGIN IL	ID2:	
	KANE	STATUS:	RAILROAD NON-RELEASE
CONTACT:		PHONE:	
SOURCE:	NRC		

TANK ABOVE GROUND:	ABOVE	TRANSPORTABLE CONTAINER:	UNKNOWN
TANK REGULATED:	UNKNOWN	TANK REGULATED BY:	
TANK ID:		CAPACITY OF TANK:	
CAPACITY OF TANK UNITS:		ACTUAL AMOUNT:	
ACTUAL AMOUNT UNITS:		PLATFORM RIG NAME:	
PLATFORM LETTER:		LOCATION AREA ID:	
LOCATION BLOCK ID:			

DESCRIPTION OF TANK:

OCSG NUMBER:		OCSF NUMBER:	
STATE LEASE NUMBER:		PIER DOCK NUMBER:	
BERTH SLIP NUMBER:		CONTIN RELEASE TYPE:	
INITIAL CONT RELEASE NUM:		CONT RELEASE PERMIT:	
ALLISION:	UNKNOWN	TYPE OF STRUCTURE:	
STRUCTURE NAME:		STRUCT OPERATIONAL:	UNKNOWN
AIRBAG DEPLOYED:	UNKNOWN	DATE NORMAL SERVICE:	
SERVICE DISRUPT TIME:		SERVICE DISRUPT UNITS:	
TRANSIT BUS FLAG:		CR BEGIN DATE:	
CR END DATE:		CR CHANGE DATE:	
FIRE INVOLVED:	NO	FIRE EXTINGUISHED:	UNKNOWN
ANY EVACUATIONS:	NO	NUMBER EVACUATED:	
WHO EVACUATED:		RADIUS OF EVACUATION:	
ANY INJURIES:	NO	NUMBER INJURED:	
NUMBER HOSPITALIZED:		ANY FATALITIES:	NO
NUMBER FATALITIES:		ANY DAMAGES:	NO
DAMAGE AMOUNT:		AIR CORRIDOR CLOSED:	NO
AIR CORRIDOR DESC:		AIR CLOSURE TIME:	
WATERWAY CLOSED:	NO	WATERWAY DESC:	
WATERWAY CLOSURE TIME:		ROAD CLOSED:	NO
ROAD DESC:		ROAD CLOSURE TIME:	
CLOSURE DIRECTION:		MAJOR ARTERY:	NO
TRACK CLOSED:	YES	TRACK DESC:	MAIN LINE
TRACK CLOSURE TIME:	1	MEDIA INTEREST:	NONE
MEDIUM DESC:	NON-RELEASE (N/A)	ADDTL MEDIUM INFO:	GRADE
CROSSING INCIDENT			
BODY OF WATER:		TRIBUTARY OF:	
NEAREST RIVER MILE MARK:		RELEASE SECURED:	UNKNOWN
EST DUR OF RELEASE:		RELEASE RATE:	
TRACK CLOSE DIR:	ALL	ST AGENCY ON SCENE:	NONE
ST AGENCY RPT NUM:	NONE	OTHER AGENCY NOTIFIED:	
WEATHER CONDITIONS:	CLEAR	AIR TEMPERATURE:	50
WIND SPEED:		WIND DIRECTION:	
WATER SUPPLY CONTAM:	UNKNOWN	SHEEN SIZE:	
SHEEN COLOR:		DIR OF SHEEN TRAVEL:	
SHEEN ODOR DESCRIPTION:		WAVE CONDITION:	
CURRENT SPEED:		CURRENT DIRECTION:	
WATER TEMPERATURE:			
DESC OF REMEDIAL ACTION:	NONE		

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS

SEARCH ID: 79 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME:		REV:	12/31/06
ADDRESS:	ELGIN and ROUTE 31	ID1:	NRC-821863
	ELGIN IL	ID2:	
	KANE	STATUS:	RAILROAD NON-RELEASE
CONTACT:		PHONE:	
SOURCE:	NRC		

EMPL FATALITY:		PASS FATALITY:	
COMMUNITY IMPACT:	NO	WIND SPEED UNITS:	
EMPLOYEE INJURIES:		PASSENGER INJURIES:	
OCCUPANT FATALITY:		CURRENT SPEED UNITS:	
ROAD CLOSURE UNITS:		TRACK CLOSURE UNITS:	
SHEEN SIZE UNITS:		STATE AGENCY NOTIFIED:	NONE
FED AGENCY NOTIFIED:	NONE	NEAREST RIVER MILE MARK:	
SHEEN SIZE LENGTH:		SHEEN SIZE LENGTH UNITS:	
SHEEN SIZE WIDTH:		SHEEN SIZE WIDTH UNITS:	
OFFSHORE:	N	DURATION UNIT:	
RELEASE RATE UNIT:		RELEASE RATE RATE:	

ADDITIONAL INFO: THE CALLER HAD NO ADDITIONAL INFORMATION

MATERIAL INFORMATION

OTHER MATERIAL INFORMATION

MOBILE DETAILS INFORMATION

TRAIN INFORMATION

TRAIN NAME/NUMBER:	2242	RAILROAD NAME:	METRA RAILROAD
TRAIN TYPE:	COMMUTER	TRACK SPEED:	30
TRAIN SPEED:	30	TRAIN DIRECTION:	S
NUMBER OF LOCOMOTIVES:	1	NUMBER OF CARS:	8
NUMBER DERAILED:		NON COMPLIANCE WITH HAZMAT:	N

VESSEL INFORMATION

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS:

SEARCH ID: 78 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME: MILEPOST 38	REV: 12/31/05
ADDRESS: UNKNOWN	ID1: NRC-782950
ELGIN IL	ID2:
KANE	STATUS: RAILROAD NON-RELEASE
CONTACT: UNKNOWN	PHONE:
SOURCE: NRC	

SITE INFORMATION

THIS INFORMATION WAS OBTAINED FROM THE NATIONAL RESPONSE CENTER

DATE RECEIVED: 12/19/2005 8:20:33 PM	12/19/2005 8:17:00 PM	DATE COMPLETE:
CALL TAKER:	TMM0760	CALL TYPE: INC

RESPONSIBLE PARTY: UNKNOWN
PHONE 1:
PHONE 2:
PHONE 3:

RESPONSIBLE COMPANY:
ORGANIZATION TYPE: UNKNOWN

ADDRESS: XX

SOURCE: TELEPHONE

INCIDENT INFORMATION

INCIDENT DESCRIPTION: CALLER STATED THAT A METRA RAILROAD TRAIN HIT A TRUCK AND KILLED THE DRIVER.

INCIDENT TYPE: RAILROAD NON-RELEASE	INCIDENT CAUSE: UNKNOWN
INCIDENT DATE: 12/19/2005 5:00:00 PM	INCIDENT DATE DESC:

OCCURRED	DISTANCE UNITS:
DISTANCE FROM CITY:	LOCATION SECTION:
DIRECTION FROM CITY:	LOCATION RANGE:
LOCATION TOWNSHIP:	

AIRCRAFT TYPE:	AIRCRAFT MODEL:	
AIRCRAFT ID:	AIRCRAFT FUEL CAPACITY:	
AIRCRAFT FUEL CAPACITY UNITS:	AIRCRAFT FUEL ON BOARD:	
AIRCRAFT FUEL ON BOARD UNITS:	AIRCRAFT SPOT NUMBER:	
AIRCRAFT HANGER:	AIRCRAFT RUNWAY NUM:	
ROAD MILE MARKER:	BUILDING ID:	
TYPE OF FIXED OBJECT:	POWER GEN FACILITY:	U
GENERATING CAPACITY:	TYPE OF FUEL:	
NPDES:	NPDES COMPLIANCE:	U
PIPELINE TYPE:	DOT REGULATED:	U
PIPELINE ABOVE GROUND: ABOVE	EXPOSED UNDERWATER:	N
PIPELINE COVERED: U	GRADE CROSSING:	N
LOCATION SUBDIVISION: CHICAGO UNION	RAILROAD MILEPOST:	
TYPE VEHICLE INVOLVED: STATIO	CROSSING DEVICE TYPE:	
DEVICE OPERATIONAL: Y		

DOT CROSSING NUMBER:	BRAKE FAILURE:	N
TANK ABOVE GROUND: ABOVE	TRANSPORTABLE CONTAINER:	U

- Continued on next page -

Environmental FirstSearch Site Detail Report

Target Property:

ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME: MILEPOST 38		REV: 12/31/05	
ADDRESS: UNKNOWN		ID1: NRC-782950	
ELGIN IL		ID2:	
KANE		STATUS: RAILROAD NON-RELEASE	
CONTACT: UNKNOWN		PHONE:	
SOURCE: NRC			
TANK REGULATED: U		TANK REGULATED BY:	
TANK ID:		CAPACITY OF TANK:	
CAPACITY OF TANK UNITS:		ACTUAL AMOUNT:	
ACTUAL AMOUNT UNITS:		PLATFORM RIG NAME:	
PLATFORM LETTER:		LOCATION AREA ID:	
LOCATION BLOCK ID:			
DESCRIPTION OF TANK:			
OCSG NUMBER:		OCSF NUMBER:	
STATE LEASE NUMBER:		PIER DOCK NUMBER:	
BERTH SLIP NUMBER:		CONTIN RELEASE TYPE:	
INITIAL CONT RELEASE NUM:		CONT RELEASE PERMIT:	
ALLISION: N		TYPE OF STRUCTURE:	
STRUCTURE NAME:		STRUCT OPERATIONAL: U	
AIRBAG DEPLOYED:		DATE NORMAL SERVICE:	
SERVICE DISRUPT TIME:		SERVICE DISRUPT UNITS:	
TRANSIT BUS FLAG:		CR BEGIN DATE:	
CR END DATE:		CR CHANGE DATE:	
FIRE INVOLVED: N		FIRE EXTINGUISHED: U	
ANY EVACUATIONS: N		NUMBER EVACUATED:	
WHO EVACUATED:		RADIUS OF EVACUATION:	
ANY INJURIES: N		NUMBER INJURED:	
NUMBER HOSPITALIZED:		ANY FATALITIES: Y	
NUMBER FATALITIES: I		ANY DAMAGES: N	
DAMAGE AMOUNT:		AIR CORRIDOR CLOSED: N	
AIR CORRIDOR DESC:		AIR CLOSURE TIME:	
WATERWAY CLOSED: N		WATERWAY DESC:	
WATERWAY CLOSURE TIME:		ROAD CLOSED: N	
ROAD DESC:		ROAD CLOSURE TIME:	
CLOSURE DIRECTION:		MAJOR ARTERY: N	
TRACK CLOSED: N		TRACK DESC:	
TRACK CLOSURE TIME:		MEDIA INTEREST: NONE	
MEDIUM DESC: RAIL REPORT (N/A)		ADDTL MEDIUM INFO:	
BODY OF WATER:		TRIBUTARY OF:	
NEAREST RIVER MILE MARK:		RELEASE SECURED: U	
EST DUR OF RELEASE:		RELEASE RATE:	
TRACK CLOSE DIR:		ST AGENCY ON SCENE:	
ST AGENCY RPT NUM: RC20050133		OTHER AGENCY NOTIFIED:	
WEATHER CONDITIONS:		AIR TEMPERATURE:	
WIND SPEED:		WIND DIRECTION:	
WATER SUPPLY CONTAM: U		SHEEN SIZE:	
SHEEN COLOR:		DIR OF SHEEN TRAVEL:	
SHEEN ODOR DESCRIPTION:		WAVE CONDITION:	
CURRENT SPEED:		CURRENT DIRECTION:	
WATER TEMPERATURE:			
DESC OF REMEDIAL ACTION: INVESTIGATION UNDERWAY			
EMPL FATALITY:		PASS FATALITY:	
COMMUNITY IMPACT: N		WIND SPEED UNITS:	

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

ERNS			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
78	NON GC		
NAME: MILEPOST 38		REV: 12/31/05	
ADDRESS: UNKNOWN		ID1: NRC-782950	
ELGIN IL		ID2:	
KANE		STATUS: RAILROAD NON-RELEASE	
CONTACT: UNKNOWN		PHONE:	
SOURCE: NRC			
EMPLOYEE INJURIES:		PASSENGER INJURIES:	
OCCUPANT FATALITY: 1		CURRENT SPEED UNITS:	
ROAD CLOSURE UNITS:		TRACK CLOSURE UNITS:	
SHEEN SIZE UNITS:		STATE AGENCY NOTIFIED: IL EMA	
FED AGENCY NOTIFIED: NONE		NEAREST RIVER MILE MARK:	
SHEEN SIZE LENGTH:		SHEEN SIZE LENGTH UNITS:	
SHEEN SIZE WIDTH:		SHEEN SIZE WIDTH UNITS:	
OFFSHORE: N		DURATION UNIT:	
RELEASE RATE UNIT:		RELEASE RATE RATE:	
ADDITIONAL INFO: NONE			
<u>MATERIAL INFORMATION</u>			
<u>OTHER MATERIAL INFORMATION</u>			
<u>MOBILE DETAILS INFORMATION</u>			
<u>TRAIN INFORMATION</u>			
TRAIN NAME/NUMBER: 2227		RAILROAD NAME: METRA RAILROAD	
TRAIN TYPE: PASSENGER		TRACK SPEED: 40	
TRAIN SPEED:		TRAIN DIRECTION: N	
NUMBER OF LOCOMOTIVES: 1		NUMBER OF CARS: 7	
NUMBER DERAILED:		NON COMPLIANCE WITH HAZMAT: N	
<u>VESSEL INFORMATION</u>			

**Environmental FirstSearch
Site Detail Report**

Target Property:

ELGIN IL 60120

JOB: ELGIN-RR-TRACK

FED IC/EC

SEARCH ID: 95 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME:	TRI-COUNTY LANDFILL CO./WASTE MANAGEMENT OF ILLINO	REV:	6/2/10
ADDRESS:	STATE ROUTE 25	ID1:	ILD048306138-IC
	SOUTH ELGIN IL 60120	ID2:	0500340
	KANE	STATUS:	EPA INST CONTROL
CONTACT:		PHONE:	
SOURCE:	EPA		

SITE INFORMATION

INSTITUTIONAL CONTROL:	ACCESS RESTRICTION, FENCING		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999
CONTAMINATED MEDIA:	GROUNDWATER		

INSTITUTIONAL CONTROL:	WATER SUPPLY USE RESTRICTION		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999
CONTAMINATED MEDIA:	GROUNDWATER		

INSTITUTIONAL CONTROL:	SWIMMING RESTRICTION		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999
CONTAMINATED MEDIA:	OTHER		

INSTITUTIONAL CONTROL:	ACCESS RESTRICTION, FENCING		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999
CONTAMINATED MEDIA:	SEDIMENT		

INSTITUTIONAL CONTROL:	DEED RESTRICTION		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999
CONTAMINATED MEDIA:	SEDIMENT		

INSTITUTIONAL CONTROL:	ACCESS RESTRICTION, FENCING		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999
CONTAMINATED MEDIA:	SOIL		

INSTITUTIONAL CONTROL:	DEED RESTRICTION		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999
CONTAMINATED MEDIA:	SOIL		

INSTITUTIONAL CONTROL: ACCESS RESTRICTION, FENCING

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

FED IC/EC			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME: TRI-COUNTY LANDFILL CO./WASTE MANAGEMENT OF ILLINO	REV: 6/2/10		
ADDRESS: STATE ROUTE 25	ID1: ILD048306138-IC		
SOUTH ELGIN IL 60120	ID2: 0500340		
KANE	STATUS: EPA INST CONTROL		
CONTACT:	PHONE:		
SOURCE: EPA			
ACTION NAME: 003	ACTION COMPLETED: 7/14/1999		
ACTION PLANNED COMPL: 7/30/1999			
CONTAMINATED MEDIA: SURFACE WATER			
INSTITUTIONAL CONTROL: ACCESS RESTRICTION, FENCING			
ACTION NAME: 001			
ACTION PLANNED COMPL: 9/30/1992	ACTION COMPLETED: 9/30/1992		
CONTAMINATED MEDIA: GROUNDWATER			
INSTITUTIONAL CONTROL: WATER SUPPLY USE RESTRICTION			
ACTION NAME: 001			
ACTION PLANNED COMPL: 9/30/1992	ACTION COMPLETED: 9/30/1992		
CONTAMINATED MEDIA: GROUNDWATER			
INSTITUTIONAL CONTROL: SWIMMING RESTRICTION			
ACTION NAME: 001			
ACTION PLANNED COMPL: 9/30/1992	ACTION COMPLETED: 9/30/1992		
CONTAMINATED MEDIA: OTHER			
INSTITUTIONAL CONTROL: ACCESS RESTRICTION, FENCING			
ACTION NAME: 001			
ACTION PLANNED COMPL: 9/30/1992	ACTION COMPLETED: 9/30/1992		
CONTAMINATED MEDIA: SEDIMENT			
INSTITUTIONAL CONTROL: DEED RESTRICTION			
ACTION NAME: 001			
ACTION PLANNED COMPL: 9/30/1992	ACTION COMPLETED: 9/30/1992		
CONTAMINATED MEDIA: SEDIMENT			
INSTITUTIONAL CONTROL: ACCESS RESTRICTION, FENCING			
ACTION NAME: 001			
ACTION PLANNED COMPL: 9/30/1992	ACTION COMPLETED: 9/30/1992		
CONTAMINATED MEDIA: SOIL			
INSTITUTIONAL CONTROL: DEED RESTRICTION			
ACTION NAME: 001			
ACTION PLANNED COMPL: 9/30/1992	ACTION COMPLETED: 9/30/1992		
CONTAMINATED MEDIA: SOIL			

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

FED IC/EC

SEARCH ID: 95 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME:	TRI-COUNTY LANDFILL CO./WASTE MANAGEMENT OF ILLINO	REV:	6/2/10
ADDRESS:	STATE ROUTE 25	ID1:	ILD048306138-IC
	SOUTH ELGIN IL 60120	ID2:	0500340
	KANE	STATUS:	EPA INST CONTROL
CONTACT:		PHONE:	
SOURCE:	EPA		

INSTITUTIONAL CONTROL:	ACCESS RESTRICTION, FENCING
ACTION NAME:	001
ACTION PLANNED COMPL:	9/30/1992 ACTION COMPLETED: 9/30/1992
CONTAMINATED MEDIA:	SURFACE WATER

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

FED IC/EC

SEARCH ID: 94 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME:	TRI-COUNTY LANDFILL CO./WASTE MANAGEMENT OF ILLINO	REV:	6/2/10
ADDRESS:	STATE ROUTE 25	ID1:	ILD048306138-EC
	SOUTH ELGIN IL 60120	ID2:	0500340
	KANE	STATUS:	EPA ENG CONTROL
CONTACT:		PHONE:	
SOURCE:	EPA		

ENGINEERING CONTROL:	DISPOSAL		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999
CONTAMINATED MEDIA:	DEBRIS		

ENGINEERING CONTROL:	DISCHARGE		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999
CONTAMINATED MEDIA:	GROUNDWATER		

ENGINEERING CONTROL:	NATURAL ATTENUATION		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999
CONTAMINATED MEDIA:	GROUNDWATER		

ENGINEERING CONTROL:	OPERATIONS and MAINTENANCE (OandM)		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999
CONTAMINATED MEDIA:	GROUNDWATER		

ENGINEERING CONTROL:	OTHER, (N.O.S.)		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999
CONTAMINATED MEDIA:	GROUNDWATER		

ENGINEERING CONTROL:	PUBLICLY OWNED TREATMENT WORKS (POTW)		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999
CONTAMINATED MEDIA:	GROUNDWATER		

ENGINEERING CONTROL:	CAP		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999
CONTAMINATED MEDIA:	SEDIMENT		

ENGINEERING CONTROL:	CONSOLIDATE		
ACTION NAME:	003		
ACTION PLANNED COMPL:	7/30/1999	ACTION COMPLETED:	7/14/1999

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**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

FED IC/EC			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME: TRI-COUNTY LANDFILL CO./WASTE MANAGEMENT OF ILLINO		REV: 6/2/10	
ADDRESS: STATE ROUTE 25		ID1: ILD048306138-EC	
SOUTH ELGIN IL 60120		ID2: 0500340	
KANE		STATUS: EPA ENG CONTROL	
CONTACT:		PHONE:	
SOURCE: EPA			
CONTAMINATED MEDIA:	SEDIMENT		
ENGINEERING CONTROL:	DISPOSAL		
ACTION NAME:	003	ACTION COMPLETED:	7/14/1999
ACTION PLANNED COMPL:	7/30/1999		
CONTAMINATED MEDIA:	SEDIMENT		
ENGINEERING CONTROL:	EXCAVATION		
ACTION NAME:	003	ACTION COMPLETED:	7/14/1999
ACTION PLANNED COMPL:	7/30/1999		
CONTAMINATED MEDIA:	SEDIMENT		
ENGINEERING CONTROL:	MONITORING		
ACTION NAME:	003	ACTION COMPLETED:	7/14/1999
ACTION PLANNED COMPL:	7/30/1999		
CONTAMINATED MEDIA:	SEDIMENT		
ENGINEERING CONTROL:	REVEGETATION		
ACTION NAME:	003	ACTION COMPLETED:	7/14/1999
ACTION PLANNED COMPL:	7/30/1999		
CONTAMINATED MEDIA:	SEDIMENT		
ENGINEERING CONTROL:	CAP		
ACTION NAME:	003	ACTION COMPLETED:	7/14/1999
ACTION PLANNED COMPL:	7/30/1999		
CONTAMINATED MEDIA:	SOIL		
ENGINEERING CONTROL:	DISPOSAL		
ACTION NAME:	003	ACTION COMPLETED:	7/14/1999
ACTION PLANNED COMPL:	7/30/1999		
CONTAMINATED MEDIA:	SOIL		
ENGINEERING CONTROL:	GAS COLLECTION/TREATMENT		
ACTION NAME:	003	ACTION COMPLETED:	7/14/1999
ACTION PLANNED COMPL:	7/30/1999		
CONTAMINATED MEDIA:	SOIL		
ENGINEERING CONTROL:	MONITORING		

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**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

FED IC/EC

SEARCH ID: 94 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME:	TRI-COUNTY LANDFILL CO./WASTE MANAGEMENT OF ILLINO	REV:	6/2/10
ADDRESS:	STATE ROUTE 25	ID1:	ILD048306138-EC
	SOUTH ELGIN IL 60120	ID2:	0500340
	KANE	STATUS:	EPA ENG CONTROL
CONTACT:		PHONE:	
SOURCE:	EPA		

ACTION NAME:	003	ACTION COMPLETED:	7/14/1999
ACTION PLANNED COMPL:	7/30/1999		
CONTAMINATED MEDIA:	SOIL		

ENGINEERING CONTROL:	DISCHARGE		
ACTION NAME:	003	ACTION COMPLETED:	7/14/1999
ACTION PLANNED COMPL:	7/30/1999		
CONTAMINATED MEDIA:	SURFACE WATER		

ENGINEERING CONTROL:	MONITORING		
ACTION NAME:	003	ACTION COMPLETED:	7/14/1999
ACTION PLANNED COMPL:	7/30/1999		
CONTAMINATED MEDIA:	SURFACE WATER		

ENGINEERING CONTROL:	TREATMENT, (N.O.S.)		
ACTION NAME:	003	ACTION COMPLETED:	7/14/1999
ACTION PLANNED COMPL:	7/30/1999		
CONTAMINATED MEDIA:	SURFACE WATER		

ENGINEERING CONTROL:	CAP		
ACTION NAME:	004	ACTION COMPLETED:	7/3/2001
ACTION PLANNED COMPL:			
CONTAMINATED MEDIA:	SOIL		

ENGINEERING CONTROL:	REVEGETATION		
ACTION NAME:	004	ACTION COMPLETED:	7/3/2001
ACTION PLANNED COMPL:			
CONTAMINATED MEDIA:	SOIL		

ENGINEERING CONTROL:	SURFACE DRAINAGE CONTROL		
ACTION NAME:	004	ACTION COMPLETED:	7/3/2001
ACTION PLANNED COMPL:			
CONTAMINATED MEDIA:	SOIL		

ENGINEERING CONTROL:	DISPOSAL		
ACTION NAME:	001	ACTION COMPLETED:	9/30/1992
ACTION PLANNED COMPL:	9/30/1992		
CONTAMINATED MEDIA:	DEBRIS		

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property:

ELGIN IL 60120

JOB: ELGIN-RR-TRACK

FED IC/EC			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
94	NON GC		
NAME: TRI-COUNTY LANDFILL CO./WASTE MANAGEMENT OF ILLINO ADDRESS: STATE ROUTE 25 SOUTH ELGIN IL 60120 KANE CONTACT: SOURCE: EPA		REV: 6/2/10 ID1: ILD048306138-EC ID2: 0500340 STATUS: EPA ENG CONTROL PHONE:	
ENGINEERING CONTROL: DISCHARGE ACTION NAME: 001 ACTION PLANNED COMPL: 9/30/1992 CONTAMINATED MEDIA: GROUNDWATER		ACTION COMPLETED: 9/30/1992	
ENGINEERING CONTROL: NATURAL ATTENUATION ACTION NAME: 001 ACTION PLANNED COMPL: 9/30/1992 CONTAMINATED MEDIA: GROUNDWATER		ACTION COMPLETED: 9/30/1992	
ENGINEERING CONTROL: OPERATIONS and MAINTENANCE (OandM) ACTION NAME: 001 ACTION PLANNED COMPL: 9/30/1992 CONTAMINATED MEDIA: GROUNDWATER		ACTION COMPLETED: 9/30/1992	
ENGINEERING CONTROL: OTHER, (N.O.S.) ACTION NAME: 001 ACTION PLANNED COMPL: 9/30/1992 CONTAMINATED MEDIA: GROUNDWATER		ACTION COMPLETED: 9/30/1992	
ENGINEERING CONTROL: PUBLICLY OWNED TREATMENT WORKS (POTW) ACTION NAME: 001 ACTION PLANNED COMPL: 9/30/1992 CONTAMINATED MEDIA: GROUNDWATER		ACTION COMPLETED: 9/30/1992	
ENGINEERING CONTROL: CAP ACTION NAME: 001 ACTION PLANNED COMPL: 9/30/1992 CONTAMINATED MEDIA: SEDIMENT		ACTION COMPLETED: 9/30/1992	
ENGINEERING CONTROL: CONSOLIDATE ACTION NAME: 001 ACTION PLANNED COMPL: 9/30/1992 CONTAMINATED MEDIA: SEDIMENT		ACTION COMPLETED: 9/30/1992	
ENGINEERING CONTROL: DISPOSAL ACTION NAME: 001 ACTION PLANNED COMPL: 9/30/1992		ACTION COMPLETED: 9/30/1992	

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**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

FED IC/EC			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME: TRI-COUNTY LANDFILL CO./WASTE MANAGEMENT OF ILLINO ADDRESS: STATE ROUTE 25 SOUTH ELGIN IL 60120 KANE CONTACT: SOURCE: EPA		REV: 6/2/10 ID1: ILD048306138-EC ID2: 0500340 STATUS: EPA ENG CONTROL PHONE:	
CONTAMINATED MEDIA:	SEDIMENT		
ENGINEERING CONTROL: ACTION NAME: ACTION PLANNED COMPL: CONTAMINATED MEDIA:	EXCAVATION 001 9/30/1992 SEDIMENT	ACTION COMPLETED: 9/30/1992	
ENGINEERING CONTROL: ACTION NAME: ACTION PLANNED COMPL: CONTAMINATED MEDIA:	MONITORING 001 9/30/1992 SEDIMENT	ACTION COMPLETED: 9/30/1992	
ENGINEERING CONTROL: ACTION NAME: ACTION PLANNED COMPL: CONTAMINATED MEDIA:	REVEGETATION 001 9/30/1992 SEDIMENT	ACTION COMPLETED: 9/30/1992	
ENGINEERING CONTROL: ACTION NAME: ACTION PLANNED COMPL: CONTAMINATED MEDIA:	CAP 001 9/30/1992 SOIL	ACTION COMPLETED: 9/30/1992	
ENGINEERING CONTROL: ACTION NAME: ACTION PLANNED COMPL: CONTAMINATED MEDIA:	DISPOSAL 001 9/30/1992 SOIL	ACTION COMPLETED: 9/30/1992	
ENGINEERING CONTROL: ACTION NAME: ACTION PLANNED COMPL: CONTAMINATED MEDIA:	GAS COLLECTION/TREATMENT 001 9/30/1992 SOIL	ACTION COMPLETED: 9/30/1992	
ENGINEERING CONTROL: ACTION NAME: ACTION PLANNED COMPL: CONTAMINATED MEDIA:	MONITORING 001 9/30/1992 SOIL	ACTION COMPLETED: 9/30/1992	
ENGINEERING CONTROL:	DISCHARGE		

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**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

FED IC/EC			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
94	NON GC		
NAME:	TRI-COUNTY LANDFILL CO./WASTE MANAGEMENT OF ILLINO	REV:	6/2/10
ADDRESS:	STATE ROUTE 25	ID1:	ILD048306138-EC
	SOUTH ELGIN IL 60120	ID2:	0500340
	KANE	STATUS:	EPA ENG CONTROL
CONTACT:		PHONE:	
SOURCE:	EPA		
ACTION NAME:	001	ACTION COMPLETED:	9/30/1992
ACTION PLANNED COMPL:	9/30/1992		
CONTAMINATED MEDIA:	SURFACE WATER		
ENGINEERING CONTROL:	MONITORING		
ACTION NAME:	001	ACTION COMPLETED:	9/30/1992
ACTION PLANNED COMPL:	9/30/1992		
CONTAMINATED MEDIA:	SURFACE WATER		
ENGINEERING CONTROL:	TREATMENT, (N.O.S.)		
ACTION NAME:	001	ACTION COMPLETED:	9/30/1992
ACTION PLANNED COMPL:	9/30/1992		
CONTAMINATED MEDIA:	SURFACE WATER		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 90 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME: CHICAGO GRAVEL CO.	REV: 6/11/10
ADDRESS: SR-25	ID1: 0894385586-991052
ELGIN IL 60120	ID2: 991052
COOK	STATUS: CLOSED
CONTACT: TERRY BROMM	PHONE: 8472516616
SOURCE: IL EPA	

SITE INFORMATION

DATE REPORTED: 4/29/1999
IEMA NUMBER: 991052

CONTENTS/PRODUCT

GASOLINE: YES
UNLEADED GASOLINE: NO
DIESEL FUEL: NO
FUEL OIL: NO
JET FUEL: NO
USED/WASTE OIL: NO
NON-PETROLEUM PRODUCT: NO
PETROLEUM: NO

NON-LUST LETTER SENT:
SEC 57.5G LETTER SENT:
NFR LETTER SENT: 3/9/2000
SITE CLASSIFICATION: NFA
IEPA PROJECT MANAGER: ROSSI

IEPA CORRESPONDENCE

DATE: 5/11/1999	DESCRIPTION: NOTICE OF RELEASE LETTER SENT
DATE: 2/14/2000	DESCRIPTION: MISCELLANEOUS CORRESPONDENCE RECEIVED
DATE: 7/21/2000	DESCRIPTION: NFR REMINDER RESPONSE RECEIVED
DATE: 6/25/1999	DESCRIPTION: 45 DAY SELECTION RECEIVED LETTER SENT

TITLE XVI INFORMATION

DOCUMENT: SITE CLASS
RECEIVED: 7/19/1999
RESPONSE DUE: 11/16/1999
RESPONSE MAILED: 8/18/1999
RESPONSE TYPE: APR

DOCUMENT: SITE CLASS
RECEIVED: 12/8/1999
RESPONSE DUE: 4/6/2000
RESPONSE MAILED: 3/9/2000
RESPONSE TYPE: APR

ENGINEERING CONTROLS

BARRIER STRUCTURE: NO	BARRIER PAVEMENT: NO
BARRIER SOIL: NO	BARRIER OTHER: NO

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME: CHICAGO GRAVEL CO.		REV: 6/11/10	
ADDRESS: SR-25		ID1: 0894385586-991052	
ELGIN IL 60120		ID2: 991052	
COOK		STATUS: CLOSED	
CONTACT: TERRY BROMM		PHONE: 8472516616	
SOURCE: IL EPA			
BARRIER OTHER DESC:			
<u>INSTITUTIONAL CONTROLS</u>			
GW USE:	NO	INDUST COM:	NO
ORDINANCE:	NO	WORKER:	NO
OTHER:	NO		
OTHER DESC:			
<u>ENVIRONMENTAL LAND USE CONTROLS</u>			
GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			
HWY AUTH AGREEMENT:	NO		
AGREEMENT DESC:	No		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 91 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME:	FOX RIVER WATER RECLAMATION DIST.	REV:	6/11/10
ADDRESS:	RT. 31 and DANA ST ELGIN IL 60121	ID1:	0894385615-20011323
	KANE	ID2:	20011323
CONTACT:	RICK MANNER	STATUS:	ACTIVE
SOURCE:	IL EPA	PHONE:	8476958839

SITE INFORMATION

DATE REPORTED: 8/3/2001
IEMA NUMBER: 20011323

CONTENTS/PRODUCT

GASOLINE: NO
UNLEADED GASOLINE: NO
DIESEL FUEL: NO
FUEL OIL: NO
JET FUEL: NO
USED/WASTE OIL: NO
NON-PETROLEUM PRODUCT: NO
PETROLEUM: YES

NON-LUST LETTER SENT:
SEC 57.5G LETTER SENT:
NFR LETTER SENT:
SITE CLASSIFICATION:
IEPA PROJECT MANAGER: NOT ASSIGNED

IEPA CORRESPONDENCE

DATE: 10/5/2001 **DESCRIPTION:** 45 DAY SELECTION RECEIVED LETTER SENT
DATE: 8/6/2001 **DESCRIPTION:** NOTICE OF RELEASE LETTER SENT

TITLE XVI INFORMATION

ENGINEERING CONTROLS

BARRIER STRUCTURE: NO **BARRIER PAVEMENT:** NO
BARRIER SOIL: NO **BARRIER OTHER:** NO
BARRIER OTHER DESC:

INSTITUTIONAL CONTROLS

GW USE: NO **INDUST COM:** NO
ORDINANCE: NO **WORKER:** NO
OTHER: NO
OTHER DESC:

ENVIRONMENTAL LAND USE CONTROLS

GW USE: NO **LAND USE:** NO
ENG BARRIER: NO **WORKER CAUTION:** NO
SOIL HANDLING: NO **OTHER:** NO

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**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 91 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME: FOX RIVER WATER RECLAMATION DIST.
ADDRESS: RT. 31 and DANA ST
ELGIN IL 60121
KANE
CONTACT: RICK MANNER
SOURCE: IL EPA

REV: 6/11/10
ID1: 0894385615-20011323
ID2: 20011323
STATUS: ACTIVE
PHONE: 8476958839

OTHER DESC:

HWY AUTH AGREEMENT: NO
AGREEMENT DESC: No

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST

SEARCH ID: 92 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME:	CENTRAL BLACKTOP CO.	REV:	6/11/10
ADDRESS:	RT. 31	ID1:	0890805023-912099
	SOUTH ELGIN IL 60177	ID2:	912099
	KANE	STATUS:	CLOSED
CONTACT:	JOSEPH BENSON	PHONE:	8474829660
SOURCE:	IL EPA		

SITE INFORMATION

DATE REPORTED: 7/31/1991
IEMA NUMBER: 912099

CONTENTS/PRODUCT

GASOLINE: NO
UNLEADED GASOLINE: NO
DIESEL FUEL: YES
FUEL OIL: NO
JET FUEL: NO
USED/WASTE OIL: YES
NON-PETROLEUM PRODUCT: NO
PETROLEUM: NO

NON-LUST LETTER SENT:
SEC 57.5G LETTER SENT:
NFR LETTER SENT: 9/4/1997
SITE CLASSIFICATION:
IEPA PROJECT MANAGER: PUTRICH

IEPA CORRESPONDENCE

DATE: 4/1/1992	DESCRIPTION:	RESPONSE LETTER RECEIVED
DATE: 2/10/1995	DESCRIPTION:	REVIEW LETTER SENT
DATE: 8/4/1993	DESCRIPTION:	REVIEW LETTER SENT
DATE: 1/26/1994	DESCRIPTION:	REVIEW LETTER SENT
DATE: 4/7/1995	DESCRIPTION:	REVIEW LETTER SENT
DATE: 8/2/1991	DESCRIPTION:	NOTICE OF RELEASE LETTER SENT
DATE: 8/14/1997	DESCRIPTION:	PROFESSIONAL ENGINEER CERTIFICATION RECEIVED
DATE: 8/16/1995	DESCRIPTION:	PROFESSIONAL ENGINEER CERTIFICATION RECEIVED
DATE: 6/23/1995	DESCRIPTION:	REVIEW LETTER SENT

TITLE XVI INFORMATION

DOCUMENT: CORRECTIVE
RECEIVED: 8/14/1997
RESPONSE DUE: 12/12/1997
RESPONSE MAILED: 9/4/1997
RESPONSE TYPE: APR

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

LUST			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME: CENTRAL BLACKTOP CO.		REV: 6/11/10	
ADDRESS: RT. 31		ID1: 0890805023-912099	
SOUTH ELGIN IL 60177		ID2: 912099	
KANE		STATUS: CLOSED	
CONTACT: JOSEPH BENSON		PHONE: 8474829660	
SOURCE: IL EPA			
<u>ENGINEERING CONTROLS</u>			
BARRIER STRUCTURE:	NO	BARRIER PAVEMENT:	NO
BARRIER SOIL:	NO	BARRIER OTHER:	NO
BARRIER OTHER DESC:			
<u>INSTITUTIONAL CONTROLS</u>			
GW USE:	NO	INDUST COM:	NO
ORDINANCE:	NO	WORKER:	NO
OTHER:	NO		
OTHER DESC:			
<u>ENVIRONMENTAL LAND USE CONTROLS</u>			
GW USE:	NO	LAND USE:	NO
ENG BARRIER:	NO	WORKER CAUTION:	NO
SOIL HANDLING:	NO	OTHER:	NO
OTHER DESC:			
HWY AUTH AGREEMENT:	NO		
AGREEMENT DESC:	No		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

NPL

SEARCH ID: 68 DIST/DIR: NON GC ELEVATION: MAP ID:

NAME:	TRI-COUNTY LANDFILL/WASTE MGMT ILLINOIS	REV:	8/1/10
ADDRESS:	RTE 25	ID1:	ILD048306138
	SOUTH ELGIN IL 60120	ID2:	0500340
	KANE	STATUS:	FINAL
CONTACT:	GLADYS WATTS	PHONE:	3128867591
SOURCE:	EPA		

SITE INFORMATION

EVENT TYPE

SITE DISCOVERY BY:	EPA	DISCOVERY DATE:	04-01-79
SITE PROPOSED BY:	EPA	PROPOSED DATE:	06-10-86
FINAL LIST BY:	EPA	FINAL LIST DATE:	03-31-89

ACTIVITIES: GRAVEL MINING OPEATION

CONTAMINANTS: VOCS,CYANIDE

SOURCE OF CONTAMINATION:

CONTAMINATED: GROUNDWATER
THREATENED: WETLANDS

SITE DESCRIPTION

Conditions at proposal (June 10, 1986): The Tri-County Landfill Co./Waste Management of Illinois, Inc., Site covers approximately 46 acres in South Elgin, Kane County, Illinois. The area was previously part of a gravel mining operation. The land surrounding the site is predominantly agricultural, although directly west are the Woodlands I and II landfills owned by Waste Management.

The site was originally owned and operated by Tri-County Landfill Co. (1968-73). It had a permit from the State to accept general municipal refuse. Waste Management of Illinois, Inc., operated the site from 1973 until it closed in 1977. The property owner from 1973 to present is Michigan Avenue National Bank.

In May 1984, EPA detected cyanide, benzene, chlorobenzene, and 1,1-dichloroethane in monitoring wells downgradient of the landfill. Over 10,000 people use wells within 3 miles of the site for drinking water. This figure includes the towns of Valley View and South Elgin, which are served by municipal systems. A residential well is 1,800 feet from the site.

The Fox River, approximately 1 mile west of the site, is used extensively for fishing and boating. A fresh water wetland is 1,100 feet from the site.

In March 1971, the Elgin Jaycees filed suit against Tri-County Landfill Co. for allegedly violating the Illinois Environmental Protection Act with respect to water, soil, leaching, and air pollution. In 1973, the Illinois Pollution Control Board ruled in favor of the complainants, issuing a penalty and invoking a bond to be posted to ensure compliance with two Consent Orders resulting from findings in the case. There are no records indicating further actions.

Status (March 31, 1989): After negotiations with Waste Management failed, EPA started planning a remedial investigation/ feasibility study to determine the type and extent of contamination at the site and identify alternatives for remedial action.

CONSTRUCTION COMPLETED DATE: 11/01/2001
FINAL DATE: 03/31/1989

CERCLIS DETAILS

ACTION/QUALITY	AGENCY/RPS	START/RAA	END
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- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

NPL					
SEARCH ID:	68	DIST/DIR:	NON GC	ELEVATION:	MAP ID:
NAME:	TRI-COUNTY LANDFILL/WASTE MGMT ILLINOIS	REV:	8/1/10		
ADDRESS:	RTE 25 SOUTH ELGIN IL 60120 KANE	ID1:	ILD048306138		
CONTACT:	GLADYS WATTS	ID2:	0500340		
SOURCE:	EPA	STATUS:	FINAL		
		PHONE:	3128867591		
five-year review	EPA Fund-Financed	1/5/2009	9/3/2009		
five-year review	Responsible Party	3/30/2004	9/23/2004		
potentially responsible party remedial action Interim RA Report	Responsible Party Primary	4/19/2001	8/28/2002		
remedial design/remedial action negotiations	Federal Enforcement Primary	2/27/1998	9/24/1998		
potentially responsible party remedial design	Responsible Party Primary	2/2/1994	9/30/1997		
remedial design/remedial action negotiations	Federal Enforcement Primary	9/30/1993	2/2/1994		
national priorities list responsible party search	Federal Enforcement	8/22/1989	5/16/2007		
combined remedial investigation/feasibility study	EPA Fund-Financed Primary	4/22/1988	9/30/1992		
state support agency cooperative agreement	State, Fund Financed Primary	3/31/1988	9/30/2004		
remedial investigation/feasibility study negotiations	Federal Enforcement Alternate	7/8/1987	4/22/1988		
national priorities list responsible party search	Federal Enforcement Alternate		7/18/1984		
hazard ranking system package	EPA Fund-Financed		6/11/1985		
proposal to national priorities list	EPA Fund-Financed		6/10/1986		
issue request letters (104e)	Federal Enforcement		3/10/1987		
special notice issued	Federal Enforcement		6/17/1987		
notice letters issued	EPA Fund-Financed		9/10/1987		
issue request letters (104e)			7/15/1988		
final listing on national priorities list	EPA Fund-Financed		3/31/1989		

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

NPL			
SEARCH ID: 68	DIST/DIR: NON GC	ELEVATION:	MAP ID:
NAME: TRI-COUNTY LANDFILL/WASTE MGMT ILLINOIS		REV: 8/1/10	
ADDRESS: RTE 25		ID1: ILD048306138	
SOUTH ELGIN IL 60120		ID2: 0500340	
KANE		STATUS: FINAL	
CONTACT: GLADYS WATTS		PHONE: 3128867591	
SOURCE: EPA			
risk/health assessment	EPA Fund-Financed		7/24/1992
administrative order on consent	Federal Enforcement Primary		2/2/1994
issue request letters (104e)	Federal Enforcement		5/3/1994
special notice issued	Federal Enforcement		2/27/1998
administrative order on consent	Federal Enforcement		6/11/1999
administrative order on consent	Federal Enforcement		3/16/2000
lodged by doj	Federal Enforcement		5/3/2000
preliminary close-out report prepared	EPA Fund-Financed Primary		11/1/2001
aerial survey	EPA Fund-Financed		9/30/2004
lodged by doj	Federal Enforcement		3/23/2007
potentially responsible party remedial action Interim RA Report	Responsible Party Primary	6/14/1999	9/30/2000
community involvement	EPA Fund-Financed Primary	4/22/1988	9/30/1992
consent agreement (administrative)	Federal Enforcement		8/6/2003
consent decree	Federal Enforcement	3/1/2000	7/12/2000
consent decree	Federal Enforcement		5/16/2007
discovery	EPA Fund-Financed		4/1/1979
ecological risk assessment	EPA Fund-Financed		7/24/1992
explanation of significant differences	Federal Enforcement		6/25/1996

- Continued on next page -

Environmental FirstSearch Site Detail Report

Target Property:

ELGIN IL 60120

JOB: ELGIN-RR-TRACK

NPL			
SEARCH ID: 68	DIST/DIR: NON GC	ELEVATION:	MAP ID:
NAME: TRI-COUNTY LANDFILL/WASTE MGMT ILLINOIS		REV: 8/1/10	
ADDRESS: RTE 25		ID1: ILD048306138	
SOUTH ELGIN IL 60120		ID2: 0500340	
KANE		STATUS: FINAL	
CONTACT: GLADYS WATTS		PHONE: 3128867591	
SOURCE: EPA			
explanation of significant differences	Federal Enforcement		4/23/1998
explanation of significant differences	Federal Enforcement		7/14/1999
explanation of significant differences	Federal Enforcement		7/3/2001
preliminary assessment Low priority for further assessment	State, Fund Financed		2/1/1983
record of decision Final Remedy Selected at Site	EPA Fund-Financed Primary		9/30/1992
remedial design	EPA Fund-Financed	1/4/2000	4/26/2000
removal assessment	EPA Fund-Financed Primary	7/31/1991	4/27/1992
removal assessment	EPA Fund-Financed Primary	9/4/1990	9/21/1990
section 107 litigation	Federal Enforcement	10/11/2005	5/16/2007
site inspection Higher priority for further assessment	EPA Fund-Financed		10/1/1984
unilateral admin order	Federal Enforcement		9/24/1998
unilateral admin order	Federal Enforcement		11/19/1998
unilateral admin order	Federal Enforcement		11/3/1999
unilateral admin order	Federal Enforcement		11/3/1999
unilateral admin order	Federal Enforcement		4/20/2000
unilateral admin order	Federal Enforcement		4/20/2000
unilateral admin order	Federal Enforcement		11/24/2000
DESCRIPTION: CLOSED LANDFILL IN UNINCORPORATED SOUTH ELGIN (KANE COUNTY) ON A 46-ACRE TRACT OF LAND. WAS A GRAVEL QUARRY			

- Continued on next page -

Environmental FirstSearch Site Detail Report

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

NPL

SEARCH ID: 68 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

<p>NAME: TRI-COUNTY LANDFILL/WASTE MGMT ILLINOIS ADDRESS: RTE 25 SOUTH ELGIN IL 60120 KANE CONTACT: GLADYS WATTS SOURCE: EPA</p>	<p>REV: 8/1/10 ID1: ILD048306138 ID2: 0500340 STATUS: FINAL PHONE: 3128867591</p>
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surface water by surface and subsurface flow. On September 30, 1992, EPA signed a Record of Decision (ROD) selecting a remedy for the Site with the concurrence of the IEPA. A ROD addressing operable unit 1 (OU1) was completed in September 1992. EPA entered into an Administrative Order on Consent (AOC) for RD on February 2, 1994, with Waste Management of Illinois, Inc. (WMI) and Browning-Ferris Industries of Illinois, Inc. (BFI). In order to ensure that the final remedy would meet the performance standards in the ROD and the statutory requirement for long-term effectiveness of the remedy, the AOC established functional design specifications for each remedy component set forth in the ROD. With regards to the landfill cap, the ROD specified low permeability as the qualitative performance standard for the clay barrier layer. This performance standard relates to the rate at which water will infiltrate through the barrier layer, potentially leaching contaminants from the underlying waste and transporting them to ground water. In 1996, EPA issued an Explanation of Significant Differences (ESD) to defer implementation of the ground water component of the remedy and allow for a monitoring period to determine how effective the other remedy components alone would be in reducing migration of ground water contamination from the landfill. EPA's decision to issue the ESD was primarily based on the results of a pre-design investigation (PDI), where EPA used a computer-aided infiltration model to study the rate of water infiltration through the landfill surface. An ESD addressing OU1 was completed in June 1996. On September 30, 1997, EPA approved the final Remedial Design submitted by WMI and BFI. The RD included a landfill cap with different design specifications than those set forth in the ROD or AOC. The RD specifies the use of synthetic materials for the cap, namely, a 40 mil geomembrane for the barrier layer, a geonet drainage layer, a geotextile to protect the drainage layer, and approximately 18 inches of soil cover. The following discussion explains EPA's rationale for approving the modified landfill cap design and explains the associated cost differences. A second ESD addressing OU1 was completed in April 1998. Negotiations for a remedial action consent decree ended in September, 1998. On September 24, 1998, a Unilateral Administrative Order (UAO) for remedial action was then issued to WMI, and the Tri-County Landfill Company. An additional UAO was issued to BFI on November 19, 1998. The Remedial Action Work Plan was approved, and the Notice of Authorization to Proceed with the Remedial Action was transmitted to the Respondents, on May 25, 1999. The RA is expected to be completed by Fall 2000. However, because of the deferred ground water component, this Site may not qualify as a construction completion until the ground water component is either constructed or eliminated. The Preconstruction Inspection and Meeting was conducted on June 9, 1999. A de minimis settlement was offered to over 400 companies, of which 125 companies signed up for a settlement worth approximately \$2.1-million. The de minimis settlement was finalized on June 11, 1999. A third ESD addressing OU1 was completed in July 1999. The U.S. EPA issued amended UAOs to WMI and BFI on November 3, 1999. BFI is responsible for implementing the design and remedial action (RA) on the Elgin Landfill portion of the site. WMI and Tri-County Landfill Company are responsible for implementing the design and RA on the Tri-County Landfill portion of the site, including the Elgin-Wayne Property. An additional de minimis settlement was completed on March 16, 2000. UAOs, dated April 20, 2000, were issued in order to provide access to the site, as well as to alert the land owners of the activities that would be allowed and those activities that would not be allowed once the landfill cap and active gas collection system was in place. A municipal solid waste (MSW) settlement was completed on July 12, 2000. A fourth ESD addressing OU1 was completed in July 2001.

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

RCRANLR

SEARCH ID: 71 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME: DANA CORP ELGIN PLANT	REV: 7/14/10
ADDRESS: SOUTH STATE ST ELGIN IL 60120 COOK	ID1: ILD005176375
CONTACT:	ID2:
SOURCE: EPA	STATUS: NLR
	PHONE:

SITE INFORMATION

CONTACT INFORMATION: WILLIAM EDWARD
SOUTH STATE ST PO BOX 727
ELGIN IL 60120

PHONE: 3128885400

UNIVERSE INFORMATION:

GOVERNMENT PERFORMANCE AND RESULTS ACT (GPRA)

GPRA PERMIT:	N - NO
GPRA POST CLOSURE:	N - NO
GPRA CA:	N - NO
GPRA COMPLIANCE MONITORING and ENFORCEMENT:	N - NO

SUBJECT TO CORRECTIVE ACTION (SUBJCA)

SUBJCA:	N - NO
SUBJCA TSD 3004:	N - NO
SUBJCA NON TSD:	N - NO

SIGNIFICANT NON-COMPLIANCE(SNC):	N - NO
BEGINNING OF THE YEAR SNC:	N - NO
PERMIT WORKLOAD:	---
CLOSURE WORKLOAD:	---
POST CLOSURE WORKLOAD:	---
PERMITTING /CLOSURE/POST-CLOSURE PROGRESS:	---
CORRECTIVE ACTION WORKLOAD:	N - NO
GENERATOR STATUS:	N

NAIC INFORMATION

ENFORCEMENT INFORMATION:

VIOLATION INFORMATION:

HAZARDOUS WASTE INFORMATION:

Corrosive waste
The following spent halogenated solvents used in degreasing: Tetrachloroethylene, trichlorethylene, methylene chloride, 1,1,1-trichloroethane, carbon tetrachloride and chlorinated fluorocarbons; all spent solvent mixtures/b1

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

SPILLS			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME: ADDRESS: SOUTH LIBERTY and VILLA ELGIN IL KANE CONTACT: BROWNSTONE DEVELOPMENT LLC SOURCE: IL EPA		REV: 2/20/10 ID1: 20060649 ID2: STATUS: PHONE:	
<u>SITE INFORMATION</u>			
POTENTIALLY RESPONSIBLE PARTY: BROWNSTONE DEVELOPMENT LLC			

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

SWL	
SEARCH ID: 81	DIST/DIR: NON GC
ELEVATION:	MAP ID:
NAME: ELGIN COMPOST FACILITY ADDRESS: UNKNOWN ELGIN IL 60123 KANE CONTACT: TISH POWELL SOURCE: IL EPA	REV: 11/1/08 ID1: 0894380047 ID2: STATUS: CLOSED PHONE: 847-931-5980
<u>SITE INFORMATION</u>	
FACILITY NAME: ELGIN COMPOST FACILITY STATUS: CLOSED HOURS OF OPER: N/A TIPPING FEE: WASTE ACCEPTED: N/A DATE OPEN: 09/12/91 DATE CLOSED: 11/07/01	PERM LANDFILL AREA/ACRES: HIGHEST PERM ELEV/FT: GW MON WELLS: YEARS REMAINING/EST:
DESIGN CAP/AIR SPACE/CU. YDS: PERM DISPOSAL AREA: LEACHATE MON STATIONS: METHANE COLL SYS: FACILITY ACREAGE:	
OWNER: CITY OF ELGIN 150 DEXTER COURT ELGIN IL 60120	
CONTACT: TISH POWELL PHONE: 847-931-5980	
OPERATOR: CITY OF ELGIN 150 DEXTER COURT ELGIN IL 60120	
CONTACT: TISH POWELL PHONE: 847-931-5980	

**Environmental FirstSearch
Site Detail Report**

Target Property:

ELGIN IL 60120

JOB: ELGIN-RR-TRACK

SWL

SEARCH ID: 82 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME: WOODLAND RECYCLING AND DISPOSAL FACILITY (CLOSED)	REV: 11/1/08
ADDRESS: UNKNOWN	ID1: 0894830005
SOUTH ELGIN IL 60120	ID2:
KANE	STATUS: CLOSED
CONTACT: MICHAEL PETERSON	PHONE: 262-253-8626 EXT. 11
SOURCE: IL EPA	

SITE INFORMATION

SITE INFORMATION

FACILITY NAME: INACTIVE IN 2002)	WOODLAND RDF LANDSCAPE WASTE TRANSFER AREA (SITE WILL MOVE IN SPRING 2002.
STATUS:	ACTIVE
HOURS OF OPER:	N/A
TIPPING FEE:	N/A
WASTE ACCEPTED:	LANDSCAPE WASTE ONLY
DATE OPEN:	1995
DATE CLOSED:	

DESIGN CAP/AIR SPACE/CU. YDS:		PERM LANDFILL AREA/ACRES:	
PERM DISPOSAL AREA:		HIGHEST PERM ELEV/FT:	
LEACHATE MON STATIONS:		GW MON WELLS:	
METHANE COLL SYS:		YEARS REMAINING/EST:	
FACILITY ACREAGE:	<1		

FACILITY NAME:	WOODLAND RECYCLING AND DISPOSAL FACILITY (CLOSED)
STATUS:	CLOSED
HOURS OF OPER:	N/A
TIPPING FEE:	N/A
WASTE ACCEPTED:	N/A
DATE OPEN:	12/31/76
DATE CLOSED:	11/05/02

DESIGN CAP/AIR SPACE/CU. YDS:	13,000,000	PERM LANDFILL AREA/ACRES:	213
PERM DISPOSAL AREA:	103	HIGHEST PERM ELEV/FT:	852
LEACHATE MON STATIONS:	4	GW MON WELLS:	24
METHANE COLL SYS:	GAS-TO-ENERGY	YEARS REMAINING/EST:	0
FACILITY ACREAGE:			

OWNER: WASTE MANAGEMENT OF ILLINOIS INC
N 96 W 13600 COUNTY LINE RD
GERMANTOWN WI 53022

CONTACT: MICHAEL PETERSON
PHONE: 262-253-8626 Ext. 115

OPERATOR: WASTE MANAGEMENT OF ILLINOIS INC
N 96 W 13600 COUNTY LINE RD
GERMANTOWN WI 53022

CONTACT: MICHAEL PETERSON
PHONE: 262-253-8626 Ext. 115

OWNER: OAK BROOK BANK, TRUST 8-1735
1400 W 16TH ST
OAK BROOK IL 60521

- Continued on next page -

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

SWL

SEARCH ID: 82 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME:	WOODLAND RECYCLING AND DISPOSAL FACILITY (CLOSED)	REV:	11/1/08
ADDRESS:	UNKNOWN	ID1:	0894830005
	SOUTH ELGIN IL 60120	ID2:	
	KANE	STATUS:	CLOSED
CONTACT:	MICHAEL PETERSON	PHONE:	262-253-8626 EXT. 11
SOURCE:	IL EPA		

CONTACT: KATHY BLUMENTHAL OR ADRIANA RODEK
PHONE: 630-571-1050

OPERATOR: WASTE MANAGEMENT OF ILLINOIS INC
1031 E. FABYAN PARKWAY
BATAVIA IL 60510

CONTACT: DOUG HOPKINS
PHONE: 630-232-7664

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST

SEARCH ID: 88 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME:	PET-AG INC	REV:	7/12/10
ADDRESS:	30 W432 ROUTE 20	ID1:	2035465
	ELGIN IL 60120	ID2:	
	KANE	STATUS:	EXEMPT
CONTACT:		PHONE:	
SOURCE:	IL FMO		

SITE INFORMATION

TOTAL NUMBER OF TANKS: 1

OWNER: PET-AG INC
30W432 ROUTE 20
ELGIN IL 60120

FACILITY TYPE: COMMERCIAL / RETAIL
GREEN TAG DECAL:
GREEN TAG ISSUED:
GREEN TAG EXPIRED:
SSP INSPECT DATE:
SSP EXPIRATION DATE:

TANK INFORMATION:

TANK NUMBER:	1	CAPACITY:	300
SUBSTANCE:	HEATING OIL	STATUS:	EXEMPT FROM REGISTRATION
LAST USED:	12/1/1973	RED TAG:	
OSFM FIRST NOTIFIED:	2/11/1997		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST			
SEARCH ID: 83	DIST/DIR: NON GC	ELEVATION:	MAP ID:
NAME: LAKE ST BOOSTER STATION		REV: 6/17/08	
ADDRESS: 31 W 124 RT 20		ID1: 8000785	
ELGIN IL		ID2:	
COOK		STATUS: MERGED	
CONTACT: 2613 INS		PHONE:	
SOURCE: IL FMO			
<u>SITE INFORMATION</u>			
OWNER:	VILLAGE OF BARTLETT		
	BARTLETT IL 60103		
1998 DECAL:	NONE		
ENFORCE ORDER:	N		
FEES OWED:			
<u>PERMIT NUMBER</u>	<u>EXPIRATION DATE</u>		
<u>TANK INFORMATION:</u>			

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST			
SEARCH ID: 84	DIST/DIR: NON GC	ELEVATION:	MAP ID:
NAME: SOUTH ELGIN MARATHON		REV: 7/12/10	
ADDRESS: MIDDLE ST/ROUTE 25		ID1: 2042666	
SOUTH ELGIN IL 60120		ID2:	
KANE		STATUS: ACTIVE	
CONTACT:		PHONE:	
SOURCE: IL FMO			
<u>SITE INFORMATION</u>			
TOTAL NUMBER OF TANKS:	3		
OWNER:	PRAIRIE STATE ENTERPRISES OF DARIEN 215 SOUTH NORTHWEST HIGHWAY, SUITE 201B BARRINGTON IL 600102655		
FACILITY TYPE:	SELF-SERVICE STATION		
GREEN TAG DECAL:	L000061		
GREEN TAG ISSUED:	2/9/2010		
GREEN TAG EXPIRED:	12/31/2012		
SSP INSPECT DATE:	4/14/2010		
<u>TANK INFORMATION:</u>			
TANK NUMBER:	1	CAPACITY:	20000
SUBSTANCE:	GASOLINE	STATUS:	CURRENTLY IN USE
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	7/27/2005		
TANK NUMBER:	2	CAPACITY:	12000
SUBSTANCE:	DIESEL FUEL	STATUS:	CURRENTLY IN USE
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	7/27/2005		
TANK NUMBER:	3	CAPACITY:	6000
SUBSTANCE:	GASOLINE	STATUS:	CURRENTLY IN USE
LAST USED:		RED TAG:	
OSFM FIRST NOTIFIED:	7/27/2005		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME: WASTE TREATMENT PLANT FOX RIVER WRD		REV: 7/12/10	
ADDRESS: DANA AND STATE ST ELGIN IL 60121 COOK		ID1: 2040582	
CONTACT:		ID2:	
SOURCE: IL FMO		STATUS: EXEMPT	
		PHONE:	
<u>SITE INFORMATION</u>			
TOTAL NUMBER OF TANKS:	1		
OWNER:	FOX RIVER WATER RECLAMATION DISTRICT P.O. BOX 328 RAYMOND STREET and PURIFY DRIVE ELGIN IL 60121		
FACILITY TYPE:	NONE		
GREEN TAG DECAL:			
GREEN TAG ISSUED:			
GREEN TAG EXPIRED:			
SSP INSPECT DATE:			
SSP EXPIRATION DATE:			
<u>TANK INFORMATION:</u>			
TANK NUMBER:	1	CAPACITY:	500
SUBSTANCE:	HEATING OIL	STATUS:	EXEMPT FROM REGISTRATION
LAST USED:	12/30/1973	RED TAG:	
OSFM FIRST NOTIFIED:			

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST			
SEARCH ID:	DIST/DIR:	ELEVATION:	MAP ID:
NAME: ELGIN PLANT ADDRESS: RTE 31 PO BOX SOUTH ELGIN IL 60177 KANE CONTACT: SOURCE: IL FMO	DIST/DIR: NON GC	ELEVATION:	MAP ID:
		REV: 7/12/10 ID1: 2018527 ID2: STATUS: CLOSED PHONE:	
<u>SITE INFORMATION</u>			
TOTAL NUMBER OF TANKS:	6		
OWNER:	CENTRAL BLACKTOP CO INC 6301 S EAST AVE PO BOX 2080 LA GRANGE IL 605258180		
FACILITY TYPE:	INDUSTRIAL / MANUFACTURING		
GREEN TAG DECAL:			
GREEN TAG ISSUED:			
GREEN TAG EXPIRED:			
SSP INSPECT DATE:			
SSP EXPIRATION DATE:			
<u>TANK INFORMATION:</u>			
TANK NUMBER:	1	CAPACITY:	8000
SUBSTANCE:	DIESEL FUEL	STATUS:	REMOVED
LAST USED:	8/5/1991	RED TAG:	
OSFM FIRST NOTIFIED:	2/10/1986		
TANK NUMBER:	2	CAPACITY:	8000
SUBSTANCE:	DIESEL FUEL	STATUS:	REMOVED
LAST USED:	8/5/1991	RED TAG:	
OSFM FIRST NOTIFIED:	2/10/1986		
TANK NUMBER:	3	CAPACITY:	3500
SUBSTANCE:	DIESEL FUEL	STATUS:	REMOVED
LAST USED:	8/5/1991	RED TAG:	
OSFM FIRST NOTIFIED:	2/10/1986		
TANK NUMBER:	4	CAPACITY:	3500
SUBSTANCE:	DIESEL FUEL	STATUS:	REMOVED
LAST USED:	8/5/1991	RED TAG:	
OSFM FIRST NOTIFIED:	2/10/1986		
TANK NUMBER:	5	CAPACITY:	3500
SUBSTANCE:	DIESEL FUEL	STATUS:	REMOVED
LAST USED:	8/5/1991	RED TAG:	
OSFM FIRST NOTIFIED:	2/10/1986		
TANK NUMBER:	7	CAPACITY:	1000
SUBSTANCE:	USED OIL	STATUS:	REMOVED
LAST USED:	8/5/1991	RED TAG:	
OSFM FIRST NOTIFIED:	4/4/1986		

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST

SEARCH ID: 87 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME: CHICAGO GRAVEL-ELGIN PIT	REV: 7/12/10
ADDRESS: RT 25	ID1: 2002874
ELGIN IL 60121	ID2:
KANE	STATUS: CLOSED
CONTACT:	PHONE:
SOURCE: IL FMO	

SITE INFORMATION

TOTAL NUMBER OF TANKS: 1

OWNER: CHICAGO GRAVEL CO
343 S DEARBORN ST
CHICAGO IL 60604

FACILITY TYPE: INDUSTRIAL / MANUFACTURING

GREEN TAG DECAL:
GREEN TAG ISSUED:
GREEN TAG EXPIRED:
SSP INSPECT DATE:
SSP EXPIRATION DATE:

TANK INFORMATION:

TANK NUMBER: 1	CAPACITY: 500
SUBSTANCE: GASOLINE	STATUS: REMOVED
LAST USED:	RED TAG:
OSFM FIRST NOTIFIED: 3/6/1986	

**Environmental FirstSearch
Site Detail Report**

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

UST

SEARCH ID: 86 **DIST/DIR:** NON GC **ELEVATION:** **MAP ID:**

NAME: ACE COFFEE BAR, INC.	REV: 7/12/10
ADDRESS: 30 W 626 ROUTE 20	ID1: 2002895
ELGIN IL 60120	ID2:
KANE	STATUS: CLOSED
CONTACT:	PHONE:
SOURCE: IL FMO	

SITE INFORMATION

TOTAL NUMBER OF TANKS: 2

OWNER: ACE COFFEE BAR, INC.
601 E. LAKE STREET
STREAMWOOD IL 60107

FACILITY TYPE: INDUSTRIAL / MANUFACTURING
GREEN TAG DECAL: D000617
GREEN TAG ISSUED: 5/30/2002
GREEN TAG EXPIRED: 12/31/2005
SSP INSPECT DATE:
S

TANK INFORMATION:

TANK NUMBER: 1	CAPACITY: 10000
SUBSTANCE: GASOLINE	STATUS: REMOVED
LAST USED: 1/1/2005	RED TAG:
OSFM FIRST NOTIFIED: 3/6/1986	

TANK NUMBER: 2	CAPACITY: 10000
SUBSTANCE: DIESEL FUEL	STATUS: REMOVED
LAST USED: 1/1/2005	RED TAG:
OSFM FIRST NOTIFIED: 3/6/1986	

Environmental FirstSearch Descriptions

NPL: EPA NATIONAL PRIORITY LIST - The National Priorities List is a list of the worst hazardous waste sites that have been identified by Superfund. Sites are only put on the list after they have been scored using the Hazard Ranking System (HRS), and have been subjected to public comment. Any site on the NPL is eligible for cleanup using Superfund Trust money.

A Superfund site is any land in the United States that has been contaminated by hazardous waste and identified by the Environmental Protection Agency (EPA) as a candidate for cleanup because it poses a risk to human health and/or the environment.

FINAL - Currently on the Final NPL

PROPOSED - Proposed for NPL

NPL DELISTED: EPA NATIONAL PRIORITY LIST Subset - Database of delisted NPL sites. The National Oil and Hazardous Substances Pollution Contingency Plan (NCP) establishes the criteria that the EPA uses to delete sites from the NPL. In accordance with 40 CFR 300.425.(e), sites may be deleted from the NPL where no further response is appropriate.

DELISTED - Deleted from the Final NPL

CERCLIS: EPA COMPREHENSIVE ENVIRONMENTAL RESPONSE COMPENSATION AND LIABILITY INFORMATION SYSTEM (CERCLIS)- CERCLIS is a database of potential and confirmed hazardous waste sites at which the EPA Superfund program has some involvement. It contains sites that are either proposed to be or are on the National Priorities List (NPL) as well as sites that are in the screening and assessment phase for possible inclusion on the NPL.

PART OF NPL- Site is part of NPL site

DELETED - Deleted from the Final NPL

FINAL - Currently on the Final NPL

NOT PROPOSED - Not on the NPL

NOT VALID - Not Valid Site or Incident

PROPOSED - Proposed for NPL

REMOVED - Removed from Proposed NPL

SCAN PLAN - Pre-proposal Site

WITHDRAWN - Withdrawn

NFRAP: EPA COMPREHENSIVE ENVIRONMENTAL RESPONSE COMPENSATION AND LIABILITY INFORMATION SYSTEM ARCHIVED SITES - database of Archive designated CERCLA sites that, to the best of EPA's knowledge, assessment has been completed and has determined no further steps will be taken to list this site on the National Priorities List (NPL). This decision does not necessarily mean that there is no hazard associated with a given site; it only means that, based upon available information, the location is not judged to be a potential NPL site.

NFRAP - No Further Remedial Action Plan

P - Site is part of NPL site

D - Deleted from the Final NPL

F - Currently on the Final NPL

N - Not on the NPL

O - Not Valid Site or Incident

P - Proposed for NPL

R - Removed from Proposed NPL

S - Pre-proposal Site

W - Withdrawn

RCRA COR ACT: EPA RESOURCE CONSERVATION AND RECOVERY INFORMATION SYSTEM SITES - Database of hazardous waste information contained in the Resource Conservation and Recovery Act Information (RCRAInfo), a national program management and inventory system about hazardous waste handlers. In general, all generators, transporters, treaters, storers, and disposers of hazardous waste are required to provide information about their activities to state environmental agencies. These agencies, in turn pass on the information to regional and national EPA offices. This regulation is governed by the Resource Conservation and Recovery Act (RCRA), as amended by the Hazardous and Solid Waste Amendments of

1984.

RCRAInfo facilities that have reported violations and subject to corrective actions.

RCRA TSD: EPA RESOURCE CONSERVATION AND RECOVERY INFORMATION SYSTEM TREATMENT, STORAGE, and DISPOSAL FACILITIES. - Database of hazardous waste information contained in the Resource Conservation and Recovery Act Information (RCRAInfo), a national program management and inventory system about hazardous waste handlers. In general, all generators, transporters, treaters, storers, and disposers of hazardous waste are required to provide information about their activities to state environmental agencies. These agencies, in turn pass on the information to regional and national EPA offices. This regulation is governed by the Resource Conservation and Recovery Act (RCRA), as amended by the Hazardous and Solid Waste Amendments of 1984.

Facilities that treat, store, dispose, or incinerate hazardous waste.

RCRA GEN: EPAMA DEP/CT DEP RESOURCE CONSERVATION AND RECOVERY INFORMATION SYSTEM GENERATORS - Database of hazardous waste information contained in the Resource Conservation and Recovery Act Information (RCRAInfo), a national program management and inventory system about hazardous waste handlers. In general, all generators, transporters, treaters, storers, and disposers of hazardous waste are required to provide information about their activities to state environmental agencies. These agencies, in turn pass on the information to regional and national EPA offices. This regulation is governed by the Resource Conservation and Recovery Act (RCRA), as amended by the Hazardous and Solid Waste Amendments of 1984.

Facilities that generate or transport hazardous waste or meet other RCRA requirements.

LGN - Large Quantity Generators

SGN - Small Quantity Generators

VGN - Conditionally Exempt Generator.

Included are RAATS (RCRA Administrative Action Tracking System) and CMEL (Compliance Monitoring & Enforcement List) facilities.

CONNECTICUT HAZARDOUS WASTE MANIFEST - Database of all shipments of hazardous waste within, into or from Connecticut. The data includes date of shipment, transporter and TSD info, and material shipped and quantity. This data is appended to the details of existing generator records.

MASSACHUSETTES HAZARDOUS WASTE GENERATOR - database of generators that are regulated under the MA DEP.

VQN-MA = generates less than 220 pounds or 27 gallons per month of hazardous waste or waste oil.

SQN-MA = generates 220 to 2,200 pounds or 27 to 270 gallons per month of waste oil.

LQG-MA = generates greater than 2,200 lbs of hazardous waste or waste oil per month.

RCRA NLR: EPA RESOURCE CONSERVATION AND RECOVERY INFORMATION SYSTEM SITES - Database of hazardous waste information contained in the Resource Conservation and Recovery Act Information (RCRAInfo), a national program management and inventory system about hazardous waste handlers. In general, all generators, transporters, treaters, storers, and disposers of hazardous waste are required to provide information about their activities to state environmental agencies. These agencies, in turn pass on the information to regional and national EPA offices. This regulation is governed by the Resource Conservation and Recovery Act (RCRA), as amended by the Hazardous and Solid Waste Amendments of 1984.

Facilities not currently classified by the EPA but are still included in the RCRAInfo database. Reasons for non classification:

Failure to report in a timely matter.

No longer in business.

No longer in business at the listed address.

No longer generating hazardous waste materials in quantities which require reporting.

ERNS: EPA/NRC EMERGENCY RESPONSE NOTIFICATION SYSTEM (ERNS) - Database of incidents reported to the National Response Center. These incidents include chemical spills, accidents involving chemicals (such as fires or explosions), oil spills, transportation accidents that involve oil or chemicals, releases of radioactive materials, sightings of oil sheens on bodies of water, terrorist incidents involving chemicals, incidents where illegally dumped chemicals have been found, and drills intended to prepare responders to handle these kinds of incidents. Data since January 2001 has been received from the National Response System database as the EPA no longer maintains this data.

Tribal Lands: *DOI/BIA* INDIAN LANDS OF THE UNITED STATES - Database of areas with boundaries established by treaty, statute, and (or) executive or court order, recognized by the Federal Government as territory in which American Indian tribes have primary governmental authority. The Indian Lands of the United States map layer shows areas of 640 acres or more, administered by the Bureau of Indian Affairs. Included are Federally-administered lands within a reservation which may or may not be considered part of the reservation.

BUREAU OF INDIAN AFFIARS CONTACT - Regional contact information for the Bureau of Indian Affairs offices.

State/Tribal SWL: *IL EPA* ANNUAL LANDFILL CAPACITY REPORT - database of sanitary landfills available disposal capacity.

State/Tribal LUST: *IL EPA* LEAKING UNDERGROUND STORAGE TANK INCIDENT TRACKING DATABASE - database of incidents reported to the Illinois Emergency Management Agency and the Illinois Environmental Protection Agency.

State/Tribal UST/AST: *IL FMO* STATEWIDE UNDERGROUND STORAGE TANK LISTING - database of underground storage tanks. The data includes tank, contact and enforcement information.

State/Tribal EC: *IL EPA* SITE REMEDIATION PROGRAM DATABASE SUBSET- database of all voluntary remediation projects administered through the Pre-Notice Site Cleanup Program (1989 to 1995) and the Site Remediation Program (1996 to the present). These sites are included in this database only if they has an engineering control placed upon them.

State/Tribal IC: *IL EPA* SITE REMEDIATION PROGRAM DATABASE SUBSET- database of all voluntary remediation projects administered through the Pre-Notice Site Cleanup Program (1989 to 1995) and the Site Remediation Program (1996 to the present). These sites are included in this database only if they has an institutional control placed upon them.

State/Tribal VCP: *IL EPA* SITE REMEDIATION PROGRAM DATABASE - database of all voluntary remediation projects administered through the Pre-Notice Site Cleanup Program (1989 to 1995) and the Site Remediation Program (1996 to the present).

RADON: *NTIS* NATIONAL RADON DATABASE - EPA radon data from 1990-1991 national radon project collected for a variety of zip codes across the United States.

DOCKET: *EPA* INTERGRATED COMPLIANCE INFORMATION SYSTEM (ICIS) - database of federal administrative and federal judicial cases under the following environmental statutes: the Clean Air Act (CAA), the Clean Water Act (CWA), the Resource Conservation and Recovery Act (RCRA), the Emergency Planning and Community Right-to-Know Act (EPCRA) Section 313, the Toxic Substances Control Act (TSCA), the Federal Insecticide, Fungicide, and Rodenticide Act (FIFRA), the Comprehensive Environmental Response, Compensation, and Liability Act (CERCLA or Superfund), the Safe Drinking Water Act (SDWA), and the Marine Protection, Research, and Sanctuaries Act (MPRSA).

Federal Other: *EPA* SECTION SEVEN TRACKING SYSTEM (SSTS) - database of registration and production data for facilities which manufacture pesticides.

VAPOR INTRUSION DATABASE - database that records the migration of volatile chemicals from the subsurface into overlying buildings. Volatile chemicals in contaminated soil or groundwater can emit vapors that may migrate through soil and into indoor air spaces.

State Other: *US DOJ* NATIONAL CLANDESTINE LABORATORY REGISTER - Database of addresses of some locations where law enforcement agencies reported they found chemicals or other items that indicated the presence of either clandestine drug laboratories or dumpsites. In most cases, the source of the entries is not the U.S. Department of Justice ("the Department"), and the Department has not verified the entry and does not guarantee its accuracy. All sites that are included in this data set will have an id that starts with NCLR.

State Other: *IL DPH* METHAMPHETAMINE LABORATORY - Database of illegal drug laboratories.

Environmental FirstSearch
Street Name Report for Streets within .5 Mile(s) of Target Property

Target Property: ELGIN IL 60120

JOB: ELGIN-RR-TRACK

Street Name	Dist/Dir	Street Name	Dist/Dir
Adams St	0.46 NW		
Arlington Ave	0.49 NE		
Ashland Ave	0.33 NW		
Bent St	0.50 NE		
Bluff City Blvd	0.16 SE		
Central Rd	0.45 NW		
Dixon Ave	0.26 SE		
Dwight St	0.13 NE		
East Rd	0.45 NW		
Elgin Ave	0.21 SE		
Elgin Blvd	0.28 SE		
Elizabeth St	0.30 SE		
Grace St	0.22 NE		
Hammond Ave	0.36 SE		
Harding St	0.45 NW		
Hastings St	0.18 NE		
Hendee St	0.27 NW		
Illinois Ave	0.49 NE		
Jay St	0.27 NE		
Kirkland Rd	0.42 NW		
Lessenden Pl	0.42 NE		
Lord St	0.28 NW		
May St	0.36 NE		
Morgan St	0.49 NW		
Purify Dr	0.27 SE		
Raymond St	0.16 SE		
Riley St	0.35 NW		
Robey St	0.22 NW		
Russell St	0.32 NE		
Ryerson Ave	0.38 NW		
S Liberty St	0.43 NE		
S State St	0.42 NW		
Souster Ave	0.18 NW		
St Charles St	0.36 NE		
State Route 25	0.36 NE		
State Route 31	0.42 NW		
Times Sq	0.50 NE		
United States Highwa	0.05 NE		
Wellington Ave	0.18 NE		
Willis St	0.23 NW		

CERCLIS Search Results Envirofacts US EPA

CERCLIS EPA ID	Facility Information	SITE NAME	ADDRESS	COUNTY	SITE NUMBER	FEDERAL FACILITY	NPL STATUS	CORPORATE LINK	MAP PIN INFO	RECORD OF DECISION (ROD) INFO	EPA REGION ALLIANCE	LATITUDE/LONGITUDE	OWNERSHIP
ILD025444837	View Facility Information	ELGIN SALVAGE & SUPPLY CO	464 MCBRIDE STELGIN, IL 60120	KANE	1600	N	Not on the NPL	No	MAP	No	No	Latitude : 42.04644Longitude: -88.28896	Code/ Descriptions
ILD005071410	View Facility Information	WOODRUFF & EDWARDS	SW1/4 SEC11 T41N R83ELGIN, IL 60120	KANE	1600	N	Not on the NPL	No	MAP	No	No	Latitude : 42.038276Longitude: -88.28845	Code/ Descriptions

0123&postal_code=60123&add_search_type=Beginning+With&city_name=&county_name=&state_code=&program_search=1&report=1&page_no=1&output_sql_switch=TRUE&database_type=CERCLIS



Envirofacts Search Results

 CERCLIS



Consolidated facility information (from multiple EPA systems) was searched to select facilities

[<< Return](#)

Search Parameters: ZIP Code: 60123

Results are based on data extracted on NOV-13-2013

No Results found.

Total Number of Facilities Retrieved: 0

CERCLIS Links

- [Overview](#)
- [Search](#)
- [Model](#)
- [Law](#)
- [CERCLIS Search User Guide](#)
- [Contact Us](#)
- [Superfund Home](#)

[Report an Error](#)

Incident Number	Incident Report Date	Street Address of Incident Location	Incident Location City	Date/Time Occurred	Name
H-2011-0958	09/06/2011	853 Dundee Ave	Elgin		Lubricating Oil
H-2011-0957	09/06/2011	853 Dundee Ave	Elgin		Heating Oil
H-2011-0731	07/07/2011	3883 Kings Mill Dr	Elgin		Hydraulic Fluid
H-2011-0690	06/28/2011	853 Dundee Ave	Elgin		Heating Oil
H-2011-0580	06/03/2011	1375 Spaulding Rd	Elgin	2011-06-03 11:26	tires
H-2011-0121	02/11/2011	St Charles	Elgin	2011-02-11 14:45	Carbon Monoxide
H-2011-0121	02/11/2011	St Charles	Elgin	2011-02-11 14:45	Carbon Monoxide
H-2011-0121	02/11/2011	St Charles	Elgin	2011-02-11 14:45	Carbon Monoxide
H-2011-0121	02/11/2011	St Charles	Elgin	2011-02-11 14:45	Carbon Monoxide
H-2011-0121	02/11/2011	St Charles	Elgin	2011-02-11 14:45	Carbon Monoxide
H-2011-0121	02/11/2011	St Charles	Elgin	2011-02-11 14:45	Carbon Monoxide
H-2011-0121	02/11/2011	St Charles	Elgin	2011-02-11 14:45	Carbon Monoxide
H-2011-0121	02/11/2011	St Charles	Elgin	2011-02-11 14:45	Carbon Monoxide
H-2011-0121	02/11/2011	St Charles	Elgin	2011-02-11 14:45	Carbon Monoxide
H-2011-0121	02/11/2011	St Charles	Elgin	2011-02-11 14:45	Carbon Monoxide
H-2011-0121	02/11/2011	St Charles	Elgin	2011-02-11 14:45	Carbon Monoxide
H-2011-0121	02/11/2011	St Charles	Elgin	2011-02-11 14:45	Carbon Monoxide
H-2009-0763	07/16/2009	25 between the Canadian National RR and Gilbert St	Elgin	2009-07-14 18:30:00	engine oil
H-2009-0763	07/16/2009		8 Elgin	2009-07-14 18:30:00	engine oil
H-2009-0763	07/16/2009	25 between the Canadian National RR and Gilbert St	Elgin	2009-07-14 18:30:00	engine oil
H-2009-0763	07/16/2009	25 between the Canadian National RR and Gilbert St	Elgin	2009-07-14 18:30:00	engine oil
H-2009-0685	06/24/2009	3N 8833 Walt Witman Lane	Elgin		mineral oil from t
H-2009-0685	06/24/2009	3N 8833 Walt Witman Lane	Elgin		mineral oil from t
H-2008-1741	12/10/2008	1337 1341 Duneee Ave	Elgin		unknown, possibl
H-2008-1535	10/15/2008	Intersection lake	Elgin		Diesel Fuel and f
H-2008-1381	09/14/2008	Willard St between Chicago St	Elgin		Miscellaneous Pe
H-2008-1360	09/10/2008	24 Chicago Ave	Elgin		former heating oi
H-2008-1116	07/24/2008	2455 South Street	Elgin		Unleaded Gasolir
H-2008-0393	03/25/2008	Highland Ave	Elgin		Unknown Petrole
H-2008-0247	02/22/2008	1001 Randell rd	Elgin		Diesel Fuel
H-2008-0094	01/21/2008	1925 Holmes	Elgin		multiple chemica
H-2007-1382	10/15/2007		Elgin		Diesel fuel
H-2007-1156	08/21/2007		Elgin		Unknown Substai
H-2007-1035	07/28/2007		Elgin		Diesel Fuel

H-2007-0938	07/10/2007 350 2nd St	Elgin		Hydraulic Fluid
H-2007-0793	06/09/2007 1156 Dundee	Elgin		Diesel, Possible t
H-2007-0781	06/06/2007 Elgin Joliet & Eastern Railroad near New	Elgin		Unidentified Yellc
H-2007-0686	05/21/2007 Randall Rd	Elgin		Diesel Fuel
H-2007-0584	05/04/2007 1124 Bluff City Blvd	Elgin		Diesel
H-2007-0411	04/03/2007 1500 Holmes Rd	Elgin		Oil and Coolant
H-2007-0281	03/09/2007 8 635 Corrin Road	Elgin		Diesel Fuel
H-2007-0010	01/04/2007 162 Grove Ave	Elgin		Gasoline
H-2006-1613	12/22/2006 1156 Dundee Ave	Elgin		unleaded gasolin
H-2006-1403	11/07/2006 825 Tollgate Rd	Elgin		Rubbing Alcohol
H-2006-1370	11/03/2006 51 State St	Elgin		Fuel oil
H-2006-1292	10/18/2006 1005 Liberty St	ELGIN		Diesel fuel
H-2006-1269	10/13/2006 1389 Dundee Rd	Elgin		heating oil
H-2006-1256	10/09/2006 934 Center St	Elgin		Diesel Fuel
H-2006-1054	08/19/2006 90 EB	ELGIN		DIESEL
H-2006-0992	08/04/2006 162 Grove Ave	Elgin		Hydrolic Fluid
H-2006-0766	06/21/2006	ELGIN		DIESEL FUEL
H-2006-0721	06/13/2006 72 AND RANDEL RD	ELGIN		FLOOR STAINER
H-2006-0649	05/30/2006 CORNER OF SOUTH LIBERTY	ELGIN		HEATING OIL
H-2006-0541	05/06/2006	ELGIN		DIESEL FUEL
H-2006-0515	05/01/2006 464 MCBRIDE	ELGIN		HEATING OIL
H 2005 1578	11/22/2005 816 ST	ELGIN	11/22/2005 @ 14:00	HYDRAULIC OIL
H 2005 1430	10/14/2005 2583 TECHNOLOGY DR	ELGIN	10/14/05 @ 1908	FORANE 507
H 2005 1390	10/04/2005 1616 BERKELEY	ELGIN	10/04/2005 @ 07:15	TRICHLOROETH
H 2005 1287	09/13/2005 1032 LARKIN AVENUE	ELGIN		GASOLINE
H 2005 0933	07/07/2005 464 MCBRIDE ST	ELGIN	Unknown @	DIESEL, GASOL
H 2005 0701	05/20/2005 100 SYMPHONY WAY	ELGIN	05/20/2005 @	31% HYDROCHL
H 2005 0220	02/10/2005 1585 TODD FARM DR	ELGIN	2/10/05 @ 0900	7 % IODINE, 88'
H 2005 0251	02/10/2005 1585 TODD FARM DR	ELGIN	2/10/2005 @ 0845	ISOPROPYL ALC
H 2005 0213	02/09/2005 1585 TODD FARM ROAD	ELGIN	Unknown @	ISPROPANOL/7%
H 2005 0031	01/09/2005	ELGIN		PROPANE
H 2004 1381	10/04/2004 1570 BIG	ELGIN		GASOLINE AND
H 2004 1331	09/23/2004 1165 JANSEN FARM COURT	ELGIN		DIESEL FUEL
H 2004 1312	09/17/2004 215 SPRING STREET	ELGIN		BENZOPYRENE
H 2004 1072	07/30/2004 965 MCLEAN BLVD	ELGIN		GASOLINE
H 2004 0937	07/04/2004 20 AT RANDAL RD	ELGIN		KEROSENE
H 2004 0317	03/09/2004	ELGIN	03/09/2004 @ Unknown	HYDRAULIC OIL

H 2003 1881	12/29/2003 585 STATE ST	ELGIN		GASOLINE
H 2003 1763	12/01/2003 20 STATE ST	ELGIN	12/01/03 @ 08:00	DIESEL FUEL
H 2003 1701	11/19/2003 206 OR 210 SOUTH GROVE STREET	ELGIN		GASOLINE
H 2003 1438	09/30/2003 960 MCLEAN BLVD	ELGIN		GASOLINE
H 2003 1359	09/14/2003 970 CHICAGO ST	ELGIN	09/14/2003 @ 22:19	POTASSIUM GC
H 2003 1361	09/14/2003 970 CHICAGO ST	ELGIN	09/14/03 @ 22:00	SOLDER MIXTU
H 2002 1686	11/20/2002 304 GROVE AVENUE	ELGIN		GASOLINE
H 2002 1578	10/30/2002 231 DOUGLAS AVE	ELGIN	Unknown @	UNK POSSIBLE
H 2002 1579	10/30/2002 304 GROVE AVE	ELGIN		USED OIL
H 2002 1191	08/21/2002 375 RIVER ROAD	ELGIN	08/21/2002 @ 10:00	SERRIC SULFAT
H 2002 0905	06/24/2002 1441 TIMBER DR	ELGIN	06/24/02 @ 18:30	TAMOL-SM AND
H 2002 0644	05/09/2002 1040 CHICAGO ST	ELGIN	05/09/2002 @ 10:30	DIESEL
H 2002 0626	05/07/2002 1171 JANSEN FARM COURT	ELGIN		DIESEL FUEL
H 2002 0242	02/20/2002 1347 GASKET DRIVE	ELGIN	02/20/02 @ 23:30	ETHYL ACETATI
H 2002 0188	02/08/2002	ELGIN	02/08/2002 @ 10:45	SOME TYPE PE'
H 2002 0174	02/04/2002 840 CHURCH ROAD	ELGIN	02/04/2002 @ 13:00	COPPER TETRA
H 2002 0068	01/12/2002	ELGIN	01/12/2002 @ 13:30	HYDRAULIC OIL
H 2001 1520	09/11/2001 335 LOCUST ST	ELGIN		HEATING OIL
H 2001 1494	09/05/2001 344 ST CHARLES STREET	ELGIN	Unknown @	SUSPECTED W/
H 2001 1323	08/03/2001 31 DANA STREETS	ELGIN		HEATING OIL
H 2001 1078	06/21/2001 255 CHICAGO STREET	ELGIN	06/21/2001 @ 11:30	COOLANT
H 2001 0826	05/14/2001 150 DEXTER	ELGIN	05/14/2001 @ 09:45	HEATING OIL
H 2001 0463	03/21/2001 8 OF NW PARKWAY	ELGIN	03/21/2001 @ 06:30	DIESEL FUEL
H 2001 0401	03/09/2001 1156 dundee avenue	ELGIN		KEROSENE
H 2001 0227	03/06/2001 464 McBRIDE STREET	ELGIN		WASTE OIL
H 2001 0330	02/23/2001 573 CRYSTAL STREET	ELGIN	Unknown @	DIESEL FUEL
H 2001 0306	02/21/2001 35 ANN ST	ELGIN		DIESEL FUEL
H 2001 0272	02/16/2001 1156 DUNDEE AVE	ELGIN		KEROSENE
H 2001 0195	01/31/2001 1313 TIMBER DR	ELGIN	Unknown @	DIESEL FUEL
H 2000 2343	12/11/2000 740 TOLLGATE ROAD	ELGIN	12/11/2000 @ 21:30	N-METHYL-2-PY
H 2000 2061	10/27/2000	ELGIN	10/27/2000 @ 14:15	SOY BEAN OIL
H 2000 1990	10/17/2000 1219 LARKIN AVENUE	ELGIN		GASOLINE
H 2000 1817	09/24/2000 450 AIRPORT ROAD	ELGIN	09/24/2000 @ 20:00	WINDSHIELD W
H 2000 1767	09/16/2000 338 MCLEAN BLVD	ELGIN	09/16/00 @ 06:00	GASOLINE
H 2000 1769	09/16/2000 20 MCLEAN ST	ELGIN	09/16/2000 @ 06:00	GASOLINE
H 2000 1552	08/14/2000 1240 CHARLES ST	ELGIN	08/14/2000 @ 2301	SUPER BECKAM
H 2000 1376	07/18/2000 72 KIMBALL STREET	ELGIN		PETROLEUM PF

H 2000 1389	07/11/2000 CORNER OF GALVIN DR	ELGIN		DIESEL FUEL
H 2000 0937	04/06/2000 620 WING ST	ELGIN		BENZENE (HEA
20000605	04/05/2000 1001 DUNDEE ROAD	ELGIN		GASOLINE/HEA
20000361	03/03/2000 1156 DUNDEE	ELGIN		KEROSENE
20000360	03/03/2000 771 WALNUT	ELGIN		GASOLINE
20000174	01/31/2000	ELGIN		DIESEL FUEL
20000162	01/28/2000 1001 SUMITT ST	ELGIN		HEATING OIL
992787	12/17/1999 1605 DUNDEE LANE	ELGIN	12/17/99 1530	DIESEL FUEL
992694	12/07/1999 39 455 BOWES ROAD	ELGIN		GASOLINE
992565	11/16/1999 50 KIMBALL ST	ELGIN		GASOLINE
992177	09/20/1999 595 STATE STREET	ELGIN		GASOLINE & DIE
991931	08/17/1999 202 MOUNTIAN ST	ELGIN		GASOLINE
991639	07/08/1999 1151 STATE ST	ELGIN	UNK	DIESEL FUEL
991536	06/24/1999 901 RAYMOND ST	ELGIN		DIESEL FUEL
991251	05/25/1999 710 CHICAGO STREET	ELGIN		GASOLINE, USE
991256	05/25/1999	ELGIN		DIESEL FUEL
991036	04/27/1999 1428 EAGLE ROAD	ELGIN		GASOLINE/DIES
991025	04/26/1999 363 BLUFF CITY BLVD	ELGIN		36 HEATING OIL
990987	04/21/1999 450 2ND ST	ELGIN		DIESEL
990983	04/20/1999 1524 DAVIS ROAD	ELGIN		INK OIL
990788	04/01/1999 1 MI	ELGIN	04/01/99 1000	ANHYDROUS AP
990625	03/16/1999 313 DUNDEE AVENUE	ELGIN		PETROLEUM
990528	03/05/1999 9175 FOX LANE	ELGIN		SOLVENT BLEN
990517	03/03/1999 31 BOUND	ELGIN	03/03/99 1500	ALITHATIC AER
990491	03/02/1999 1441 TIMBER DR	ELGIN	03/02/99 0815	DIESEL FUEL
990463	02/25/1999 345 WILLARD AVE	ELGIN		GASOLINE & FU
990227	02/02/1999 1570 BIG	ELGIN		WASTE OIL
990187	01/28/1999 RAYMOND ST	ELGIN		DIESEL
990054	01/12/1999 371 WILLARD AVE	ELGIN		USED OIL
983019	12/09/1998 355 HENDEE ST	ELGIN		DIESEL FUEL
982890	11/21/1998 90 EB TOLL PLAZA	ELGIN	11/21/98 0100	DIESEL FUEL
982856	11/18/1998 466 RENNER DRIVE	ELGIN		GASOLINE
982840	11/17/1998 945 BLUFF CITY BLVD	ELGIN		GASOLINE
982838	11/17/1998 1969 SPARTAN DRIVE	ELGIN		GASOLINE
982834	11/16/1998 1730 BERKEY ST	ELGIN		UNLEADED GAS
982831	11/16/1998 1010 WING ST	ELGIN		GASOLINE
982827	11/16/1998 573 CRYSTAL	ELGIN		GASOLISNE/DIE

982741	11/02/1998 1611 VILLA STREET	ELGIN		DIESEL FUEL
982406	09/28/1998 269 275 DUPAGE ST	ELGIN		HEATING OIL
982295	09/16/1998 1601 VILLA STREET	ELGIN		DIESEL FUEL
982142	08/27/1998 647 LAUREL ST	ELGIN		GASOLINE
982084	08/21/1998 280 PARK LANE	ELGIN		GASOLINE
982001	08/13/1998 740 POLLGATE	ELGIN		MOBILTHERM 6
981958	08/07/1998 1219 LARKIN AVE	ELGIN		GASOLINE
981699	07/13/1998 750 STATE STREET	ELGIN		DIESEL FUEL
981344	06/05/1998 750 STATE	ELGIN		DIESEL FUEL
981282	05/29/1998 1156 DUNDEE AVE	ELGIN		GASOLINE/DIES
981142	05/15/1998 60 ANN ST	ELGIN		DIESEL FUEL
981115	05/13/1998 440 AIRPORT RD FOX RIVER	ELGIN		POSSIBLE DIES
980964	04/28/1998 401 DAVIS ROAD	ELGIN		GASOLINE
980817	04/13/1998 180 KIMBALL ST	ELGIN		FUEL OIL & WA
980763	04/07/1998 STATE	ELGIN	04/07/98 1600	POTASSIUM HY
980708	04/01/1998 975 STATE	ELGIN		ANTIFREEZE
980615	03/21/1998 90 PLAZA	ELGIN	03/21/98 0901	XYLENE
980604	03/20/1998 419 ST CHARLES RD	ELGIN		DIESEL FUEL
980496	03/05/1998 1100 BRANDT DRIVE	ELGIN		WASTE OIL/DIE
980459	03/03/1998 35 BROOKSIDE DRIVE	ELGIN		DIESEL FUEL
980238	02/03/1998 965 MCLEAN BLVD	ELGIN		GASOLINE
980195	01/28/1998 300 AIRPORT ROAD	ELGIN		HEATING OIL
980168	01/23/1998 740 TOLLGATE ROAD	ELGIN		TOULENE/HEPT
980025	01/05/1998 1395 TIMBER DRIVE	ELGIN		GASOLINE
972366	12/11/1997 3 MI	ELGIN		APPEARS TO BI
972365	12/11/1997 77 RIVERSIDE DRIVE	ELGIN		APPEARS TO BI
972319	12/04/1997 771 WALNUT AVENUE	ELGIN		HEATING OIL
972089	10/31/1997 850 DAVIS ROAD	ELGIN		DIESEL FUEL
971616	09/01/1997 HIGHLAND	ELGIN		UNKNOWN PET
971372	07/29/1997 1470 ABBOTT STREET	ELGIN		GASOLINE, DIE
971290	07/18/1997 1450 BOWEN RD	ELGIN	UNKNOWN	DIESEL
971286	07/17/1997 236 DUNDEE AVENUE	ELGIN		GASOLINE, HEA
971260	07/14/1997 1400 ABBOTT DR	ELGIN		HYDRAULIC FLL
971197	07/03/1997 36 TYLER CREEK PLAZA	ELGIN	07/03/97 1501	HYPO-CHLORIT
970831	05/12/1997 30 TWP 4	ELGIN		CRUDE OIL
970828	05/12/1997 740 TOLLGATE ROAD	ELGIN		METHYLENE CF
970638	04/15/1997 222 DOUGLAS AVE	ELGIN		HEATING OIL

970543	03/28/1997	816 CHARLES ROAD	ELGIN		BENZENE
970064	01/10/1997	845 CHICAGO STREET	ELGIN		GASOLINE
962386	12/27/1996	1050 ABBOTT DRIVE	ELGIN	UNKNOWN	DIESEL
962383	12/27/1996	470 DUNDEE AVE	ELGIN	UNKNOWN	GASOLINE
962338	12/19/1996		ELGIN		GASOLINE
962165	11/20/1996	740 TOLLGATE RD	ELGIN	11/20/96 UNK	TOLUENE
962075	11/07/1996	425 AIRPORT ROAD	ELGIN		DIESEL FUEL
962044	11/01/1996	1156 DUNDEE ROAD	ELGIN		UNLEADED GAS
961958	10/22/1996	771 WALNUT AVE	ELGIN	UNK	UNLEADED GAS
961875	10/10/1996	520 MCBRIDE ST	ELGIN		GASOLINE
961867	10/09/1996	502 GROVE AVE	ELGIN	UNKNOWN	LEADED GASOL
961862	10/08/1996	222 DOUGLAS AVE	ELGIN		GASOLINE
961840	10/06/1996	435 AIRPORT ROAD	ELGIN		SUSPECT ACET
961746	09/20/1996	1450 MCLEAN BLVD	ELGIN	UNK	DRUG ENEMA B
961572	08/29/1996	3100 GOLF ROAD	ELGIN		DIESEL FUEL
961378	08/01/1996	120 STATE STREET	ELGIN	07/02/96 1350	SULFURIC ACID
961317	07/23/1996	1045 CHICAGO ST	ELGIN	07/23/96 0945	MOTOR OIL -US
961280	07/17/1996	1200 CHICAGO STREET	ELGIN		USED MOTOR C
961242	07/12/1996	1435 HOLMES ROAD	ELGIN		NICKEL PLATIN
961167	06/28/1996	775 LAUREL STREET	ELGIN		DIESEL FUEL
961082	06/17/1996	1156 DUNDEE RD	ELGIN	UNK	UNLEADED GAS
960924	05/28/1996	816 ST CHARLES ST	ELGIN		GASOLINE
960739	05/03/1996	155 TIMBER DRV	ELGIN		DIESEL FUEL
960672	04/25/1996	470 DUNDE AVENUE	ELGIN		WASTE OIL
960506	03/29/1996	725 MCLEEN BLVD	ELGIN	UNK	DIESEL FUEL
960412	03/13/1996	366 WILLARD AVE	ELGIN		DIESEL FUEL
960311	02/23/1996	551 MCLEAN BLVD	ELGIN	UNK	UNLEADED GAS
960262	02/14/1996	571 COOPER AVENUE	ELGIN		GASOLINE
952462	12/06/1995	1500 VILLA	ELGIN		150/PETROLEUM
952446	12/04/1995	1580 LARKIN AVENUE	ELGIN		UNLEADED GAS
952397	11/25/1995	990 CHICAGO STREET	ELGIN	11/25/95 1000	NUMEROUS CH
952341	11/15/1995	350 2ND STREET	ELGIN		GASOLINE AND
952317	11/10/1995	1500 VILLA	ELGIN		PAINT WASTE
952082	10/06/1995	1919 BIG TIMBER ROAD	ELGIN	UNK	DIESEL FUEL
951877	09/07/1995	934 CENTER STREET	ELGIN	09/07/95 1500	WETSPO IN LIQ
951723	08/13/1995	1500 VILLA STREET	ELGIN		WATER/OIL MIX
951446	07/06/1995	777 BIG TIMBER ROAD	ELGIN		R-22HCFC/FREC

951400	06/28/1995	1500 VILLA	ELGIN		CLEAN MINERAL
951250	06/12/1995	1500 VILLA	ELGIN	06/12/95 1620	WASTE ETHANOL
950479	03/09/1995	1333 TIMBER DR	ELGIN		GASOLINE
950409	02/25/1995	210 GROVE AVE	ELGIN	UNK	UNK TYPE PETR
950319	02/15/1995	25 NEAR	ELGIN	02/14/95 AM	DIESEL FUEL
942794	12/13/1994	630 CONGDON	ELGIN		HEATING OIL
942793	12/13/1994	1103 DUNDEE AVENUE	ELGIN		HEATING OIL
942767	12/08/1994	1124 BLUFF CITY BLVD	ELGIN	12/08/94 1610	KEROSENE
942766	12/08/1994	268 STATE STREET	ELGIN		GASOLINE & #2
942623	11/18/1994	1385 CHICAGO STREET	ELGIN		UNLEADED GAS
942343	10/18/1994	3100 GOLF RD	ELGIN		UNLEADED GAS
942139	09/20/1994	50 KIMBAL	ELGIN		DIESEL
942043	09/08/1994	1500 VILLA ST	ELGIN		WASTE OIL
941983	08/31/1994	601 VILLA	ELGIN		FUEL OIL
941963	08/29/1994	1005 LIBERTY	ELGIN		GASOLINE
941917	08/24/1994	1122 ST CHARLES STREET	ELGIN	08/23/94 2200	TOLUENE DIISC
941884	08/19/1994	450 AIRPORT ROAD	ELGIN		UNKNOWN
941864	08/17/1994	300 STATE STREET	ELGIN		FUEL OIL
941066	05/12/1994	380 BELMONT	ELGIN		SUSP. GEAR LU
941019	05/05/1994	1500 VILLA ST	ELGIN		R.Q. WASTE, FL
940865	04/20/1994	2075 LARKIN AVE	ELGIN		DIESEL
940763	04/12/1994		ELGIN	UNKNOWN	OIL PRODUCT
940720	04/06/1994	1500 VILLA STREET	ELGIN		SAFETY KLEEN
940678	03/30/1994	20 WB	ELGIN	03/30/94 0915	HYDRAULIC FLL
940634	03/25/1994	235 GROVE AVENUE	ELGIN	03/25/94 0900	WASTE OIL
940491	03/08/1994	1500 VILLA ST	ELGIN	03/08/94 1500	MINERAL SPIRI
940443	03/02/1994	922 DUNDEE AVE	ELGIN		GASOLINE
940421	02/25/1994		ELGIN		DIESEL FUEL &
940326	02/15/1994	1500 VILLA ST	ELGIN		CLEAN 105 MINI
940281	02/08/1994	1500 VILLA RD	ELGIN		PETROLEUM N/
940182	01/25/1994	1400 TOASTMASTER DRIVE	ELGIN	01/25/94 1300	MINERAL OIL
940168	01/24/1994	1500 VILLA ROAD	ELGIN		HAZARDOUS W
940032	01/05/1994	152 SOUTH GROVE	ELGIN	01/04/94 1500	SUSPECT WAS
940022	01/04/1994	750 STATE	ELGIN		SUSPECT PETR
933256	12/21/1993	1500 VILLA	ELGIN	12/20/93 1900	MINERAL SPIRI
933235	12/17/1993	1500 VILLA	ELGIN	12/17/93 1615	MINERAL SPIRI
933234	12/17/1993	1500 VILLA	ELGIN	12/17/93 1530	MINERAL SPIRI

933185	12/13/1993	1500 VILLA ST	ELGIN		LIQUID PETROL
933183	12/13/1993	1500 VILLA ST	ELGIN		IMMERSION CLI
932883	11/03/1993	31 RIVER RD	ELGIN		PETROLEUM PF
932834	10/27/1993	955 TOLLGATE ROAD	ELGIN	10/27/93 0200	DIESEL FUEL
932714	10/13/1993	900 STATE STREET	ELGIN		#6 FUEL OIL
932704	10/12/1993	1500 VILLA STREET	ELGIN	10/12/93 1045	DIRTY MINERAL
932610	09/30/1993	1500 VILLA	ELGIN		SPENT MINERA
932598	09/28/1993	1500 VILLA ST	ELGIN		DIRTY IMMERSC
932566	09/24/1993	280 GROVE AVENUE	ELGIN		PETROLEUM PF
932529	09/21/1993	CLIFFORD	ELGIN		GASOLINE
932527	09/21/1993	5 WALNUT AVE	ELGIN		#2 FUEL OIL
932386	09/04/1993	1500 VILLA	ELGIN		DIESEL FUEL
932343	08/31/1993	31 I	ELGIN		GASOLINE
932187	08/16/1993	1500 VILLA ST	ELGIN		IMMERSION CLI
932134	08/10/1993	901 DUNDEE AVENUE	ELGIN		GASOLINE
931949	07/21/1993	1313 TIMBER DRIVE	ELGIN		DIESEL FUEL
931885	07/15/1993	15N 482 N RTE	ELGIN		DIESEL FUEL
931867	07/13/1993	1025 MCLEAN BLVD	ELGIN		GASOLINE
931698	06/24/1993	250 PARK LANE	ELGIN		GASOLINE
931526	06/08/1993	1020 BLUFF CITY BLVD	ELGIN		GASOLINE
931349	05/21/1993	220 N SPRING	ELGIN		GASOLINE
931155	05/05/1993	1601 VILLA ST	ELGIN	05/05/93 2210	CHLOROFORM
931105	05/01/1993	1640 MAPLE	ELGIN		DIESEL
931075	04/29/1993	515 N GROVE	ELGIN		UNK TYPE OF C
931019	04/23/1993	210 SOUTH GROVE	ELGIN		PETROLEUM PF
930813	04/02/1993	1580 LARKEN AVENUE	ELGIN		WASTE OIL/FUE
930703	03/24/1993	640 VILLA STREET	ELGIN		GASOLINE
923563	12/15/1992	1500 VILLA	ELGIN		MINERAL SPIRIT
923542	12/14/1992	600 VARSITY DRIVE	ELGIN		MOTOR OIL SU
923521	12/11/1992	1601 VILLA STREET	ELGIN		SICO ORANGE I
923431	12/02/1992	1400 ABBOTT DRIVE	ELGIN		ETHANOL & ISO
923309	11/20/1992	1480 ILLINOIS PARKWAY	ELGIN		GASOLINE/DIES
923199	11/12/1992	805 STATE ST	ELGIN		HYDRAULIC OIL
922846	10/09/1992	1725 LARKIN AVENUE	ELGIN		SUSPECT HEAT
922689	09/24/1992	305 RAMONA	ELGIN		POSS. GASOLIN
922518	09/09/1992	223 DUNDEE AVE	ELGIN		GASOLINE, HEA
922444	09/02/1992	945 CHICAGO ST	ELGIN		GASOLINE

922375	08/27/1992	1500 VILLA STREET	ELGIN		WASTE PERCHI
922319	08/24/1992	90 TOLL PLAZA	ELGIN		HYDROCHLORIK
922209	08/12/1992	1400 ABBOTT DRIVE	ELGIN	08/12/92 1030	NORMAL PROP'
922011	07/24/1992	2 SLADE AVE	ELGIN		GASOLINE/DIES
921966	07/21/1992	783 HIGHLAND AVE	ELGIN		FUEL OIL
921897	07/15/1992	229 N GROVE	ELGIN		SUSPECTED TA
921885	07/14/1992	150 DEXTER COURT	ELGIN		DIESEL FUEL
921604	06/12/1992	573 CRYSTAL	ELGIN		DIESEL
921480	06/02/1992	INTS OF MCLEAN BLVD	ELGIN		GASOLINE
921414	05/27/1992	1200 ST CHARLES RD	ELGIN		HEATING OIL
920880	04/06/1992	152 SPRING ST	ELGIN		PETROLEUM
920660	03/12/1992		ELGIN		DIESEL FUEL
920455	02/19/1992	100 OAKWOOD BLVD	ELGIN		FUEL OIL
920294	01/31/1992	LAKE ST	ELGIN	01/31/92 1230	DIESEL FUEL
920205	01/23/1992	90 TOLLWAY	ELGIN		GASOLINE
920207	01/23/1992	90 TOLLWAY	ELGIN		GASOLINE
920155	01/17/1992	1580 LARKIN AVENUE	ELGIN		GASOLINE
920030	01/06/1992	2ND	ELGIN		FLAMMABLE LIC
913774	12/31/1991	710 CHICAGO	ELGIN	UNK	LEADED & UNLE
913687	12/20/1991	206 S GROVE	ELGIN		GASOLINE
913512	12/05/1991	1 BOX	ELGIN		DIESEL
913496	12/04/1991	350 2ND	ELGIN		DIESEL
913447	11/26/1991	470 DUNDEE	ELGIN		GASOLINE
913084	10/29/1991	939 MCLEAN	ELGIN		UNK.
912807	10/02/1991	1560 ILLINOIS PARKWAY	ELGIN	10/02/91 1200	ASPHALT AND I
912689	09/21/1991	425 AIRPORT ROAD	ELGIN		SUSPECTED PE
912590	09/12/1991	710 CHICAGO STREET	ELGIN		GASOLINE
912575	09/11/1991	1500 HOLMES ROAD	ELGIN		RATAK MSN-15
912336	08/20/1991	23 NEAR	ELGIN		ETHANOL
912206	08/08/1991		ELGIN	08/08/91 0715	DIESEL
912200	08/07/1991	305 MCLEAN	ELGIN		UNLEADED GAS
912120	08/01/1991		ELGIN		PCB OIL
912058	07/26/1991	909 CHICAGO STREET	ELGIN		GASOLINE
911817	07/03/1991	RANDALL RD	ELGIN	07/03/91 0711	4 DIFFERENT CI
911791	06/28/1991	750 STATE ST	ELGIN		SULFURIC ACID
911573	06/11/1991	1137 GUNDY RD	ELGIN		GASOLINE
911455	05/30/1991	740 POLLGATE RD	ELGIN		ETHYL ACETATI

911444	05/30/1991	25 OF	ELGIN		GASOLINE
911044	04/18/1991	RIVER RD	ELGIN		UNK.
911011	04/15/1991	1570 LARKIN	ELGIN		WASTE OIL & M
910703	03/19/1991	1171 JANSEN FARM COURT	ELGIN		DIESEL FUEL
910495	02/26/1991	1570 BIG TIMBER RD	ELGIN		NO LEAD GASO
910004	01/02/1991	1570 BIG	ELGIN		GASOLINE
903763	12/20/1990	255 CHICAGO ST	ELGIN		DIESEL FUEL
903209	10/31/1990	1520 BIG	ELGIN		GASOLINE
903181	10/29/1990	SPAULDING RD	ELGIN		2-4D, (HERBICID
903018	10/13/1990	POPLAR CREEK	ELGIN		UNKNOWN HYD
902801	09/26/1990	49 AIRPORT ROAD	ELGIN	PRIOR TO12/88	PETROLEUM PF
902759	09/24/1990	1140 CHICAGO	ELGIN		WASTE OIL
902665	09/17/1990	771 WALNUT AVE	ELGIN	09/13/90 1030	PREMIUM NO LI
902607	09/11/1990	896 BLUFF CITY BLVD	ELGIN		PAINT REMOVE
902534	09/03/1990	1500 VILLA	ELGIN	09/02/90 2330	WASTE OIL
902525	08/31/1990	174 S GROVE	ELGIN		FUEL OIL
902433	08/23/1990	464 MCBRIDE	ELGIN		PROPANE
902338	08/15/1990	STATE	ELGIN	08/15/90 0820	GASOLINE
902251	08/08/1990	SPEULDING	ELGIN	08/08/90 0834	PCB
902051	07/23/1990	425 RENNER DR	ELGIN		UNLEADED GAS
902027	07/20/1990	1640 LAFOX ST	ELGIN	07/18/90 1000	SODIUM HYDRIC
901934	07/12/1990	1455 MAIN LANE	ELGIN		GASOLINE
901695	06/22/1990	1570 BIG TIMBER AVE	ELGIN	06/21/90 2000	UNLEADED GAS
901440	05/30/1990		ELGIN		PCB OIL
901412	05/25/1990	836 WALNUT AVE	ELGIN		GASOLINE
901374	05/22/1990	390 SADLER RD	ELGIN		GASOLINE
901137	04/27/1990	GOLF RD	ELGIN		GASOLINE
901087	04/24/1990		ELGIN	04/24/90 1330	POSSIBLY GAS
901028	04/18/1990	450 2ND ST	ELGIN	04/18/90 0800	OCTYLAMINE
900871	04/04/1990	725 CHICAGO ST	ELGIN		GASOLINE
900724	03/19/1990	1500 HOLMES RD	ELGIN		FUEL OIL
900535	02/26/1990	771 WALNUT ST	ELGIN		GASOLINE
900437	02/16/1990	1156 DUNDEE AVE	ELGIN		SUSPECT GASC
900193	01/20/1990	507 STATE ST	ELGIN		UNLEADED GAS
900127	01/15/1990	1500 VILLA ST	ELGIN	01/15/90 0300	WASTE WATER
900071	01/09/1990	1601 VILLA	ELGIN	01/09/90 0800	DIRTY MINERAL
900013	01/03/1990	450 2ND ST	ELGIN	12/30/89 1500	#2 DIESEL FUEL

892532	12/06/1989 1500 Villa	ELGIN		CHLORINATED
892002	10/11/1989 1325 South Street	ELGIN		GASOLINE AND
891755	09/12/1989 771 Walnut Avenue	ELGIN	09/11/89 P.M.	GASOLINE
891451	08/05/1989 36 Tyler Creek Plaza	ELGIN		SODIUM HYPOC
891438	08/04/1989 1091 Davis Road	ELGIN	7/21/1989	PCB
891435	08/04/1989 970 Villa	ELGIN		WASTE OIL
891371	07/28/1989 1500 Villa Street	ELGIN	07/28/89 1030	WASTE MINERA
891277	07/18/1989 240 Clifton Avenue	ELGIN		#5 HEATING OIL
891217	07/12/1989 750 State Street	ELGIN		GASOLINE
891165	07/07/1989 Willard	ELGIN	07/07/89 0745	CLEAN MINERA
891078	06/22/1989 31 Davis Road	ELGIN	06/22/89 1030	SOME KIND OF
891002	06/14/1989 1600 Dundee Avenue	ELGIN		UNLEADED GAS
890825	05/22/1989 1010 Chicago Street	ELGIN		GASOLINE
890647	04/24/1989 1156 Dundee	ELGIN		GASOLINE
890646	04/24/1989 1570 Big Timber Lane	ELGIN		GASOLINE
890325	03/03/1989 1171 Jansen Farm Court	ELGIN	03/03/89 0820	#2 DIESEL FUEL
890026	01/05/1989 1500 Villa Street	ELGIN		IMMERSION CLI
881677	12/17/1988 888 Villa Street	ELGIN	12/17/88 1445	GAS & WATER I
880474	04/20/1988 Jefferson	ELGIN	04/20/88 0900	UNKNOWN
880245	03/04/1988 1463 Eagle Road	ELGIN		ELEMENTEL ME
880227	02/29/1988	ELGIN		SUSPECT:NAPT
880174	02/16/1988	ELGIN		MISC. CHEMICA
880170	02/16/1988 1500 Villa Street	ELGIN	02/16/88 1215	MINERAL SPIRI
871462	09/01/1987 Big Timber Road	ELGIN		HYDROXY METH
870552	04/15/1987	ELGIN	04/14/87 1831	PCB
870197	02/09/1987 1500 Villa	ELGIN	1600	TRI-CHLORO ET

LUST Sites

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BL_ID	NAME	STREET	CITY	STATE	ZIP	RTK_DTM
0894385074	School District U-46	647 Laurel Street	Elgin	IL	60120	5/28/2014
0894385795	McDonald's Corporation	401 Summit Street	Elgin		60120	9/12/2012
0894385017	Lake Superior Band of Chippewa I	853 Dundee Avenue	Elgin	IL	60120	12/1/2011
0894385017	Lake Superior Band of Chippewa I	853 Dundee Avenue	Elgin	IL	60120	12/1/2011
0894383013	Griffith Enterprises	220 North Spring	Elgin	IL	60120	6/15/2011
0894385188	Clark Oil & Refining	901 Dundee Ave.	Elgin	IL	60120	2/24/2010
0894385166	Chicago Jr. School	1600 Dundee	Elgin	IL	60120	12/27/2007
0894385213	Marvi, Kaizar	1005 North Liberty	Elgin	IL	60120	7/5/2007
0894385724	Elgin, City of	162 South Grove Avenue	Elgin		60120	6/25/2007
0894385466	Shell Oil Products US	1389 Dundee Road	Elgin	IL	60120	1/22/2007
0894385599	Zagone, George Estate of	72 Kimball St.	Elgin	IL	60120	12/12/2006
0894385710	Brownstone Development LLC	NE Corner of South Liberty & Villa Stree	Elgin		60120	8/1/2006
0894385131	Shell Oil Products US	1032 Larkin Avenue	Elgin	IL	60120	4/21/2006
0894385587	Arc Disposal	7 North 540 Rt. 25	Elgin	IL	60120	3/23/2006
0894385070	Checker Gas Station	851 St. Charles St.	Elgin	IL	60120	3/9/2006
0894385178	Amoco Oil Co. #19564	470 Dundee Ave.	Elgin	IL	60120	12/29/2005
0894385173	Bertrand & Cochran	710 East Chicago St.	Elgin	IL	60120	12/29/2005
0314385020	Currie Motors	909 East Chicago St.	Elgin	IL	60120	12/29/2005
0894380056	Amoco Oil Co. #15095	1137 Gundy Rd.	Elgin	IL	60120	12/29/2005
0894385147	Illinois Bell Telephone	255 East Chicago St.	Elgin	IL	60120	12/29/2005
0894380048	Venture Stores	49 Airport Rd.	Elgin	IL	60120	12/29/2005
0894385084	Fox Valley Nissan	1040 East Chicago St.	Elgin	IL	60120	12/29/2005
0894380046	Elgin, City of	174 South Grove	Elgin	IL	60120	12/29/2005
0894380044	Vencosky Oil Co.	1455 Effingham Ln.	Elgin	IL	60120	12/29/2005
0894385163	Brady Ready Mix	Rt. 25 South of	Elgin	IL	60120	12/29/2005
0894380039	Jeff's Mobil	836 Walnut Ave.	Elgin	IL	60120	12/29/2005
0314385039	Norwood Transportation Inc.	Rt. 1, Box 96	Elgin	IL	60120	12/29/2005
0894380046	Elgin, City of	206 South Grove	Elgin	IL	60120	12/29/2005
0894380046	Elgin, City of	240 South Grove	Elgin	IL	60120	12/29/2005
0894385173	John's Amoco	710 East Chicago St.	Elgin	IL	60120	12/29/2005

LUST Sites

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BL_ID	NAME	STREET	CITY	STATE	ZIP	RTK_DTM
0894385182	Uno-ven	1580 Larkin Ave.	Elgin	IL	60120	12/29/2005
0314380005	Elgin, City of	1100 Oak Wood Blvd., Lords Park Pavilli	Elgin	IL	60120	12/29/2005
0894385201	Eby-Brown Co.	152 North Spring St.	Elgin	IL	60120	12/29/2005
0894385125	Lee Wards Creative Crafts	1200 St. Charles Rd.	Elgin	IL	60120	12/29/2005
0894385067	Elgin Dept. of Transportation	573 North Crystal	Elgin	IL	60120	12/29/2005
0894385185	Elgin, City of	150 Dexter Ct.	Elgin	IL	60120	12/29/2005
0894385039	Commonwealth Edison Elgin Dist.	350 East 2nd St.	Elgin	IL	60120	12/29/2005
0894385805	Ace Coffee Bar	30 West 626 Route 20 (20 Lake Street)	Elgin		60120	12/29/2005
0894385095	Speedway SuperAmerica	1156 Dundee	Elgin	IL	60120	12/29/2005
0894385594	Wendy's Int'l, Inc.	1001 Dundee Ave.	Elgin	IL	60120	12/29/2005
0894383007	Elgin Salvage	464 McBride St.	Elgin	IL	60120	12/29/2005
0894385095	Speedway SuperAmerica	1156 Dundee Ave.	Elgin	IL	60120	12/29/2005
0894385190	Elgin, City of	35 Ann St.	Elgin	IL	60120	12/29/2005
0894385095	Speedway SuperAmerica	1156 Dundee Ave.	Elgin	IL	60120	12/29/2005
0894385633	Gail Borden Public Library	304 North Grove Avenue	Elgin		60120	12/29/2005
0894385633	Elgin, City of	304 North Grove Avenue	Elgin		60120	12/29/2005
0894385655	CVS Pharmacy	500 Dundee Ave.	Elgin	IL	60120	12/29/2005
0894385667	Elgin, City of	206-210 South Grove Street	Elgin		60120	12/29/2005
0314385475	East Automotive Service	640 Varsity Street	Elgin	IL	60120	12/29/2005
0894383007	Elgin Salvage	464 McBride Street	Elgin	IL	60120	12/29/2005
0314385008	Safety Kleen	390 Sadler Rd.	Elgin	IL	60120	12/29/2005
0894385055	Price Rite Liquors	180 Kimball St.	Elgin	IL	60120	12/29/2005
0894385182	Uno-ven	1580 Larkin Ave.	Elgin	IL	60120	12/29/2005
0894385170	Active Auto Sales	881 Villa St.	Elgin	IL	60120	12/29/2005
0894385095	Emro Marketing	1156 Dundee Ave.	Elgin	IL	60120	12/29/2005
0894380019	Larry Faul Chrysler Plymouth	1010 East Chicago St.	Elgin	IL	60120	12/29/2005
0894385182	Uno-ven	1580 Larkin Ave.	Elgin	IL	60120	12/29/2005
0894385153	School Dist. #6-46	240 South Clifton Ave.	Elgin	IL	60120	12/29/2005
0894385212	Elgin, City of	229 North Grove	Elgin	IL	60120	12/29/2005
0894385133	Illinois Bell Telephone	1325 South St.	Elgin	IL	60120	12/29/2005

BL_ID	NAME	STREET	CITY	STATE	ZIP	RTK_DTM
0894385154	Amoco Oil Co. #18869	507 North State St.	Elgin	IL	60120	12/29/2005
0894385095	Emro Unit #7756	1156 Dundee Ave.	Elgin	IL	60120	12/29/2005
0894385458	Z Investments LLC	215 North Spring Street	Elgin	IL	60120	12/29/2005
0314385024	Consolidated Freightways	1601 Villa St.	Elgin	IL	60120	12/29/2005
0894385095	Emro Marketing Co.	1156 Dundee Rd.	Elgin	IL	60120	12/29/2005
0314380001	Safety Kleen	1500 East Villa	Elgin	IL	60120	12/29/2005
0894385178	Amoco Oil Co.	470 Dundee Ave.	Elgin	IL	60120	12/29/2005
0314385018	Northwest Valley Dodge	845 East Chicago St.	Elgin	IL	60120	12/29/2005
0894385179	Amoco Oil Co.	816 South Charles Rd.	Elgin	IL	60120	12/29/2005
0894385513	Elgin, City of	222 Douglas Ave.	Elgin	IL	60120	12/29/2005
0894385532	Elgin, City of	236 Dundee Ave.	Elgin	IL	60120	12/29/2005
0894385551	L & J Riemer	35 Brookside Dr.	Elgin	IL	60120	12/29/2005
0894380032	Plote, Inc.	1100 Brandt Dr.	Elgin	IL	60120	12/29/2005
0894385055	Mowinski, Jerome	180 Kimball St.	Elgin	IL	60120	12/29/2005
0314385003	McGrath Buick	945 East Chicago St.	Elgin	IL	60120	12/29/2005
0894385095	Speedway SuperAmerica	1156 Dundee Ave.	Elgin	IL	60120	12/29/2005
0894385514	Seigles Home Builders	520 McBride St.	Elgin	IL	60120	12/29/2005
0894385565	Hill, Bonnie	269-275 DuPage St.	Elgin	IL	60120	12/29/2005
0314385079	Elgin Warehouse	1611 Villa St.	Elgin	IL	60120	12/29/2005
0894385067	Elgin, City of	573 North Crystal	Elgin	IL	60120	12/29/2005
0894385574	Elgin, City of	1010 Wing St.	Elgin	IL	60120	12/29/2005
0314385078	Elgin, City of	945 Bluff City Blvd.	Elgin	IL	60120	12/29/2005
0894385579	Graf & Sons, Inc.	371 Willard Ave.	Elgin	IL	60120	12/29/2005
0894385582	Elgin Car Wash	313 Dundee Ave.	Elgin	IL	60120	12/29/2005
0894380006	Fox Group II	363 Bluff City Blvd.	Elgin	IL	60120	12/29/2005
0894385586	Chicago Gravel Co.	Rt. 25 -- Facility #2-002874	Elgin	IL	60120	12/29/2005
0894385173	Ortkemp, Harry	710 East Chicago St.	Elgin	IL	60120	12/29/2005
0894385589	Fox Group II	901 Raymond St.	Elgin	IL	60120	12/29/2005
0894385553	Columbia Pipe & Supply Co.	60 Ann St.	Elgin	IL	60120	12/29/2005
0894380046	Elgin, City of	235 South Grove	Elgin	IL	60120	12/29/2005

BL_ID	NAME	STREET	CITY	STATE	ZIP	RTK_DTM
0314385040	Rasmussen Steel	305 Ramona	Elgin	IL	60120	12/29/2005
0314385085	Corner Stone Church	1001 Sumitt Street	Elgin	IL	60120	12/29/2005
0894380063	Radzinski, Bill	640 Villa St.	Elgin	IL	60120	12/29/2005
0894380046	Elgin, City of	210 South Grove	Elgin	IL	60120	12/29/2005
0894385445	Amoco Oil Co. #15607	Rt. 31 & I-90	Elgin	IL	60120	12/29/2005
0314380001	Safety Kleen	1500 East Villa	Elgin	IL	60120	12/29/2005
0894380046	Elgin, City of	280 South Grove	Elgin	IL	60120	12/29/2005
0894380046	Elgin, City of	152 South Grove	Elgin	IL	60120	12/29/2005
0894385451	Waste Management West	7 North 904 Rt. 25	Elgin	IL	60120	12/29/2005
0894383008	Continental Baking Co.	425 Airport Rd.	Elgin	IL	60120	12/29/2005
0894385452	922 Dundee Bldg. Corp.	922 Dundee Ave.	Elgin	IL	60120	12/29/2005
0894385515	Seigles Home Builders	502 Grove Ave..	Elgin	IL	60120	12/29/2005
0894385095	Emro Marketing	1156 Dundee Rd.	Elgin	IL	60120	12/29/2005
0894385513	Seigles Home Builders	222 Douglas Ave.	Elgin	IL	60120	12/29/2005
0314384055	Cook County Forest Preserve	3100 West Golf Rd.	Elgin	IL	60120	12/29/2005
0314385016	Ron Hopkins Ford	1045 East Chicago St.	Elgin	IL	60120	12/29/2005
0314385011	Ryan Enterprises Corp.	1200 East Chicago St.	Elgin	IL	60120	12/29/2005
0314384055	Cook County Forest Preserve	3100 West Golf Rd.	Elgin	IL	60120	12/29/2005
0894385178	Amoco Oil Co. #19564	470 Dundee Ave.	Elgin	IL	60120	12/29/2005
0314385062	Skully, Jack	366 Willard Ave.	Elgin	IL	60120	12/29/2005
0894385182	Uno-ven	1580 Larkin Ave.	Elgin	IL	60120	12/29/2005
0894385039	Commonwealth Edison Elgin Dist.	350 East 2nd St.	Elgin	IL	60120	12/29/2005
0894385487	Martin, DH	1333 Timber Dr.	Elgin	IL	60120	12/29/2005
0894385480	Ginsberg, Marian	630 Congdon	Elgin	IL	60120	12/29/2005
0894385071	Bigger's Chevy, Inc.	1385 East Chicago Street	Elgin	IL	60120	12/29/2005
0894383007	Elgin, City of	464 McBride Street	Elgin	IL	60120	
0894385698	Elgin, City of	1337-1341 Dundee Avenue	Elgin	IL	60120	
0894385017	Lake Superior Band of Chippewa I	853 Dundee Avenue	Elgin	IL	60120	
0894385745	Elgin, City of	24 East Chicago Avenue	Elgin		60120	
0894385131	True North Energy #2065	1032 Larkin Avenue	Elgin	IL	60120	

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BL_ID	NAME	STREET	CITY	STATE	ZIP	RTK_DTM
0894385108	Judson College	1151 North State Street	Elgin	IL	60123	3/8/2016
0894385669	Go-Tane Service Stations, Inc.	585 North State Street	Elgin		60123	7/21/2015
0894385669	Go-Tane Service Stations, Inc.	585 North State Street	Elgin		60123	7/21/2015
0894385137	Speedway LLC #7617	771 Walnut Ave.	Elgin	IL	60123	11/18/2014
0894385171	Larkin Mobil	1725 West Larkin Avenue	Elgin	IL	60123	4/11/2014
0894385171	Larkin Mobil	1725 West Larkin Avenue	Elgin	IL	60123	4/11/2014
0894385472	W.D. Oil Company	50 Kimball St.	Elgin	IL	60123	4/8/2014
0894385567	Larkin Amoco	1219 Larkin Ave.	Elgin	IL	60123	7/19/2013
0894385137	Speedway LLC #7617	771 Walnut Ave.	Elgin	IL	60123	3/27/2013
0894385137	Speedway LLC #7617	771 Walnut Avenue	Elgin	IL	60123	3/27/2013
0894385232	Elgin Paper Co.	1025 North McLean Blvd.	Elgin	IL	60123	6/20/2011
0894385660	Former Sun Oil Co. Service Statio	960 McLean Boulevard	Elgin		60123	11/23/2010
0894385140	Speedway SuperAmerica	1570 Big Timber	Elgin	IL	60123	10/1/2010
0894385140	Emro Marketing	1570 Big Timber Ln.	Elgin	IL	60123	10/1/2010
0894385140	Emro Marketing	1570 Big Timber Ln.	Elgin	IL	60123	10/1/2010
0894385140	Emro Marketing	1570 Big Timber Ln.	Elgin	IL	60123	10/1/2010
0894385140	Speedway SuperAmerica LLC	1570 Big Timber Lane	Elgin	IL	60123	9/28/2010
0894385214	T&L Property	1480 Illinois Pkwy.	Elgin	IL	60123	7/17/2008
0894385669	Go-Tane Go-Mart Convenience St	585 North State Street	Elgin		60123	4/2/2008
0894385578	Boncosky Oil Co.	355 Hendee St.	Elgin	IL	60123	3/7/2008
0894385550	Autonation USA	300 Airport Rd.	Elgin	IL	60123	7/17/2007
0894385549	McLean 76 Service	965 North McLean Blvd	Elgin	IL	60123	5/18/2007
0894385139	Elgin, City of - Building Maintenanc	51 North State Street	Elgin	IL	60123	3/21/2007
0894385455	Ted & Ed's Rental	2075 Larkin Ave.	Elgin	IL	60123	7/28/2006
0894385167	Suntory Water Group	1171 Jansen Farm Court	Elgin	IL	60123	6/23/2006
0894385167	Henkley & Schmidt	1171 Jansen Farm Ct.	Elgin	IL	60123	6/23/2006
0314385464	State & Walnut Quik Mart	300 South State St.	Elgin	IL	60123	3/9/2006
0894385159	Brittain Express Oil	1570 Larkin	Elgin	IL	60123	12/29/2005
0894385099	Elgin Industries, Inc.	620 Wing St.	Elgin	IL	60123	12/29/2005
0894385442	Eby-Brown, Inc.	1313 Timber Dr.	Elgin	IL	60123	12/29/2005

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BL_ID	NAME	STREET	CITY	STATE	ZIP	RTK_DTM
0894385067	Elgin, City of	573 North Crystal St.	Elgin	IL	60123	12/29/2005
0894385211	Ollman, Ralph & Sue	223 Dundee Ave.	Elgin	IL	60123	12/29/2005
0894380009	Printpack Inc.	1400 Abbott Dr.	Elgin	IL	60123	12/29/2005
0894385202	Elgin, City of	2 Slade Ave.	Elgin	IL	60123	12/29/2005
0894385204	Highland Ave. Church of Brethern	783 West Highland Ave.	Elgin	IL	60123	12/29/2005
0894385200	Thornton Sod Nursery	37 West 711 McDonald Rd.	Elgin	IL	60123	12/29/2005
0894385172	Bradley Mobil	725 West Chicago St.	Elgin	IL	60123	12/29/2005
0894385043	Adhesives Consultants	740 Tollgate Rd.	Elgin	IL	60123	12/29/2005
0894385115	Elgin Mental Health Ctr.	750 South State	Elgin	IL	60123	12/29/2005
0894385185	Elgin, City of	150 Dexter	Elgin	IL	60123	12/29/2005
0894385616	St. Edwards High School	335 Locust St.	Elgin	IL	60123	12/29/2005
0894385142	Amoco Oil Co. #15966	1520 Big Timber	Elgin	IL	60123	12/29/2005
0894385148	Parr Electric	425 Renner Dr.	Elgin	IL	60123	12/29/2005
0894380038	Illinois State Toll Hwy. Authority	I-90	Elgin	IL	60123	12/29/2005
0894380009	Printpack Inc.	1400 Abbott Dr.	Elgin	IL	60123	12/29/2005
0894385115	Elgin Mental Health Center	750 South State	Elgin	IL	60123	12/29/2005
0894385177	Hansen Shell	305 South McLean	Elgin	IL	60123	12/29/2005
0894385560	Lakeview Screw Machine Product	466 Renner Dr.	Elgin	IL	60123	12/29/2005
0894385446	Lewa Co.	5 West Walnut Ave.	Elgin	IL	60123	12/29/2005
0894385145	Pace Suburban Bus	975 South State	Elgin	IL	60123	12/29/2005
0894385557	Kane County Forest Preserve	401 Davis Rd.	Elgin	IL	60123	12/29/2005
0894385115	Elgin Mental Health Dept. Human	750 South State	Elgin	IL	60123	12/29/2005
0894385115	DHS Elgin Mental Health Ctr.	750 South State St.	Elgin	IL	60123	12/29/2005
0894385554	Willow Lake Estates	280 Park Ln.	Elgin	IL	60123	12/29/2005
0894385543	Fox Valley Bldg. Materials	1395 Timber Dr.	Elgin	IL	60123	12/29/2005
0894385575	Elgin, City of	1969 Spartan Dr.	Elgin	IL	60123	12/29/2005
0894385535	Justus Limited Partnership	850 Davis Rd.	Elgin	IL	60123	12/29/2005
0894385068	Universal Chemicals & Coatings	1975 Fox Ln.	Elgin	IL	60123	12/29/2005
0894385584	Flint Ink Corp.	1524 Davis Rd.	Elgin	IL	60123	12/29/2005
0894380022	Preston Trucking Co.	450 South 2nd St.	Elgin	IL	60123	12/29/2005

BL_ID	NAME	STREET	CITY	STATE	ZIP	RTK_DTM
0894385585	Burnidge Properties Ltd.	1428 Eagle Rd.	Elgin	IL	60123	12/29/2005
0894385079	CMS	595 South State St.	Elgin	IL	60123	12/29/2005
0894385591	NBC Development Corp.	39 West 455 Bowes Rd.	Elgin	IL	60123	12/29/2005
0894385144	Fox Valley Fire & Safety, Inc.	1730 Berkey St.	Elgin	IL	60123	12/29/2005
0894385496	Read Excavating Co.	1919 Big Timber Rd.	Elgin	IL	60123	12/29/2005
0314385238	Willow Lake Estates	250 Park Ln.	Elgin	IL	60123	12/29/2005
0894385442	Eby-Brown	1313 Timber Dr.	Elgin	IL	60123	12/29/2005
0894380005	CR Ind.	900 North State St.	Elgin	IL	60123	12/29/2005
0894385043	Henkel Adhesives	740 Tollgate Rd.	Elgin	IL	60123	12/29/2005
0894385479	Ginsberg, Marian	1103 Dundee Ave.	Elgin	IL	60123	12/29/2005
0314384048	Bluff City Investments	1020 Bluff City Blvd.	Elgin	IL	60123	12/29/2005
0894385509	Mobil Oil Corp.	551 North McLean Blvd.	Elgin	IL	60123	12/29/2005
0894385510	Georgia Pacific Corp.	115 Timber Rd.	Elgin	IL	60123	12/29/2005
0894380052	Van Den Bergh Foods Co.	775 Laurel St.	Elgin	IL	60123	12/29/2005
0894385043	Adhesive Consultants	740 Tollgate Rd.	Elgin	IL	60123	12/29/2005
0894385521	Schneider Excavating Inc.	38W645 Highland Ave.	Elgin	IL	60123	12/29/2005
0894385533	Elgin, City of	1450 Bowes Rd.	Elgin	IL	60123	12/29/2005
0894385534	Schuring Marital Trust	1470 Abbott St.	Elgin	IL	60123	12/29/2005
0894385475	Pit Stop	268 South State St.	Elgin	IL	60123	12/29/2005
0894385176	Elgin McLean Real Estate, LLC	338 South McLean Blvd.	Elgin	IL	60123	
0894385137	Speedway SuperAmerica	771 Walnut	Elgin	IL	60123	
0894385778	Elgin, City of	313 West Highland Avenue	Elgin		60123	

RCRAInfo Search Results Envirofacts US EPA

HANDLER ID	NAME	STREET	CITY	CO UNT Y	ST AT E	ZIP COD E	LATITUDE/L ONGITUDE
ILR000 053736	7 ELEVEN # 32236	811 E CHICAGO AVE	ELGIN	CO OK	IL	60120	42.03514/-88 .25983
ILR000 033365	777 CLEANERS	825 SUMMIT	ELGIN	CO OK	IL	60120	42.046238/-8 8.259974
ILD005 453832	ABRASIVE TECHNOLOGY	1280 BLUFF CITY BLVD	ELGIN	KAN E	IL	60120	42.01897/-88 .24138
ILD982 611758	AIRTRONICS	516 SLADE AVE	ELGIN	KAN E	IL	60120	42.0535/-88. 26913
ILD984 785642	ALL AMERICAN AUTO	953 VILLA	ELGIN	KAN E	IL	60120	42.0246/-88. 25408
ILD106 920903	ALL TRUCK INC	1601 VILLA ST	ELGIN	KAN E	IL	60120	42.016/-88.2 3252
ILR000 026864	ALLENTON LUMBER/ROTH JEROME	222 DOUGLAS AVE	ELGIN	KAN E	IL	60120	42.040987/-8 8.28397
ILR000 055335	ALLIED WASTE SERVICES	1330 GASKET DR	ELGIN	KAN E	IL	60120	41.997398/-8 8.241487
ILD984 922203	ALPHABET SHOP INC	300 E ELGIN AVE	ELGIN	KAN E	IL	60120	42.01663/-88 .27438
ILD059 484196	AMAX PLATING INC	667 N STATE	ELGIN	KAN E	IL	60120	42.04998/-88 .2949
ILD980 990881	AMAX PLATING INC	990 E CHICAGO ST	ELGIN	KAN E	IL	60120	42.03442/-88 .25363
ILR000 108977	AMERENENERGY MEDINA VALLEY COGEN LLC-ELGIN ENERGY CENTER	1559 GIFFORD RD	ELGIN	CO OK	IL	60120	41.99965/-88 .24493
ILD984 846246	AMERICAN DEMOLITION CORP	305 N RAMONA A	ELGIN	KAN E	IL	60120	42.02965/-88 .25392
ILD067 997288	AMERICHEM INC	1441 TIMBER DR	ELGIN	KAN E	IL	60120	42.057652/-8 8.31276
ILD984 811315	AMOCO 18869	507 N STATE AND WING	ELGIN	KAN E	IL	60120	42.04507/-88 .29338
IL0000 014712	AMOCO 19564	470 DUNDEE	ELGIN	KAN E	IL	60120	42.04612/-88 .27665
ILD984 813642	AMOCO OIL CO 15095	1137 DUNDEE	ELGIN	KAN E	IL	60120	42.05802/-88 .26657
ILD984 818088	AMOCO STA 15966	1520 BIG TIMBER AND MCLEAN	ELGIN	KAN E	IL	60120	42.05839/-88 .3154
ILR000 167486	ARTSPACE	51 S SPRING ST	ELGIN	KAN E	IL	60120 6449	/
ILD025 443573	B AND B CLEANERS	835 WALNUT AVE	ELGIN	KAN E	IL	60120	42.02825/-88 .29935
ILD980 904601	BALL AEROSOL & SPECIALTY CONTAINERS INC	1717 GIFFORD RD	ELGIN	CO OK	IL	60120	41.99588/-88 .24625
IL0000 561340	BAZOS CLEANERS	805 SUMMIT	ELGIN	KAN E	IL	60120	42.046418/-8 8.260269

HANDLER ID	NAME	STREET	CITY	CO UNT Y	ST AT E	ZIP COD E	LATITUDE/L ONGITUDE
ILR000 145144	BE PRODUCTS INC	227 DUPAGE ST	ELGIN	KAN E	IL	60120	42.03604/-88 .28133
ILD984 810044	BELL LAND IMPROVEMENT INC	1350 GASKET DR	ELGIN	KAN E	IL	60120	41.99804/-88 .23894
ILD025 443664	BIGGERS CHEVROLET INC	1385 E CHICAGO ST	ELGIN	KAN E	IL	60120	42.029386/-8 8.237941
ILD984 921437	BIGGERS MITSUBISHI	1325 E CHICAGO ST	ELGIN	CO OK	IL	60120	42.0294/-88. 23974
ILR000 016501	BISON ELECTRIC	667 N STATE ST	ELGIN	KAN E	IL	60120	42.04998/-88 .2949
ILR000 053678	BLUFF CITY CEMETARY	945 BLUFF CITY BLVD	ELGIN	CO OK	IL	60120	42.0194/-88. 25397
ILD984 816330	BRADY READY MIX CO INC	9 N 419 ST CHARLES RD	ELGIN	KAN E	IL	60120	42.007527/-8 8.268168
ILD982 620346	BRIANS AUTO BODY	848A VILLA ST	ELGIN	KAN E	IL	60120	42.026905/-8 8.258195
ILD984 887901	BRIANS AUTO BODY	353 WILLARD AVE	ELGIN	KAN E	IL	60120	42.0294/-88. 25993
ILD025 443896	BROADWAY TIRE & SERVICE	368 DUNDEE AVE	ELGIN	KAN E	IL	60120	42.04491/-88 .27815
ILD150 053270	BUILDERS SQUARE NO 1433	400 AIRPORT RD	ELGIN	KAN E	IL	60120	42.06946/-88 .28706
ILD008 871782	BURREN TRANSFER CO	2ND AND BERKLEY STS	ELGIN	KAN E	IL	60120	42.02263/-88 .3217
ILD005 145024	BUTLER PHARMPAC	1300 ABBOTT DR	ELGIN	KAN E	IL	60120	42.05488/-88 .31005
ILD097 271290	BWIP INTL PUMP	695 CHURCH RD	ELGIN	KAN E	IL	60120	42.073379/-8 8.295316
ILD984 818377	CHAMPION FRAME ALIGNMENT	864 BLUFF CITY BLVD	ELGIN	KAN E	IL	60120	42.01967/-88 .25723
ILD984 817866	CHICAGO JR SCHOOL	1600 DUNDEE AVE	ELGIN	KAN E	IL	60120	42.065977/-8 8.263688
ILD984 906743	CHUCKS TRUCK AND TRAILER PAINTING	7 N 980 RT 25	ELGIN	KAN E	IL	60120	42.040316/-8 8.28678
IL0000 999268	CITGO GAS STATION	640 VILLA ST	ELGIN	KAN E	IL	60120	42.03038/-88 .26594
ILR000 040022	CITY OF ELGIN	236 DUNDEE AVE	ELGIN	KAN E	IL	60120	42.041301/-8 8.279823
ILR000 048637	CITY OF ELGIN	150 DEXTER CT	ELGIN	KAN E	IL	60120	42.04001/-88 .28474
ILR000 049957	COLUMBIA PIPE & SUPPLY CO	60 ANN ST	ELGIN	KAN E	IL	60120	42.04392/-88 .28608
ILD984 827337	COM ED ELGIN REPORTING CTR	350 SECOND ST	ELGIN	KAN E	IL	60120	42.024507/-8 8.321879
ILR000 161596	COMED	IRVING & KILPATRICK	CHICA GO	CO OK	IL	60120	/
ILR000 160747	COMED MANHOLE	DUNDEE RD & RTE 25	ELGIN	CO OK	IL	60120	/

HANDLER ID	NAME	STREET	CITY	CO UNT Y	ST AT E	ZIP COD E	LATITUDE/L ONGITUDE
ILR000 160853	COMED MANHOLE	DES PLAINES & HARRISON	FORE ST PARK	CO OK	IL	60120	/
ILD048 310924	COMPONENT PLASTICS	700 TOLLGATE RD	ELGIN	KAN E	IL	60120	42.0715/-88. 29654
ILR000 126052	CONCRETE SPECIALTY CO	1375 GIFFORD RD	ELGIN	KAN E	IL	60120	42.00526/-88 .24568
ILD980 896658	CONNECTOR SERV CORP	970 E CHICAGO ST	ELGIN	KAN E	IL	60120	42.03442/-88 .25363
ILD025 089087	CONTINENTAL DATAFORMS INC	1555 TIMBER CT	ELGIN	KAN E	IL	60120	42.057982/-8 8.314554
ILR000 133132	COOK COMMUNICATIONS	850 N GROVE AVE	ELGIN	KAN E	IL	60120	42.0524/-88. 28782
ILD005 216239	CORONA CORP FOX VALLEY MANUFACTURING DIV	1600 FLEETWOOD DR	ELGIN	KAN E	IL	60120	42.02173/-88 .31867
ILD984 908418	CRAWFORDS AUTOMOTIVE INC	151 N SPRING ST	ELGIN	KAN E	IL	60120	42.03976/-88 .28223
ILR000 172924	CVS PHARMACY 5829	500 DUNDEE AVE	ELGIN	KAN E	IL	60120	42.04666/-88 .27622
ILD005 082599	DAILY COURIER-NEWS	300 LAKE ST	ELGIN	KAN E	IL	60120	42.03355/-88 .27893
ILD005 176375	DANA CORP ELGIN PLANT	SOUTH STATE ST PO BOX 727	ELGIN	KAN E	IL	60120	/
ILD058 587759	DSM DESOTECH INC	1122 ST CHARLES ST	ELGIN	KAN E	IL	60120	42.01228/-88 .27097
ILD984 784140	DUNDEE AVE AUTO BODY REBUILDERS	432 DUNDEE AVE	ELGIN	KAN E	IL	60120	42.045207/-8 8.277072
ILR000 014068	EASTVIEW MANUFACTURING	970 ELIZABETH ST	ELGIN	KAN E	IL	60120	42.01588/-88 .27038
ILD984 838904	EBY BROWN	177 DOUGLAS	ELGIN	CO OK	IL	60120	42.04002/-88 .283979
ILR000 056895	EDS ATOZ RENTAL	720 E CHICAGO ST	ELGIN	CO OK	IL	60120	42.0363/-88. 26165
ILD062 409073	ELGIN CHRYSLER PLYMOUTH	1010 E CHICAGO ST	ELGIN	KAN E	IL	60120	42.03381/-88 .25138
ILD982 425803	ELGIN CITY GARAGE	37 ANN ST	ELGIN	KAN E	IL	60120	42.043753/-8 8.287233
IL0000 302786	ELGIN CITY OF	240 S GROVE AVE	ELGIN	KAN E	IL	60120	42.032495/-8 8.280013
ILR000 066415	ELGIN CITY OF	150 DEXTER COURT	ELGIN	KAN E	IL	60120	42.04001/-88 .28474
ILD089 826531	ELGIN CLEANERS	475 DUNDEE AVE	ELGIN	KAN E	IL	60120	42.046/-88.2 7606
ILD984 792077	ELGIN CORRUGATED BOX INC	824 RAYMOND	ELGIN	KAN E	IL	60120	42.018671/-8 8.273483
ILR000 189209	ELGIN FIRE DEPT STATION ONE	550 SUMMIT ST	ELGIN	KAN E	IL	60120	42.04663/-88 .2695

HANDLER ID	NAME	STREET	CITY	CO UNT Y	ST AT E	ZIP COD E	LATITUDE/L ONGITUDE
ILR000 106971	ELGIN LANDFILL	7N802 RTE 25	ELGIN	KAN E	IL	60120	42.03706/-88 .267749
IL0000 366880	ELGIN PLAZA INC	999 THRU 1019 E CHICAGO ST	ELGIN	CO OK	IL	60120	42.032868/-8 8.251047
ILD041 049867	ELGIN PRECISION GLASS CO INC	1200 ABBOTT DR	ELGIN	KAN E	IL	60120	42.05549/-88 .30717
ILR000 110155	ELGIN PUBLIC MUSEUM	225 GRAND BLVD	ELGIN	KAN E	IL	60120	42.041356/-8 8.263915
ILR000 183707	ELGIN RECYCLING ELECTRONICS WHSE	1615 DUNDEE AVE STE A	ELGIN	KAN E	IL	60120	42.067833/-8 8.262299
ILR000 158592	ELGIN RIFLE CLUB	405 RAMONA AVE	ELGIN	CO OK	IL	60120	42.026604/-8 8.255499
ILD000 672311	ELGIN SANITARY DISTRICT CITY OF	RAYMOND ST & PURIFY DR	ELGIN	KAN E	IL	60120	42.015111/-88 .27357
ILR000 166330	ELGIN SCHOOL DIST UNIT 46	1200 MAROON DR	ELGIN	CO OK	IL	60120	42.02525/-88 .24413
ILD984 818229	ELGIN SIGN SHOP	40 ANN ST	ELGIN	KAN E	IL	60120	42.043752/-8 8.287113
ILD054 326418	ELGIN SUPER AUTO PARTS	250 WILLARD	ELGIN	KAN E	IL	60120	42.03228/-88 .26106
ILD005 212303	ELGIN SWEEPER CO	1300 W BARTLETT RD	ELGIN	KAN E	IL	60120 7529	41.99512/-88 .24038
ILR000 046003	ELGIN TECH CENTER	RT 31 AND JERUSHA RD	ELGIN	KAN E	IL	60120	/
ILD139 588974	ELGIN TOYOTA	1200 E CHICAGO ST	ELGIN	KAN E	IL	60120	42.03163/-88 .24401
ILR000 054601	ELGIN WAREHOUSE & EQUIPMENT INC	1611 VILLA ST	ELGIN	CO OK	IL	60120	42.01468/-88 .2305
ILD070 166772	ELGIN WAYNE DISPOSAL CONTRACTORS	RTE 25	ELGIN	KAN E	IL	60120	/
ILD025 444837	ELGIN, CITY OF	464 MCBRIDE ST	ELGIN	KAN E	IL	60120	42.04644/-88 .28896
ILD984 781674	EMRO MARKETING NO 7095	ST CHARLES ST AND BLUFF BLVD	ELGIN	KAN E	IL	60120	42.030987/-8 8.272486
ILD984 827048	FAITH UNITED METHODIST CHURCH	19 CENTER ST	ELGIN	KAN E	IL	60120	42.03785/-88 .28061
IL0000 274811	FINCH & BARRY PROP LLC	1200 ST CHARLES ST	ELGIN	KAN E	IL	60120	42.01048/-88 .27085
ILR000 188110	FINISHMASTER INC 020	1050 BLUFF CITY BLVD	ELGIN	KAN E	IL	60120	42.0198/-88. 25023
ILD005 110804	FJW INDUSTRIES	667 N STATE	ELGIN	KAN E	IL	60120	42.04998/-88 .2949
IL0000 952242	FORREST AUTO BODY	950 VILLA ST	ELGIN	CO OK	IL	60120	42.02535/-88 .25323
ILD982 626020	FORTIS MACHINERY CORP	1464 SHELDON DR	ELGIN	KAN E	IL	60120	42.023815/-8 8.236441

HANDLER ID	NAME	STREET	CITY	CO UNT Y	ST AT E	ZIP COD E	LATITUDE/L ONGITUDE
ILR000 065532	FOX GROUP I	999 RAYMOND BLVD	ELGIN	KAN E	IL	60120	42.016509/-8 8.27283
ILD005 070529	FOX GROUP II	363 BLUFF CITY BLVD	ELGIN	KAN E	IL	60120	42.01797/-88 .27152
ILD082 049008	FOX VALLEY NISSAN	1040 E CHICAGO ST	ELGIN	KAN E	IL	60120	42.03353/-88 .25027
ILD058 590167	FREUNDORFER	1551 COMMERCE DR	ELGIN	KAN E	IL	60120	42.07229/-88 .29488
ILR000 114447	GARFIELD ELEM SCHOOL	420 S MAY	ELGIN	KAN E	IL	60120	42.0248/-88. 27002
IL0000 875559	GASTVIEW MFG INC	1107 DUNDEE AVE	ELGIN	KAN E	IL	60120	42.057029/-8 8.267466
ILR000 181032	GIFFORD STREET HIGH SCHOOL	46 S GIFFORD ST	ELGIN	KAN E	IL	60120	42.036349/-8 8.275849
ILD984 906891	GOODYEAR AUTO TRAINING CTR	211 DOUGLAS	ELGIN	KAN E	IL	60120	42.04089/-88 .28363
ILR000 039917	GRAND VICTORIA CASINO	250 S GROVE	ELGIN	KAN E	IL	60120	42.03144/-88 .27965
ILR000 037895	H & H USED AUTO & TRUCK	1175 BLUFF CITY BLVD	ELGIN	CO OK	IL	60120	42.019525/-8 8.245455
ILD005 214796	HAUMILLER ENGINEERING CO	445 RENNER DR	ELGIN	KAN E	IL	60120	42.0218/-88. 32375
ILD064 395577	HENKEL ADHESIVES CORP	740 TOLLGATE RD	ELGIN	KAN E	IL	60120	42.07154/-88 .29739
ILD984 886747	HENKEL ADHESIVES TECHNOLOGIES	1347 GASKET DR	ELGIN	CO OK	IL	60120	41.9965/-88. 23927
ILR000 052308	HERTZ EQUIPMENT RENTAL	1040 E CHICAGO/SITE B	ELGIN	CO OK	IL	60120	/
ILR000 154666	HIWYN CORP	1400 MADELINE LN	ELGIN	CO OK	IL	60120	42.06894/-88 .34927
ILD982 211617	HOPKINS RON FORD	1045 E CHICAGO ST	ELGIN	KAN E	IL	60120	42.03195/-88 .25066
ILD982 620460	HUSSMANN FOODSERVICE TOASTMASTER	1050 CONGDON AVE	ELGIN	KAN E	IL	60120	42.05981/-88 .2463
ILD984 781997	HY TECH AUTO FRAME AND ALIGNMENT INC	910 E CHICAGO	ELGIN	KAN E	IL	60120	42.03529/-88 .25632
ILD981 955271	IL CENTRAL MGMNT SERV DEPT OF VEHICLES	595 S STATE ST	ELGIN	KAN E	IL	60120	42.02025/-88 .28367
ILR000 051730	IL STATE TOLL HWY PLAZA 13	190 NW TOLLWAY @ MP 22.5	ELGIN	KAN E	IL	60120	/
ILR000 051748	IL STATE TOLLWAY PLAZA 11	190 NW TOLLWAY @ MP 24.1	ELGIN	KAN E	IL	60120	/
ILD984 775502	ILDOT	109 CENTER ST	ELGIN	KAN E	IL	60120	42.038827/-8 8.281024

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ILD982 062549	ILLINOIS CLEANERS	674 DUNDEE AVE	ELGIN	KAN E	IL	60120	42.04972/-88 .27359
ILD012 123139	ILLINOIS RACING BOARD LABORATORY	750 S STATE MENDEL BLDG EMHC	ELGIN	KAN E	IL	60120	/
ILR000 167106	ILLINOIS WHOLESALE CASH REGISTER	2790 PINNACLE DR	ELGIN	WIL L	IL	60120	42.095172/-8 8.345028
ILD984 921874	INDUSTRIAL METALS RECYCLING CTR	955 BRANDT DR	ELGIN	KAN E	IL	60120	42.06801/-88 .25561
ILD990 817249	ITW SHAKEPROOF AUTO	1209 ST CHARLES RD	ELGIN	KAN E	IL	60120	42.00998/-88 .26917
IL0000 122093	JAD DETAILING	1468 SLELDON DR	ELGIN	KAN E	IL	60120	42.023566/-8 8.236297
ILR000 174326	JEWEL OSCO 3291	1040 SUMMIT AVE	ELGIN	KAN E	IL	60120	42.046474/-8 8.252559
ILD984 847459	JONES LES AUTOMOTIVE	147 S LIBERTY ST	ELGIN	KAN E	IL	60120	42.03437/-88 .2676
ILR000 080432	K & R CHRISTOPHER INC.	216 PRAIRIE ST	ELGIN	KAN E	IL	60120	42.03473/-88 .28106
ILD062 413570	KATY IND BLUFF CITY	366 BLUFF CITY BLVD	ELGIN	KAN E	IL	60120	42.01942/-88 .2716
ILD984 901942	KCK GRAPHICS INC	1000 N PRESTON	ELGIN	KAN E	IL	60120	42.05502/-88 .26549
ILR000 041293	KELLENBERGER AUTO	217 SYMPHONY WAY	ELGIN	KAN E	IL	60120	42.04019/-88 .28196
ILD984 816892	KIMBALL HILL INC BRAY FARM	SHOE FACTORY RD	ELGIN	CO OK	IL	60120	/
ILD005 142492	KINNEY ELECTRICAL MFG CO	678 BUCKEYE ST	ELGIN	KAN E	IL	60120	42.04865/-88 .2956
ILR000 180638	KLOCKNER DESMA SCHUHMASCHINEN GMBH	1605 DUNDEE AVE UNIT C	ELGIN	KAN E	IL	60120	42.06835/-88 .26004
ILD055 409536	KNOWLES ELECTRONICS INC ELGIN DIV	440 S MCLEAN BLVD	ELGIN	KAN E	IL	60120	42.02163/-88 .31273
ILR000 051813	L A AUTO CLINIC	212 DUNDEE AVE	ELGIN	KAN E	IL	60120	42.04059/-88 .28071
ILR000 186767	LAKESIDE BANK	1501 W BARTLETT RD	ELGIN	CO OK	IL	60120	41.99444/-88 .21809
ILR000 191825	LARSON MIDDLE SCHOOL	665 DUNDEE AVE	ELGIN	KAN E	IL	60120	/
ILD982 623019	LEMON GROVE MOTORS	350 WILLARD AVE	ELGIN	KAN E	IL	60120	42.02981/-88 .26079
ILD982 065237	LORDS ONE HOUR MARTINIZING	830 SUMMIT ST	ELGIN	KAN E	IL	60120	42.046427/-8 8.259282
ILR000 053686	LORDS PARK MAINT BLDG	GRAND BLVD	ELGIN	CO OK	IL	60120	42.03804/-88 .26319
ILD984 916072	LOSE DICK MARATHON	789 SUMMIT ST	ELGIN	CO OK	IL	60120	42.04607/-88 .26079

HANDLER ID	NAME	STREET	CITY	CO UNT Y	ST AT E	ZIP COD E	LATITUDE/L ONGITUDE
ILD981 097843	MAACO AUTO PAINTING AND BODYWORKS	235 S GROVE AVE	ELGIN	KAN E	IL	60120	42.032499/-8 8.280017
ILR000 050898	MCGRATH HONDA	955 E CHICAGO	ELGIN	CO OK	IL	60120	42.033763/-8 8.25409
ILR000 175224	MCKINLEY SCHOOL	258 LOVELL ST	ELGIN	KAN E	IL	60120	42.05028/-88 .2802
ILR000 053199	MEMORIAL FIELD	1199 EAST CHICAGO ST	ELGIN	KAN E	IL	60120	42.03167/-88 .2465
ILD025 447053	METRO PAINT SUPPLIES INC	1048 BLUFF CITY BLVD	ELGIN	KAN E	IL	60120	42.0198/-88. 25053
ILD981 798416	MEYER MATERIAL CO YARD 14	RTE 25 AND JEWEL RD	ELGIN	KAN E	IL	60120	/
ILD005 532866	MIDLAND STANDARD INC	603 E CHICAGO ST	ELGIN	KAN E	IL	60120	42.03689/-88 .26765
ILR000 045542	MIDWEST SOIL REMEDIATION INC	1480 SHELDON DR	ELGIN	CO OK	IL	60120	42.023785/-8 8.236112
ILR000 138115	MIDWESTERN AUTO	1201 BLUFF CITY	ELGIN	KAN E	IL	60120	42.01873/-88 .24391
ILD000 682047	MOGUL CORP MIDWEST DIV	SECOND AND BURKLEY	ELGIN	KAN E	IL	60120	42.02239/-88 .32151
ILD982 425878	MOTRA TRANSMISSION	575 PAGE AVE	ELGIN	KAN E	IL	60120	42.05387/-88 .26869
ILD106 928500	NATIONAL ELECTRO PLATING LTD	951 RAYMOND ST	ELGIN	KAN E	IL	60120	42.017113/-8 8.272739
ILD982 219156	NAVARRETE PONTIAC GMC	909 E CHICAGO ST	ELGIN	CO OK	IL	60120	42.034518/-8 8.256621
ILR000 110163	NEIGHBORHOOD HOUSING SERVICES	300 DOUGLAS AVE	ELGIN	KAN E	IL	60120	42.04301/-88 .28439
ILD113 775795	NORTHWEST VALLEY DODGE	845 E CHICAGO ST	ELGIN	KAN E	IL	60120	42.03451/-88 .25881
ILD982 219149	NORTHWEST VALLEY DODGE HYUNDAI	881 E CHICAGO ST	ELGIN	KAN E	IL	60120	42.0338/-88. 25691
ILD984 849216	NORWOOD TRANSPORT INC	RT 1 BOX 96	ELGIN	KAN E	IL	60120	/
ILD005 480678	NOVENCO FANS INC	1400 SHELDONS DR	ELGIN	KAN E	IL	60120	42.02492/-88 .23602
ILD984 907170	OLLMAN RALPH AND SUE	223 DUNDEE AVE	ELGIN	KAN E	IL	60120	42.04095/-88 .27971
ILD044 232304	OLYMPIC CONTROLS CORP	161 S GROVE AVE	ELGIN	KAN E	IL	60120	42.03349/-88 .28081
ILR000 155283	PADRON, TERESA	315 CONGDON AVE	ELGIN	KAN E	IL	60120	42.05662/-88 .2788
ILR000 184473	PELUSO, PATSY	426 BOWEN CT	ELGIN	KAN E	IL	60120	42.02824/-88 .27037
ILD984 919100	PEP BOYS 1407	1020-1050 SUMMIT ST	ELGIN	CO OK	IL	60120	42.04766/-88 .2534
ILD980 701270	PICCS	415 N GROVE	ELGIN	KAN E	IL	60120	42.04486/-88 .286825

HANDLER ID	NAME	STREET	CITY	CO UNT Y	ST AT E	ZIP COD E	LATITUDE/L ONGITUDE
ILD981 000631	PICCS INC	20 S STATE ST	ELGIN	KAN E	IL	60120	42.03601/-88 .28732
ILD025 719063	PLOTE CONSTRUCTION INC	1100 BRANDT RD	ELGIN	KAN E	IL	60120	/
ILD151 788551	POPLAR CREEK AUTO BODY	600 VARSITY DR	ELGIN	KAN E	IL	60120	42.02565/-88 .25116
ILD984 920033	POPLAR CREEK AUTO BODY INC	360 WILLARD AVE	ELGIN	KAN E	IL	60120	42.02928/-88 .26101
ILD984 924613	PRICE RIGHT AMOCO	816 ST CHARLES	ELGIN	KAN E	IL	60120	42.01935/-88 .26905
ILD984 925891	PROGRESSIVE PLASTICS INC	303 N RAMONA AVE	ELGIN	KAN E	IL	60120	42.029803/-8 8.253614
ILR000 049114	PROMAC INC	38 S GROVE	ELGIN	KAN E	IL	60120	/
ILR000 151704	R3 ENVIRONMENTAL MGT INC	1050 E CHICAGO ST	ELGIN	CO OK	IL	60120	42.03338/-88 .24968
ILD984 846253	RASMUSSEN STEEL	305 RAMONA B	ELGIN	KAN E	IL	60120	42.02965/-88 .25392
ILD982 211401	REGENT AUTO BODY	956 VILLA ST	ELGIN	KAN E	IL	60120	42.02517/-88 .25309
ILR000 016956	RIVER PLACE	52 S GROVE AVE	ELGIN	KAN E	IL	60120	42.03626/-88 .28379
ILR000 122614	ROADSTER SHOP	55 FRANKLIN BLVD	ELGIN	KAN E	IL	60120	42.04267/-88 .28647
ILR000 119164	ROUTE 19 AUTOBODY	1050 E CHICAGO ST REAR	ELGIN	CO OK	IL	60120	42.03338/-88 .24968
ILD984 766980	RR DONELLEY AND SONS CO	168 E HIGHLAND AVE	ELGIN	KAN E	IL	60120	42.03846/-88 .28303
ILD000 805911	SAFETY-KLEEN SYSTEMS INC	1500 E VILLA ST	ELGIN	CO OK	IL	60120	42.019029/-8 8.234981
ILR000 158212	SCHOOL DIST U46	355 E CHICAGO ST	ELGIN	KAN E	IL	60120	42.03702/-88 .27609
ILD984 903989	SCHOOL DIST U46 TRANS FAC	500 SHALES PKWY	ELGIN	CO OK	IL	60120	42.03045/-88 .23679
ILR000 149906	SCHOOL DISTRICT U-46	1460 SHELDON DR	ELGIN	CO OK	IL	60120	42.02476/-88 .23457
ILD981 789860	SCHOOL DISTRICT U46	647 LAUREL ST	ELGIN	KAN E	IL	60120	42.033789/-8 8.265699
ILR000 026757	SEIGLES HOME & BLDG	502 N GROVE	ELGIN	KAN E	IL	60120	42.04693/-88 .28777
ILR000 026765	SEIGLES HOME & BLDG	520 MC BRIDE ST	ELGIN	KAN E	IL	60120	42.046532/-8 8.288263
ILD984 917773	SERVICEMASTER OF ELGIN SCHAUMBURG	692 MAGNOLIA CT	ELGIN	KAN E	IL	60120	42.05032/-88 .19247
ILR000 133033	SET PIECE PRODUCTIONS LTD	853 DUNDEE AVE-B	ELGIN	KAN E	IL	60120	42.0527/-88. 2693

HANDLER ID	NAME	STREET	CITY	CO UNT Y	ST AT E	ZIP COD E	LATITUDE/L ONGITUDE
ILD982 606683	SHELL OIL PRODUCTS	1032 LARKIN AVE	ELGIN	KAN E	IL	60120	42.03682/-88 .30406
ILR000 036434	SHELL OIL PRODUCTS US	1389 DUNDEE	ELGIN	KAN E	IL	60120	42.06256/-88 .26482
ILD025 448366	SHERMAN HOSPITAL	934 CENTER	ELGIN	KAN E	IL	60120	42.05416/-88 .28138
ILR000 180265	SHERWIN WILLIAMS 3061	1310 DUNDEE RD	ELGIN	KAN E	IL	60120	42.06182/-88 .26593
ILD051 081545	SIMPSON ELECTRIC CO	853 DUNDEE AVE	ELGIN	KAN E	IL	60120	42.0527/-88. 2693
ILR000 160275	SITEX REALTY GROUP	1700 BIG TIMBER RD	ELGIN	KAN E	IL	60120	42.05894/-88 .31861
ILR000 029652	SMG CORP	1150 ST CHARLES ST	ELGIN	KAN E	IL	60120	42.011461/-8 8.269258
ILD116 048570	SMITHS OIL WELL	826 E CHICAGO ST	ELGIN	KAN E	IL	60120	42.03542/-88 .2586
ILD021 435961	SPEEDWAY 7756	1156 DUNDEE AVE	ELGIN	KAN E	IL	60120 2263	42.05866/-88 .26716
ILD000 674036	SUNOCO SERVICE STATION	1414 DUNDEE AVE	ELGIN	KAN E	IL	60120	42.06313/-88 .26547
ILD984 802371	SVENDSEN BROS	964 ELIZABETH ST	ELGIN	KAN E	IL	60120	42.01606/-88 .27035
ILD981 949134	TEMP HEAT RUPP INDUSTRIES INC	39 W 207 W HIGHLAND RD	ELGIN	KAN E	IL	60120	/
ILD984 887687	TG AUTO BODY	339 SADLER	ELGIN	CO OK	IL	60120	42.02987/-88 .25765
ILD984 775684	TOASTMASTER A MIDDLEBY CO	1400 TOASTMASTER DR	ELGIN	KAN E	IL	60120	42.05981/-88 .2463
ILD059 481515	TOMS AUTO CLINIC AND BODY SHOP	264 PRAIRIE ST	ELGIN	KAN E	IL	60120	42.03472/-88 .28021
ILD981 802291	TORRES AUTO REPAIR	966 VILLA AVE	ELGIN	KAN E	IL	60120	42.02496/-88 .25293
ILD146 868443	TYLER CREEK CLEANERS	12 TYLER CREEK PLAZA	ELGIN	KAN E	IL	60120	42.058446/-8 8.314331
ILD984 822767	UNO VEN 76 THE	1580 LARKIN AVE	ELGIN	LAK E	IL	60120	42.03453/-88 .31718
ILD984 767079	VALLEY BUSINESS CENTER	1020 N MCLEAN BLVD	ELGIN	KAN E	IL	60120	42.05765/-88 .31659
ILD984 848960	VAN DEN BERGH FOODS	775 LAUREL ST	ELGIN	KAN E	IL	60120	42.033851/-8 8.26109
ILD981 797822	VAN ECK COLLISION	1045 E CHICAGO ST	ELGIN	KAN E	IL	60120	42.03195/-88 .25066
ILD981 098981	VERIKLEEN	390 SADLER AVE	ELGIN	KAN E	IL	60120	42.02941/-88 .25866
ILR000 113324	VULCAN CONSTRUCTION MATERIALS	9N419W RTE 25	ELGIN	KAN E	IL	60120	42.038016/-8 8.285078

HANDLER ID	NAME	STREET	CITY	COUNTY	STATE	ZIP CODE	LATITUDE/LONGITUDE
ILR000 000737	WASTE MGMT WEST	7 N 904 RT 25	ELGIN	KANE	IL	60120	41.984272/-88.268924
ILR000 010199	WAUCONDA TOOL & ENG CO	690 CHURCH RD	ELGIN	KANE	IL	60120	42.07387/-88.295722
ILD122 318207	WEST SIDE ONE HOUR CLEANERS	315 S MCLEAN BLVD	ELGIN	KANE	IL	60120	42.02512/-88.31158

RCRAInfo Search Results Envirofacts US EPA

HANDLE R ID	NAME	STREET	CITY	COUN TY	ST AT E	ZIP CODE	LATITUDE/LO NGITUDE
ILR0001 15469	7-ELEVEN 33135	1570 N RANDALL RD	ELGIN	KANE	IL	60123	42.07339/-88.33636
ILR0000 14639	875 TOLLGATE	875 TOLLGATE RD	ELGIN	KANE	IL	60123	42.07091/-88.3008
ILR0001 36986	ABRADING METHODS	1011 DAVIS RD	ELGIN	KANE	IL	60123	42.068099/-88.304597
ILR0000 67371	ABRASIVE TECHNOLOGIES	1175 BOWES RD	SOUTH ELGIN	KANE	IL	60123	42.00703/-88.30594
ILR0000 35766	AJ FUNK & CO	1471 TIMBER DR	ELGIN	KANE	IL	60123	42.058006/-88.312995
ILD9822 06716	ALCHEMITRON INC	1435-1437 HOLMES RD	ELGIN	KANE	IL	60123	42.069655/-88.312534
ILR0000 65342	ALPHA METALS	580 A TOLLGATE RD B	ELGIN	KANE	IL	60123	/
ILR0001 60721	AMANO ENZYMES USA CO LTD	2150 POINT BLVD STE 100	ELGIN	KANE	IL	60123	42.08069/-88.32813
ILD9849 12212	AMERICAN INK AND SUPPLY	809 N STATE ST	ELGIN	KANE	IL	60123	42.053343/-88.295582
ILD0852 17966	AMERICAN NTN BEARING MFG CORP	1500 HOLMES RD	ELGIN	KANE	IL	60123	42.07108/-88.31488
ILD9849 24522	AMOCO 15477	338 S MCLEAN BLVD B	ELGIN	KANE	IL	60123	42.02434/-88.31256
ILD9849 01074	AMSTAR	800 N STATE	ELGIN	KANE	IL	60123	42.05276/-88.29583
ILD9847 82631	AMTEC PRECISION PRODUCTS INC	1875 HOLMES RD	ELGIN	KANE	IL	60123	42.07025/-88.32314
ILD9849 05984	APEX CLEANERS	120 TYLER CREEK PLAZA	ELGIN	KANE	IL	60123	42.058833/-88.314342
ILD9847 93117	APPLIED PROCESS	700 CHURCH RD	ELGIN	KANE	IL	60123	42.07378/-88.29625
ILD9849 20645	APPLIED WEB SYSTEMS	1875 FOX LN	ELGIN	KANE	IL	60123	42.07339/-88.32177
ILD9848 49737	ARIANS AUTO BODY	38 W 604 RTE 20	ELGIN	KANE	IL	60123	/
ILD9848 48200	ARTISTIC CARTON CO	1975 BIG TIMBER RD	ELGIN	KANE	IL	60123	42.05952/-88.32623
ILD9848 29549	ASSOC FOR INDIVIDUAL DEV	1485 DAVIS RD	ELGIN	KANE	IL	60123	42.06768/-88.31424
IL00003 51684	ASSOCIATED MACHINE REBUILDING	1150 DAVIS UNIT N	ELGIN	KANE	IL	60123	42.067382/-88.311788
ILD9847 74554	ASTRO OPTICS CORP	1200 ABBOTT DR	ELGIN	KANE	IL	60123	42.05549/-88.30717
ILD9848 33517	BAKER RD FURNITURE CORP	730 SCHNEIDER DR	SOUTH ELGIN	KANE	IL	60123	42.006/-88.3019
ILD1447 87553	BALZERS TOOL COATING INC	1181 JANSEN FARM	ELGIN	KANE	IL	60123	42.06083/-88.32938

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ILR0001 48924	BFI INNOVATIONS INC	1925 HOLMES RD	ELGIN	KANE	IL	60123	42.07056/-88. 32498
ILR0001 33389	BP AMOCO 86944	1700 N STATE	ELGIN	KANE	IL	60123	42.07399/-88. 2902
ILD0127 77041	BRIDGEPORT HARIG PRODUCTS	1875 BIG TIMBER RD	ELGIN	KANE	IL	60123	42.05835/-88. 32154
ILR0000 51649	BRIGITFLEX INC	1725 FLEETWOOD DR	ELGIN	KANE	IL	60123	42.02033/-88. 31947
ILD9848 41338	BRITTHINS EXPRESS OIL AND LUBE	1570 LARKIN AVE	ELGIN	KANE	IL	60123	42.03473/-88. 31627
ILR0001 11898	CAGAN MGT GROUP	1433 DAVIS ST	ELGIN	KANE	IL	60123	42.068259/-88 .312623
ILD9826 32218	CAP AND SEAL CO	1625 FLEETWOOD DR	ELGIN	KANE	IL	60123	42.020497/-88 .316479
ILD0433 55460	CAPSONIC GROUP INC	460 S 2ND ST	ELGIN	KANE	IL	60123	42.093971/-88 .278564
ILD9847 84934	CAR MON PRODUCTS	1225 DAVIS RD	ELGIN	KANE	IL	60123	42.06763/-88. 30839
ILR0001 91254	CARLSON TOOL & MACHINE CO	1875 BIG TIMBER RD STE A	ELGIN	KANE	IL	60123	42.05835/-88. 32154
ILD9848 87919	CENTRICO INC	725 TOLLGATE RD STE B	ELGIN	KANE	IL	60123	42.06933/-88. 29707
ILD9848 30976	CHICAGO ST AUTOBODY	314 W CHICAGO ST	ELGIN	KANE	IL	60123	42.03587/-88. 28928
ILR0000 40014	CITY OF ELGIN SITE 43	1450 BOWES RD	ELGIN	KANE	IL	60123	42.006957/-88 .314149
ILR0001 65035	CLEANERS MART	2375 BOWES RD	ELGIN	KANE	IL	60123	42.007155/-88 .325977
ILR0001 43719	CLIFTON ST MERCURY SPILL	61 S CLIFTON	ELGIN	KANE	IL	60123	42.03278/-88. 30898
ILR0001 00248	COBRA METAL WORKS	1130 JANSEN FARM DR	ELGIN	KANE	IL	60123	42.06116/-88. 33205
IL00010 02997	COLONY INC	350 RIVER RIDGE RD	ELGIN	KANE	IL	60123	42.07416/-88. 28488
ILR0001 46233	COLONY INC	2500 GALVIN DR	ELGIN	KANE	IL	60123	42.09703/-88. 34597
ILR0001 08332	COMMUNICATIONS TEST DESIGN INC	2200 CALVIN DR	ELGIN	KANE	IL	60123	/
ILD9848 11497	COMPLETE AUTO BODY	1730 BERKLEY ST	ELGIN	KANE	IL	60123	42.02312/-88. 31869
ILD9826 36649	CONTAINER PRINT OF ILL INC	1725 WELD RD	ELGIN	KANE	IL	60123	42.02438/-88. 31927
ILR0001 17119	CORPORATE PLAZA OF ELMHURST	501 W LAKE ST STE 206	ELMHU RST	DU PAGE	IL	60123	41.91968/-87. 955
ILD0685 06286	CREEKSIDE PRTG	1175 DAVIS RD	ELGIN	KANE	IL	60123	42.06763/-88. 30756
ILR0000 14027	D & S COMMUNICATION	1355 N MCCLEAN	ELGIN	COOK	IL	60123	42.06726/-88. 31593

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ILR0000 07427	DAILY HERALD	440 H AIRPORT RD	ELGIN	KANE	IL	60123	/
ILR0000 50385	DEANZA WILLOW LAKE ESTATES	280 PARK LANE RD	ELGIN	KANE	IL	60123	42.06341/-88.28296
ILR0001 18265	DIGITAL APPLIANCE CONTROLS	1901 SOUTH ST	ELGIN	KANE	IL	60123	42.02506/-88.32407
ILD1614 13406	DIGITAL APPLIANCE CONTROLS INC	620 WING ST	ELGIN	KANE	IL	60123	42.04581/-88.29458
ILR0001 74508	DNR CONSULTING INC	1150 DAVIS RD UNIT B	ELGIN	KANE	IL	60123	42.06896/-88.30714
IL00002 42404	DONNELLEY R R AND SONS CO	1275 DAVIS RD	ELGIN	KANE	IL	60123	42.06763/-88.30922
ILD0391 29622	DONNELLEY R R AND SONS ELGIN SERVICE CTR	655 BIG TIMBER RD	ELGIN	KANE	IL	60123	42.05777/-88.29587
ILD9849 19480	DONNELLEY RR AND SONS CO ELGIN SVC CTR	645 TOLLGATE RD	ELGIN	KANE	IL	60123	42.07029/-88.29447
ILD0519 43355	DORLE REALTY	1100 DAVIS RD	ELGIN	KANE	IL	60123	42.06859/-88.30537
ILR0001 03341	DUO FAST CORP	2400 GALVIN DR	ELGIN	COOK	IL	60123	42.09342/-88.34748
IL00010 21393	DYNACAST INC	195 CORPORATE DR	ELGIN	COOK	IL	60123	42.07208/-88.27981
ILR0000 36889	E-Z GO 107	1841 WAUKEGAN RD B	GLENV IEW	COOK	IL	60123	42.0886/-87.79763
ILD1160 46467	ELGILOY SPECIALTY METALS	1616 BERKLEY	ELGIN	KANE	IL	60123	42.02317/-88.31734
ILD9810 91622	ELGILOY SPECIALTY METALS	1565 FLEETWOOD DR	ELGIN	KANE	IL	60123	42.02043/-88.31536
ILD9820 61665	ELGIN CITY OF DEPT OF TRANSPORTATION	573 N CRYSTAL ST	ELGIN	KANE	IL	60123	42.047774/-88.296537
ILD9849 07980	ELGIN CITY OF WATER DEPT	375 W RIVER RD	ELGIN	KANE	IL	60123	42.063056/-88.285833
ILD9849 07998	ELGIN CITY OF WATER DEPT	74 N AIRLITE ST	ELGIN	KANE	IL	60123	42.034213/-88.328911
ILD0680 07491	ELGIN COMMUNITY COLLEGE	1700 SPARTAN DR	ELGIN	KANE	IL	60123	42.01789/-88.32253
IL00001 22101	ELGIN EBY BROWN CO	1313 TIMBER DR	ELGIN	KANE	IL	60123	42.05805/-88.310064
ILD0442 26751	ELGIN EQUIPMENT CO	2ND AND BERKLEY	ELGIN	KANE	IL	60123	42.02263/-88.3217
ILD0052 13038	ELGIN INDUSTRIES INC	1100 JANSEN FARM DR	ELGIN	KANE	IL	60123	42.06125/-88.33275
ILD0820 50543	ELGIN MENTAL HEALTH CENTER	750 S STATE	ELGIN	KANE	IL	60123	42.01443/-88.28978
ILR0000 63602	ELGIN MOLDED PLASTICS	909 GRACE	ELGIN	COOK	IL	60123	42.01732/-88.27059

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ILD0254 46956	ELGIN ORIVELINE	226 N STATE ST	ELGIN	KANE	IL	60123	42.04036/-88. 29063
ILD9848 49521	ELGIN PRINTING	1436 DAVIS RD	ELGIN	KANE	IL	60123	42.068259/-88 .312707
ILR0001 45367	ELGIN TOWNSHIP ROAD DISTRICT	725 S MCLEAN BLVD	ELGIN	KANE	IL	60123	42.016596/-88 .314279
ILR0001 07367	FABRIC IMAGES INC	300 CORPORATE DR	ELGIN	KANE	IL	60123	42.07389/-88. 28262
ILR0000 36061	FEDERAL AVIATION ADM	1100 BOWES RD	ELGIN	KANE	IL	60123	42.01034/-88. 3044
ILR0001 55333	FIRST AYD	1325 GATEWAY DR	ELGIN	KANE	IL	60123	42.06501/-88. 34328
ILR0001 24222	FIRST PRIORITY	1585 TODD FARM DR	ELGIN	KANE	IL	601231 146	42.06215/-88. 31728
ILR0001 48452	FIRST PRIORITY INC	1590 TODD FARM DR	ELGIN	KANE	IL	60123	42.06318/-88. 31785
ILD9847 93364	FLINT INK CORP	1524 DAVIS RD	ELGIN	KANE	IL	60123	42.06827/-88. 31463
ILR0001 07813	FLINT INK NORTH AMERICA CORP	1524 DAVIS RD	ELGIN	KANE	IL	60123	42.06827/-88. 31463
ILD9847 87168	FORMER SPEEDWAY 7617	771 WALNUT AVE	ELGIN	KANE	IL	601237 466	42.02841/-88. 29752
ILR0001 65514	GIBBON PRINTING INKS	801 N STATE ST STE A	ELGIN	COOK	IL	60123	42.05317/-88. 2948
ILR0000 44156	GIVAUDAN FLAVORS CORP	580 TOLLGATE RD STE A	ELGIN	KANE	IL	60123	42.07183/-88. 29302
ILR0000 05553	GMT INC	180 S MELROSE	ELGIN	COOK	IL	60123	42.02968/-88. 3085
ILR0000 80689	GRAND EAGLE SERVICES	1215 BOWES ROAD	ELGIN	KANE	IL	60123	42.00665/-88. 30699
ILR0001 87872	GREENWOOD MOTORLINES DBA R&L CARRIERS	375 S 2ND ST	ELGIN	KANE	IL	60123	42.095348/-88 .27857
ILD0450 48584	HAGG PRESS INC	1165 JANSEN FARM CT	ELGIN	KANE	IL	60123	42.05971/-88. 33022
ILD9848 33806	HANSEN SHELL	305 S MCLEAN BLVD	ELGIN	KANE	IL	60123	42.02556/-88. 31174
ILD0102 32965	HAWKS AUTO PARTS INC	1480 ABBOTT DR	ELGIN	KANE	IL	60123	42.05559/-88. 3144
ILR0001 30062	HERITAGE CRYSTAL CLEAN LLC	2175 POINT BLVD STE 375	ELGIN	KANE	IL	60123	42.08249/-88. 33186
ILR0001 25591	HOME DEPOT 1934	955 N RANDALL RD	ELGIN	KANE	IL	60123	42.05137/-88. 33906
ILR0001 62644	HYDRAULIC ENGINEERING	11N068 RIPPBURGER RD UNIT C	ELGIN	KANE	IL	60123	42.02788/-88. 430241
ILR0001 05106	HYDROX	825B TOLLGATE RD	ELGIN	KANE	IL	60123	42.0709/-88.2 994

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ILR0001 12060	IHC CONSTRUCTION CO INC	1500 EXECUTIVE DR	ELGIN	KANE	IL	60123	42.07156/-88. 30002
ILD9826 05990	ILL DEPT OF TRANS	595 S STATE ST	ELGIN	KANE	IL	60123	42.02025/-88. 28367
ILR0000 16550	ILLINOIS TOLLWAY AUTHORITY	TOLL PLAZA 9 I 90 MP25	ELGIN	KANE	IL	60123	/
ILR0001 08837	IMAGE CIRCUITS INC	937 DAVIS RD	ELGIN	KANE	IL	60123	42.067984/-88 .303251
ILR0000 17871	IMAGING OFFICE SYSTEMS OF IL	585 TOLLGATE RD STE A E	ELGIN	KANE	IL	60123	42.06986/-88. 29345
ILD9849 05513	ISP DIST 2 ELGIN HQ	777 S STATE ST	ELGIN	KANE	IL	60123	42.01466/-88. 28595
IL00008 75526	ITW CCNA	1765 HOLMES AVE	ELGIN	KANE	IL	60123	42.06991/-88. 32089
ILD0680 07384	JUDSON COLLEGE	1151 N STATE ST	ELGIN	KANE	IL	60123	42.06106/-88. 29257
ILR0001 82691	KIMBALL MIDDLE SCHOOL	451 N MCLEAN BLVD	ELGIN	KANE	IL	60123	42.021816/-88 .312647
ILR0000 79947	KREIS TOOL & MFG	1615 CAMBRIDGE	ELGIN	KANE	IL	60123	42.06246/-88. 31945
ILR0000 53165	LAKEVIEW SCREW MACHINE PRODUCTS INC	466 RENNER DR	ELGIN	KANE	IL	60123	42.02135/-88. 3252
ILR0001 01808	LARKIN AMOCO	1219 LARKIN AVE	ELGIN	KANE	IL	60123	42.03557/-88. 30855
ILR0000 06874	LARKIN CENTER	1212 LARKIN	ELGIN	COOK	IL	60123	42.03659/-88. 30887
ILR0001 16921	LARKIN CENTER	59 PARK ROW	ELGIN	KANE	IL	60123	42.03582/-88. 27683
ILR0001 13787	LARKIN HIGH SCHOOL	1475 LARKIN AVE	ELGIN	KANE	IL	60123	42.03202/-88. 31372
ILD1447 79410	LINATEX CORP OF AMERICA	1150 A DAVIS RD	ELGIN	KANE	IL	60123	/
ILR0001 45003	LOWES 2316	629 S RANDALL RD	ELGIN	KANE	IL	60123	42.018424/-88 .337149
ILR0001 23281	M TEK	1175 JANSEN FARM CT	ELGIN	KANE	IL	60123	42.06057/-88. 32887
ILR0000 33068	M&B ENTERPRISE CORP	11N263 BROOKSIDE DR UNIT B	ELGIN	KANE	IL	60123	42.03297/-88. 3356
ILR0001 22515	MARATHON OIL	500 S MCLEAN	ELGIN	KANE	IL	60123	42.02047/-88. 31371
ILD0051 98510	MASTER MOLDED PRODUCTS CORP	1000 DAVIS RD	ELGIN	KANE	IL	60123	42.06879/-88. 30398
ILD9849 21296	MATSUSHITA ELECTRIC CORP OF AM	1703 N RANDALL RD	ELGIN	KANE	IL	60123	42.075/-88.33 352
ILD9848 31388	MC LEAN AMOCO	338 S MC LEAN BLVD	ELGIN	KANE	IL	60123	42.02434/-88. 31256

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ILD9847 81179	MC LEAN SVCS	348 N STATE ST	ELGIN	KANE	IL	60123	42.04242/-88.29236
ILD9848 12560	MCGRATH BUICK	945 E CHICAGO AVE	ELGIN	KANE	IL	60123	42.03336/-88.25573
ILR0001 06872	MEIJER 183	801 S RANDALL RD	ELGIN	KANE	IL	60123	42.0148/-88.33637
ILR0001 40756	MEIJER STORE/GAS 183	815 S RANDALL RD	ELGIN	KANE	IL	60123	42.01223/-88.33452
ILR0000 34785	MERLINS MUFFLER & BRAKE	2465 SOUTH ST	ELGIN	KANE	IL	60123	42.024543/-88.34068
ILR0001 39303	METRA ELGIN MAINTENANCE FACILITY	75 S STATE ST	ELGIN	COOK	IL	60123	42.034768/-88.286184
ILR0000 10421	MODERN TRACK MACHINERY	1415 DAVIS RD	ELGIN	KANE	IL	60123	42.06764/-88.31254
IL00000 55939	MULTIFILM PACKAGING CORP	1040 N MCLEAN BLVD	ELGIN	KANE	IL	60123	42.05762/-88.31718
ILR0001 86866	MULTIFOIL	1700 BIG TIMBER RD-B	ELGIN	KANE	IL	60123	42.05894/-88.31861
ILR0000 44255	MULTITEK CIRCUITRONICS INC	1250 CRISPIN DR	ELGIN	KANE	IL	60123	42.00959/-88.3082
ILR0000 57141	NEW RANDALL CLEANERS	265 S RANDALL RD	ELGIN	KANE	IL	60123	42.026155/-88.338793
ILD9817 88110	NICOR GAS	1800 BIG TIMBER RD	ELGIN	KANE	IL	60123	42.05894/-88.31988
ILD9849 01421	NICOR GAS	SHOE FACTORY RD	ELGIN	KANE	IL	60123	/
ILR0000 06973	NORITSU AMERICA CORP	755 TOLLGATE RD	ELGIN	KANE	IL	60123	42.07075/-88.2976
ILD0745 59972	NORTHWESTERN TOOL AND DIE MFG	375 RENNER DR	ELGIN	KANE	IL	60123	42.02341/-88.32405
ILD9847 88562	OAK VIEW OFFICE PROPERTIES	505 DAVIS RD	ELGIN	KANE	IL	60123	42.0666/-88.29556
ILD0640 01340	P AND K PRODUCTS CO	1575 HOLMES RD	ELGIN	KANE	IL	60123	42.06972/-88.31569
ILD9847 87762	PACE RIVER DIV	975 S STATE ST	ELGIN	KANE	IL	60123	42.01127/-88.28735
ILR0001 45359	PALAPA COATINGS	330 CORPORATE DR	ELGIN	KANE	IL	60123	42.07389/-88.2823
ILR0001 65571	PANASONIC	410 AIRPORT RD	ELGIN	KANE	IL	60123	42.068284/-88.287441
ILR0001 19735	PARKER SEALS	2565 NORTHWEST PKWY	ELGIN	COOK	IL	60123	42.041026/-88.371081
ILR0000 41228	PHYSICIAN SALES & SERVICE	1450 N MCLEAN BLVD	ELGIN	KANE	IL	60123	42.07013/-88.31742
IL00009 17187	PIT SHOP THE	268 S STATE ST	ELGIN	KANE	IL	60123	42.02924/-88.28313

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ILD9809 94586	PLASTIC DECORATORS INC	1330 HOLMES RD	ELGIN	KANE	IL	60123	42.070056/-88 .31098
ILD9849 22260	PLATO AUTO BODY	11 N 435 MUIRHEAD RD	ELGIN	KANE	IL	60123	42.02932/-88. 420495
ILR0001 09488	POLY COMPOUNDING LLC	1390 GATEWAY SUITE 6	ELGIN	KANE	IL	60123	42.06833/-88. 34422
ILD9822 19594	PRECISION BODY WORKS	65 NATIONAL ST	ELGIN	KANE	IL	60123	42.02815/-88. 2805
ILD9822 19727	PRECISION DIAMOND TOOL CO	1741 FLEETWOOD DR	ELGIN	KANE	IL	60123	42.02033/-88. 31999
ILD9848 34127	PRICE CIRCUITS LLC	1300 HOLMES RD	ELGIN	KANE	IL	60123	42.06894/-88. 30998
ILD0898 25780	PRINTPACK INC	1400 ABBOTT DR	ELGIN	KANE	IL	601231 882	42.055856/-88 .31148
ILR0000 56598	PRO TECH GRAPHICS INC	1700 TODD FARM DR	ELGIN	KANE	IL	60123	42.06104/-88. 32281
ILD9849 15686	PROMAC INC	805 N STATE ST	ELGIN	KANE	IL	60123	42.0532/-88.2 9564
ILD0953 05132	PROTOTYPE TOOLONG AND PLASTIC	1439 HOLMES	ELGIN	KANE	IL	60123	42.069676/-88 .312634
ILR0001 02038	PROVENA SAINT JOSEPH HOSPITAL	77 N AIRLITE ST	ELGIN	KANE	IL	60123	42.03572/-88. 32661
ILR0000 26369	QUALEX INC	370 RIVER RIDGE RD	ELGIN	KANE	IL	60123	42.074/-88.28 49
ILR0000 07971	READ EXCAVATING CO	1919 BIG TIMBER RD	ELGIN	KANE	IL	60123	42.05913/-88. 32498
IL00003 66450	REIS MACHINES INC	1320 HOLMES RD	ELGIN	KANE	IL	60123	42.06974/-88. 30998
ILR0000 39842	REISHAUER CORP	1525 HOLMES RD	ELGIN	KANE	IL	60123	42.06956/-88. 31457
ILR0001 28074	RELIANCE TOOL	900 N STATE ST	ELGIN	KANE	IL	60123	42.054936/-88 .298569
ILD0052 17187	RELIANCE TOOL AND MFG	617 N STATE ST	ELGIN	KANE	IL	60123	42.04834/-88. 29452
ILD9848 52590	RIEKE OFFICE INTERIORS	2000 FOX LN	ELGIN	KANE	IL	60123	42.07437/-88. 32227
ILR0000 20552	RIEKE OFFICE INTERIORS	800 N STATE ST	ELGIN	KANE	IL	60123	42.05276/-88. 29583
ILD0050 86863	RINN CORP	1212 ABBOTT DR	ELGIN	KANE	IL	60123	42.05469/-88. 30819
ILD9849 08202	SAFETY KLEEN CORP	ONE BRINKMAN WAY	ELGIN	KANE	IL	60123	42.055521/-88 .341926
ILR0001 73294	SAMS CLUB 4942	1000 S RANDALL RD	ELGIN	KANE	IL	60123	42.00017/-88. 33613
ILD9848 91697	SEB ENTERPRISES INC	1702 BERKLEY	ELGIN	KANE	IL	60123	42.022649/-88 .318786
IL00003 38061	SEEGOTT INC	1675 D HOLMES RD	ELGIN	KANE	IL	60123	/

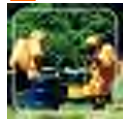
HANDLE R ID	NAME	STREET	CITY	COUN TY	ST AT E	ZIP CODE	LATITUDE/LO NGITUDE
ILR0001 74862	SHAW INDUSTRIES GROUP LOC 33	2410 GALVIN DR	ELGIN	KANE	IL	60123	42.091183/-88 .344387
ILR0001 56901	SHERMAN HOSPITAL	1425 N RANDALL RD	ELGIN	KANE	IL	60123	42.07001/-88. 33067
ILD0051 05598	SKF USA INC	900 N STATE ST	ELGIN	KANE	IL	60123	42.054936/-88 .298569
ILR0001 66595	SONDERHOFF USA CORP	1895 BIG TIMBER RD UNIT B	ELGIN	KANE	IL	60123	42.059019/-88 .322561
ILD9848 09905	SONIC PRINTED CIRCUITS INC	840 CHURCH RD	ELGIN	KANE	IL	60123	42.07382/-88. 29958
ILR0000 53694	SPARTAN MEADOWS GOLF COURSE	SPARTAN DR	ELGIN	KANE	IL	60123	/
ILR0001 57602	SPEEDLINE TECHNOLOGIES INC	2541 TECHNOLOGY DR	ELGIN	WILL	IL	60123	42.090867/-88 .341139
ILD9847 82797	SPEEDWAY 7514	1570 BIG TIMBER	ELGIN	KANE	IL	601231 702	42.0586/-88.3 159
ILD9848 48481	STAR DISPLAYS INC	38 W 636 RTE 20	ELGIN	KANE	IL	60123	/
ILD0051 71731	STARRO PRECISION PRODUCTS INC	37 N UNION ST	ELGIN	KANE	IL	60123	42.03569/-88. 29618
ILD0054 52958	SUBURBAN PLASTICS	340 RENNER DR	ELGIN	KANE	IL	60123	42.023/-88.32 532
ILD9824 28054	SUPREME COATING INC	925 TOLLGATE RD	ELGIN	KANE	IL	60123	42.07025/-88. 30154
ILD9848 02322	SYNTHETIC FUEL	432 WING PARK BLVD	ELGIN	KANE	IL	60123	42.04411/-88. 30503
ILD9849 05430	T AND L PROPERTIES	1480 ILLINOIS PKWY	ELGIN	KANE	IL	60123	42.047348/-88 .314741
ILR0001 39824	TARGET STORE 0834	300 S RANDALL RD	ELGIN	KANE	IL	60123	42.02579/-88. 34167
IL00009 74345	TED AND EDS RENTAL	2075 LARKIN AVE	ELGIN	KANE	IL	60123	42.03183/-88. 32819
ILD0054 69382	THOMPSON D H INC	11 N UNION ST	ELGIN	KANE	IL	60123	42.034807/-88 .296418
ILD9848 10499	THOMSENS AUTO	823 WALNUT AVE	ELGIN	KANE	IL	60123	42.02827/-88. 29861
ILR0001 02525	TOP CLEANERS	851 S RANDALL RD	ELGIN	KANE	IL	60123	42.012335/-88 .336197
ILR0000 36608	TOWNSHIP OF ELGIN	270 FULTON ST	ELGIN	KANE	IL	60123	42.03559/-88. 27982
ILD9820 69585	TRICOR SYSTEMS INC	400 RIVER RIDGE DR	ELGIN	KANE	IL	60123	42.0739/-88.2 8697
ILR0000 54254	TRICOR SYSTEMS INC	1650 TODD FARM DR	ELGIN	KANE	IL	60123	42.06161/-88. 31988
ILD9847 81344	UNILEVER BEST FOODS	51 N STATE ST	ELGIN	KANE	IL	60123	42.0376/-88.2 8781

HANDLE R ID	NAME	STREET	CITY	COUN TY	ST AT E	ZIP CODE	LATITUDE/LO NGITUDE
ILD0972 82719	UNITED BODY WORKS	533 N STATE ST	ELGIN	KANE	IL	60123	42.04633/-88. 2938
ILD9811 89376	UNIVERSAL CHEMICALS & COATINGS	1975 FOX LN	ELGIN	KANE	IL	60123	42.073436/-88 .323881
ILD9819 60339	UNIVERSAL SPC INC	412 N STATE ST	ELGIN	KANE	IL	60123	42.04484/-88. 29346
IL00009 99169	US CAN CO	1111 BOWES RD	ELGIN	KANE	IL	60123	42.00766/-88. 30498
ILD9824 24889	US PRECISION GLASS	1900 HOLMES RD	ELGIN	KANE	IL	60123	42.07206/-88. 32328
ILR0001 21277	VICTORY LITHOGRAPH INC	39 W 433 HIGHLAND	ELGIN	KANE	IL	60123	42.038061/-88 .285658
ILR0001 18539	VISION	2425 ALFT LN	ELGIN	KANE	IL	60123	42.07477/-88. 33845
ILR0001 79580	WALGREENS 13591	1435 RANDALL RD STE 101	ELGIN	KANE	IL	60123	42.07077/-88. 33543
ILR0001 04174	WALMART 1814	1001 N RANDALL RD	ELGIN	KANE	IL	60123	42.0529/-88.3 3736
ILR0001 67353	WALMART SUPERCENTER 1814	1100 S RANDALL RD	ELGIN	KANE	IL	60123	42.004274/-88 .33627
ILR0001 54591	WASHINGTON ELEMENTARY SCHOOL	819 W CHICAGO ST	ELGIN	KANE	IL	60123	42.03399/-88. 2986
ILR0001 13498	WEILER ENGINEERING INC	1395 GATEWAY DR	ELGIN	COOK	IL	60123	42.06684/-88. 34212
ILD9848 81110	WESTSIDE AUTO BODY	215 W CHICAGO ST	ELGIN	KANE	IL	60123	42.03594/-88. 28768
ILD0925 45557	WILLIAMS HEALTHCARE SYSTEMS LLC	158 N EDISON AVE	ELGIN	KANE	IL	60123	42.03722/-88. 30492
ILR0000 53793	WING PARK MAINT BLDG	WING ST	ELGIN	KANE	IL	60123	42.04743/-88. 30453
ILR0001 91023	WISDOM ADHESIVES	1500 SCOTTSDALE CT	ELGIN	KANE	IL	60123	42.07223/-88. 30567
ILD1139 76856	XPO LOGISTICS FREIGHT XEJ	4150 2ND ST	ELGIN	KANE	IL	60123	42.094161/-88 .278565
ILR0001 84689	XPO LOGISTICS FREIGHT XJO	1950 TERMINAL CT	JOLIET	WILL	IL	60123	/
IL00003 51700	YOUNGS CLEANERS & TAILORS	744 W CHICAGO	ELGIN	KANE	IL	60123	42.034676/-88 .296138
NJ00007 63623	SAFETY KLEEN CORP	777 BIG TIMBER RD	ELGIN	MIDD LESE X	NJ	60123	42.05804/-88. 29992



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ALPHABET SHOP INC
 Handler ID: ILD984922203
 300 E ELGIN AVE
 ELGIN, IL 60120

County Name: KANE

Latitude: 42.01663
 Longitude: -88.27438

Hazardous Waste Generator: Conditionally Exempt Small
 Quantity Generator



**You can navigate within the map with your mouse.*

Owner Name: ALPHABET SHOP INC

No BIENNIAL REPORT data is available for the facility listed above.

LIST OF FACILITY CONTACTS

NAME	STREET	CITY	STATE	ZIP CODE	PHONE	TYPE OF CONTACT
SHELDON BERNSTEIN	300 E ELGIN AVE	ELGIN	IL	60120	8478883150	Public
SHELDON BERNSTEIN	300 E ELGIN AVE	ELGIN	IL	60120	8478883150	Permit

HANDLER / FACILITY CLASSIFICATION

Unspecified Universe for the facility listed above.

HANDLER TYPE
Conditionally Exempt Small Quantity Generator

No PROCESS INFORMATION is available for the facility listed above.

No NAICS Codes are available for the facility listed above.

LIST OF WASTE CODES AND DESCRIPTIONS

WASTE CODE	WASTE DESCRIPTION
D001	IGNITABLE WASTE
D035	METHYL ETHYL KETONE

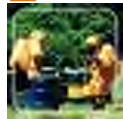
F003	THE FOLLOWING SPENT NONHALOGENATED SOLVENTS: XYLENE, ACETONE, ETHYL ACETATE, ETHYL BENZENE, ETHYL ETHER, METHYL ISOBUTYL KETONE, N-BUTYL ALCOHOL, CYCLOHEXANONE, AND METHANOL; ALL SPENT SOLVENT MIXTURES/BLENDS CONTAINING, BEFORE USE, ONLY THE ABOVE SPENT NONHALOGENATED SOLVENTS; AND ALL SPENT SOLVENT MIXTURES/BLENDS CONTAINING, BEFORE USE, ONE OR MORE OF THE ABOVE NONHALOGENATED SOLVENTS, AND A TOTAL OF TEN PERCENT OR MORE (BY VOLUME) OF ONE OR MORE OF THOSE SOLVENTS LISTED IN F001, F002, F004, AND F005; AND STILL BOTTOMS FROM THE RECOVERY OF THESE SPENT SOLVENTS AND SPENT SOLVENT MIXTURES.
F005	THE FOLLOWING SPENT NONHALOGENATED SOLVENTS: TOLUENE, METHYL ETHYL KETONE, CARBON DISULFIDE, ISOBUTANOL, PYRIDINE, BENZENE, 2-ETHOXYETHANOL, AND 2-NITROPROPANE; ALL SPENT SOLVENT MIXTURES/BLENDS CONTAINING, BEFORE USE, A TOTAL OF TEN PERCENT OR MORE (BY VOLUME) OF ONE OR MORE OF THE ABOVE NONHALOGENATED SOLVENTS OR THOSE SOLVENTS LISTED IN F001, F002, OR F004; AND STILL BOTTOMS FROM THE RECOVERY OF THESE SPENT SOLVENTS AND SPENT SOLVENT MIXTURES.

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ELGIN SANITARY DISTRICT CITY OF

Handler ID: ILD000672311
 RAYMOND ST & PURIFY DR
 ELGIN, IL 60120

County Name: KANE

Latitude: 42.01511
 Longitude: -88.27357

Hazardous Waste Generator:

Owner Name: NAME NOT REPORTED



*You can navigate within the map with your mouse.

No BIENNIAL REPORT data is available for the facility listed above.

LIST OF FACILITY CONTACTS

NAME	STREET	CITY	STATE	ZIP CODE	PHONE	TYPE OF CONTACT
ALBIN PAGORSKI	PO BOX 92 RAYMOND ST/PURIFY DR	ELGIN	IL	60120	3127422068	Public
ALBIN PAGORSKI	PO BOX 92 RAYMOND ST/PURIFY DR	ELGIN	IL	60120	3127422068	Permit

HANDLER / FACILITY CLASSIFICATION

Unspecified Universe for the facility listed above.

HANDLER TYPE

Not in a universe

No PROCESS INFORMATION is available for the facility listed above.

LIST OF NAICS CODES AND DESCRIPTIONS

NAICS CODE	NAICS DESCRIPTION
22132	SEWAGE TREATMENT FACILITIES

LIST OF WASTE CODES AND DESCRIPTIONS

WASTE CODE	WASTE DESCRIPTION
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D006

CADMIUM

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Total Number of Facilities Retrieved: 1

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IEPA ID	Site Name	Street	City	Zip
0314385009 (/land/srp/srp_sites.asp?IEPAID=0314385009)	Lords One Hour Martinizing	830-844 Summitt Street	Elgin	60120
0314385018 (/land/srp/srp_sites.asp?IEPAID=0314385018)	Rosen Kia of Elgin	845 East Chicago Street	Elgin	60120
0314385066 (/land/srp/srp_sites.asp?IEPAID=0314385066)	777 Cleaners	823 Summit Street	Elgin	60120
0314385082 (/land/srp/srp_sites.asp?IEPAID=0314385082)	Singles Roofing Company	345 Willard Avenue	Elgin	60120
0314385473 (/land/srp/srp_sites.asp?IEPAID=0314385473)	Elgin Shales, LLC	769 Shales Parkway	Elgin	60120
0314385489 (/land/srp/srp_sites.asp?IEPAID=0314385489)	Elgin Rifle Club	405 Ramona Avenue	Elgin	60120
0894380007 (/land/srp/srp_sites.asp?IEPAID=0894380007)	DSM Desoto, Inc.	1122 St. Charles Street	Elgin	60120
0894380010 (/land/srp/srp_sites.asp?IEPAID=0894380010)	Connector Service Corporation	970-1000 East Chicago Street	Elgin	60120
0894380033 (/land/srp/srp_sites.asp?IEPAID=0894380033)	Sherman Hospital	934 Center Street	Elgin	60120
0894383007 (/land/srp/srp_sites.asp?IEPAID=0894383007)	Elgin Salvage & Supply Company, Inc.	464 McBride Street	Elgin	60120
0894385054 (/land/srp/srp_sites.asp?IEPAID=0894385054)	Olympic Controls Corporation	161 South Grove Avenue	Elgin	60120
0894385081 (/land/srp/srp_sites.asp?IEPAID=0894385081)	W.R. Meadows,	2 Kimball Street	Elgin	60120

	Inc.			
0894385120 (/land/srp/srp_sites.asp?IEPAID=0894385120)	Elgin Corrugated Box	824 Raymond Street	Elgin	60120
0894385127 (/land/srp/srp_sites.asp?IEPAID=0894385127)	Elgin, City of	30-40 Ann Street	Elgin	60120
0894385218 (/land/srp/srp_sites.asp?IEPAID=0894385218)	Crest Photo Labs	955 Brandt Drive	Elgin	60120
0894385467 (/land/srp/srp_sites.asp?IEPAID=0894385467)	Based Dry Cleaners	805 Summit Street	Elgin	60120
0894385480 (/land/srp/srp_sites.asp?IEPAID=0894385480)	Shoe Factory	630 Congdon Road	Elgin	60120
0894385514 (/land/srp/srp_sites.asp?IEPAID=0894385514)	Rye Home & Building	520 McBride Street	Elgin	60120
0894385515 (/land/srp/srp_sites.asp?IEPAID=0894385515)	Rye Home & Building	502 North Grove Avenue	Elgin	60120
0894385624 (/land/srp/srp_sites.asp?IEPAID=0894385624)	Crocker Theater	96-122 South Grove Avenue	Elgin	60120
0894385625 (/land/srp/srp_sites.asp?IEPAID=0894385625)	Festival Park	136 South Grove Avenue	Elgin	60120
0894385652 (/land/srp/srp_sites.asp?IEPAID=0894385652)	Elgin Child and Family Resource Center	210 National Street	Elgin	60120
0894385681 (/land/srp/srp_sites.asp?IEPAID=0894385681)	Elgin Salvage Yard- Jefferson Avenue	20 Jefferson Avenue	Elgin	60120
0894385694 (/land/srp/srp_sites.asp?IEPAID=0894385694)	Russ' Automotive	970 North Liberty Street	Elgin	60120
0894385718 (/land/srp/srp_sites.asp?IEPAID=0894385718)	Enbridge Energy Pipeline Release	1301 Bluff City Boulevard	Elgin	60120

Site Search Results

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IEPA ID	Site Name	Street	City	Zip
0894380012 (/land/srp/srp_sites.asp?IEPAID=0894380012)	Fox Valley Manufacturing	1600 Fleetwood Drive	Elgin	60123
0894385030 (/land/srp/srp_sites.asp?IEPAID=0894385030)	Elgiloy	1565 Fleetwood Drive	Elgin	60123
0894385045 (/land/srp/srp_sites.asp?IEPAID=0894385045)	Bridgeport Harig Products	1875 Big Timber Road	Elgin	60123
0894385066 (/land/srp/srp_sites.asp?IEPAID=0894385066)	West Side One Hour Cleaners	315 South McLean Boulevard	Elgin	60123
0894385069 (/land/srp/srp_sites.asp?IEPAID=0894385069)	B&B Cleaners	835 Walnut Avenue	Elgin	60123
0894385099 (/land/srp/srp_sites.asp?IEPAID=0894385099)	Elgin Industries, Inc.	620 Wing Street	Elgin	60123
0894385115 (/land/srp/srp_sites.asp?IEPAID=0894385115)	Elgin Mental Health Center	750 South State Street	Elgin	60123
0894385176 (/land/srp/srp_sites.asp?IEPAID=0894385176)	McLean Amoco	338 South McLean Boulevard	Elgin	60123
0894385226 (/land/srp/srp_sites.asp?IEPAID=0894385226)	ComEd TDC 570	20 Walnut Avenue	Elgin	60123
0894385239 (/land/srp/srp_sites.asp?IEPAID=0894385239)	Matsushita Electric Corporation of America	1707 North Randall Road	Elgin	60123
0894385462 (/land/srp/srp_sites.asp?IEPAID=0894385462)	Young's Cleaners	744 West Chicago Street	Elgin	60123
0894385484 (/land/srp/srp_sites.asp?IEPAID=0894385484)	Gibbon America II Corporation	801 North State Street	Elgin	60123
0894385548 (/land/srp/srp_sites.asp?IEPAID=0894385548)	New Randall Cleaners	265 South Randall	Elgin	60123

		Road		
0894385593 (/land/srp/srp_sites.asp?IEPAID=0894385593)	Elgin Community College	1779 Fleetwood Drive	Elgin	60123
0894385635 (/land/srp/srp_sites.asp?IEPAID=0894385635)	Eagles Club	1600 Eagle Road	Elgin	60123
0894385676 (/land/srp/srp_sites.asp?IEPAID=0894385676)	Reliance Tool & Manufacturing Company	900 North State Street	Elgin	60123

Appendix G Census Data



S0103

POPULATION 65 YEARS AND OVER IN THE UNITED STATES

2010-2014 American Community Survey 5-Year Estimates

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Data and Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities and towns and estimates of housing units for states and counties.

Subject	Elgin city, Illinois			
	Total		65 years and over	
	Estimate	Margin of Error	Estimate	Margin of Error
Total population	110,906	+/-806	10,482	+/-627
SEX AND AGE				
Male	50.1%	+/-0.7	41.6%	+/-2.6
Female	49.9%	+/-0.7	58.4%	+/-2.6
Median age (years)	32.9	+/-0.6	73.3	+/-1.0
RACE AND HISPANIC OR LATINO ORIGIN				
One race	97.6%	+/-0.5	99.6%	+/-0.3
White	65.5%	+/-2.0	88.9%	+/-2.2
Black or African American	6.9%	+/-0.9	4.4%	+/-1.3
American Indian and Alaska Native	0.5%	+/-0.3	0.3%	+/-0.4
Asian	6.2%	+/-0.9	4.7%	+/-1.6
Native Hawaiian and Other Pacific Islander	0.0%	+/-0.1	0.0%	+/-0.3
Some other race	18.5%	+/-1.7	1.4%	+/-0.8
Two or more races	2.4%	+/-0.5	0.4%	+/-0.3
Hispanic or Latino origin (of any race)	44.4%	+/-1.6	12.4%	+/-2.8
White alone, not Hispanic or Latino	41.3%	+/-1.6	78.3%	+/-2.8
RELATIONSHIP				
Population in households	109,024	+/-905	9,861	+/-614
Householder or spouse	48.5%	+/-0.8	82.5%	+/-3.5
Parent	1.9%	+/-0.4	10.1%	+/-2.8
Other relatives	44.9%	+/-1.0	5.0%	+/-1.7
Nonrelatives	4.7%	+/-0.6	2.3%	+/-1.1
Unmarried partner	2.1%	+/-0.3	1.0%	+/-0.6
HOUSEHOLDS BY TYPE				
Households	34,755	+/-608	6,089	+/-513
Family households	71.7%	+/-1.7	52.1%	+/-4.2
Married-couple family	52.7%	+/-2.0	40.3%	+/-4.6
Female householder, no husband present, family	12.2%	+/-1.3	6.7%	+/-2.1
Nonfamily households	28.3%	+/-1.7	47.9%	+/-4.2
Householder living alone	23.4%	+/-1.6	45.9%	+/-4.1

Subject	Elgin city, Illinois			
	Total		65 years and over	
	Estimate	Margin of Error	Estimate	Margin of Error
MARITAL STATUS				
Population 15 years and over	84,152	+/-1,008	10,482	+/-627
Now married, except separated	50.3%	+/-1.7	49.0%	+/-4.1
Widowed	5.4%	+/-0.5	31.9%	+/-3.5
Divorced	9.0%	+/-0.8	13.4%	+/-2.7
Separated	1.8%	+/-0.4	0.8%	+/-0.6
Never married	33.5%	+/-1.4	4.8%	+/-1.7
EDUCATIONAL ATTAINMENT				
Population 25 years and over	68,766	+/-991	10,482	+/-627
Less than high school graduate	21.4%	+/-1.5	21.1%	+/-2.5
High school graduate, GED, or alternative	26.5%	+/-1.4	31.9%	+/-3.2
Some college or associate's degree	28.1%	+/-1.2	23.8%	+/-3.1
Bachelor's degree or higher	24.0%	+/-1.5	23.2%	+/-3.3
RESPONSIBILITY FOR GRANDCHILDREN UNDER 18 YEARS				
Population 30 years and over	60,863	+/-1,061	10,482	+/-627
Living with grandchild(ren)	6.4%	+/-0.9	6.0%	+/-2.0
Responsible for grandchild(ren)	1.9%	+/-0.5	0.4%	+/-0.5
VETERAN STATUS				
Civilian population 18 years and over	79,095	+/-901	10,482	+/-627
Civilian veteran	5.4%	+/-0.6	16.7%	+/-2.4
DISABILITY STATUS				
Civilian noninstitutionalized population	109,787	+/-874	9,878	+/-612
With any disability	8.8%	+/-0.8	34.2%	+/-3.0
No disability	91.2%	+/-0.8	65.8%	+/-3.0
RESIDENCE 1 YEAR AGO				
Population 1 year and over	109,371	+/-844	10,482	+/-627
Same house	86.6%	+/-1.4	93.3%	+/-2.2
Different house in the United States	13.1%	+/-1.4	5.9%	+/-2.0
Same county	8.1%	+/-1.1	3.0%	+/-1.7
Different county	5.0%	+/-0.7	2.9%	+/-1.2
Same state	3.7%	+/-0.7	1.8%	+/-1.0
Different state	1.3%	+/-0.4	1.1%	+/-0.8
Abroad	0.2%	+/-0.1	0.8%	+/-0.9
PLACE OF BIRTH, NATIVITY AND CITIZENSHIP STATUS, AND YEAR OF ENTRY				
Total population	110,906	+/-806	10,482	+/-627
Native	82,036	+/-1,753	8,571	+/-562
Foreign born	28,870	+/-1,533	1,911	+/-380
Entered 2010 or later	3.4%	+/-1.2	2.7%	+/-2.8
Entered 2000 to 2009	28.4%	+/-3.5	9.7%	+/-6.2
Entered before 2000	68.2%	+/-3.6	87.5%	+/-6.4
Naturalized U.S. citizen	34.8%	+/-3.2	81.8%	+/-7.5
Not a U.S. citizen	65.2%	+/-3.2	18.2%	+/-7.5
LANGUAGE SPOKEN AT HOME AND ABILITY TO SPEAK ENGLISH				
Population 5 years and over	100,363	+/-1,074	10,482	+/-627
English only	53.3%	+/-1.9	78.2%	+/-2.9
Language other than English	46.7%	+/-1.9	21.8%	+/-2.9
Speak English less than "very well"	26.3%	+/-1.6	13.5%	+/-2.5
EMPLOYMENT STATUS				

Subject	Elgin city, Illinois			
	Total		65 years and over	
	Estimate	Margin of Error	Estimate	Margin of Error
Civilian population 16 years and over	82,355	+/-985	10,482	+/-627
In labor force	71.0%	+/-1.1	20.8%	+/-2.7
Employed	64.2%	+/-1.3	20.2%	+/-2.7
Unemployed	6.8%	+/-0.8	0.6%	+/-0.3
Percent of civilian labor force	9.6%	+/-1.1	2.9%	+/-1.7
Not in labor force	29.0%	+/-1.1	79.2%	+/-2.7
INCOME IN THE PAST 12 MONTHS (IN 2014 INFLATION-ADJUSTED DOLLARS)				
Households	34,755	+/-608	6,089	+/-513
With earnings	85.3%	+/-1.1	42.3%	+/-4.1
Mean earnings (dollars)	72,220	+/-2,629	46,008	+/-5,937
With Social Security income	23.9%	+/-1.4	89.6%	+/-2.9
Mean Social Security income (dollars)	17,608	+/-711	19,602	+/-1,055
With Supplemental Security Income	4.0%	+/-0.9	4.2%	+/-1.7
Mean Supplemental Security Income (dollars)	10,689	+/-1,587	11,304	+/-2,736
With cash public assistance income	2.4%	+/-0.5	1.4%	+/-1.2
Mean cash public assistance income (dollars)	4,513	+/-1,136	2,208	+/-2,717
With retirement income	12.7%	+/-1.3	43.9%	+/-4.7
Mean retirement income (dollars)	23,920	+/-2,729	24,473	+/-3,535
With Food Stamp/SNAP benefits	14.1%	+/-1.4	8.1%	+/-2.2
POVERTY STATUS IN THE PAST 12 MONTHS				
Population for whom poverty status is determined	108,869	+/-905	9,878	+/-612
Below 100 percent of the poverty level	14.4%	+/-1.8	7.6%	+/-2.1
100 to 149 percent of the poverty level	11.3%	+/-1.3	8.3%	+/-2.1
At or above 150 percent of the poverty level	74.3%	+/-2.1	84.1%	+/-2.6
Occupied housing units	34,755	+/-608	6,089	+/-513
HOUSING TENURE				
Owner-occupied housing units	67.9%	+/-1.6	83.6%	+/-3.1
Renter-occupied housing units	32.1%	+/-1.6	16.4%	+/-3.1
Average household size of owner-occupied unit	3.08	+/-0.07	1.88	+/-0.10
Average household size of renter-occupied unit	3.26	+/-0.14	1.47	+/-0.16
SELECTED CHARACTERISTICS				
No telephone service available	1.9%	+/-0.5	0.9%	+/-0.7
1.01 or more occupants per room	6.1%	+/-0.9	0.6%	+/-0.6
Owner-occupied housing units	23,610	+/-760	5,092	+/-475
SELECTED MONTHLY OWNER COSTS AS A PERCENTAGE OF HOUSEHOLD INCOME IN THE PAST 12 MONTHS				
Less than 30 percent	63.9%	+/-2.3	62.0%	+/-5.1
30 percent or more	36.1%	+/-2.3	38.0%	+/-5.1
OWNER CHARACTERISTICS				
Median value (dollars)	171,000	+/-3,777	179,900	+/-9,134
Median selected monthly owner costs with a mortgage (dollars)	1,745	+/-31	1,614	+/-118
Median selected monthly owner costs without a mortgage (dollars)	659	+/-21	629	+/-41
Renter-occupied housing units	11,145	+/-558	997	+/-207
GROSS RENT AS A PERCENTAGE OF HOUSEHOLD INCOME IN THE PAST 12 MONTHS				
Less than 30 percent	45.8%	+/-3.2	33.8%	+/-8.7
30 percent or more	54.2%	+/-3.2	66.2%	+/-8.7
GROSS RENT				

Subject	Elgin city, Illinois			
	Total		65 years and over	
	Estimate	Margin of Error	Estimate	Margin of Error
Median gross rent (dollars)	971	+/-26	708	+/-95

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The 65 years and over column of data refers to the age of the householder for the estimates of households, occupied housing units, owner-occupied housing units, and renter-occupied housing units lines.

The age specified on the population 15 years and over, population 25 years and over, population 30 years and over, civilian population 18 years and over, civilian population 5 years and over, population 1 years and over, population 5 years and over, and population 16 years and over lines refer to the data shown in the "Total" column while the second column is limited to the population 65 years and over.

Methodological changes to data collection in 2013 may have affected language data for 2013. Users should be aware of these changes when using multi-year data containing data from 2013.

The Census Bureau introduced a new set of disability questions in the 2008 ACS questionnaire. Accordingly, comparisons of disability data from 2008 or later with data from prior years are not recommended. For more information on these questions and their evaluation in the 2006 ACS Content Test, see the Evaluation Report Covering Disability.

Telephone service data are not available for certain geographic areas due to problems with data collection. See Errata Note #93 for details.

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Estimates of urban and rural population, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Source: U.S. Census Bureau, 2010-2014 American Community Survey 5-Year Estimates

Explanation of Symbols:

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S1101

HOUSEHOLDS AND FAMILIES

2010-2014 American Community Survey 5-Year Estimates

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Data and Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities and towns and estimates of housing units for states and counties.

Subject	Elgin city, Illinois				
	Total		Married-couple family household		Male householder, no wife present, family household
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total households	34,755	+/-608	18,324	+/-658	2,353
Average household size	3.14	+/-0.05	3.92	+/-0.08	3.67
FAMILIES					
Total families	24,932	+/-552	18,324	+/-658	2,353
Average family size	3.77	+/-0.06	3.89	+/-0.08	3.24
AGE OF OWN CHILDREN					
Households with own children under 18 years	13,112	+/-561	9,615	+/-568	898
Under 6 years only	25.3%	+/-2.8	24.9%	+/-3.4	32.2%
Under 6 years and 6 to 17 years	25.3%	+/-3.4	27.1%	+/-4.1	16.4%
6 to 17 years only	49.4%	+/-3.5	48.0%	+/-4.4	51.4%
Total households	34,755	+/-608	18,324	+/-658	2,353
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	41.6%	+/-1.7	56.0%	+/-2.1	50.8%
Households with one or more people 60 years and over	31.1%	+/-1.5	27.6%	+/-2.2	29.7%
Householder living alone	23.4%	+/-1.6	(X)	(X)	(X)
65 years and over	8.0%	+/-1.1	(X)	(X)	(X)
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	0.5%	+/-0.3	(X)	(X)	(X)
Opposite sex	6.0%	+/-0.9	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	72.5%	+/-1.4	83.2%	+/-2.0	68.4%
2-or-more-unit structures	25.6%	+/-1.4	15.5%	+/-2.0	31.6%
Mobile homes and all other types of units	2.0%	+/-0.5	1.3%	+/-0.6	0.0%
HOUSING TENURE					
Owner-occupied housing units	67.9%	+/-1.6	78.9%	+/-2.2	45.6%

Subject	Elgin city, Illinois				
	Total		Married-couple family household		Male householder, no wife present, family household
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Renter-occupied housing units	32.1%	+/-1.6	21.1%	+/-2.2	54.4%

Subject	Elgin city, Illinois				
	Male householder, no wife present, family household	Female householder, no husband present, family household		Nonfamily household	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Total households	+/-440	4,255	+/-451	9,823	+/-681
Average household size	+/-0.26	3.83	+/-0.17	1.25	+/-0.04
FAMILIES					
Total families	+/-440	4,255	+/-451	(X)	(X)
Average family size	+/-0.24	3.57	+/-0.15	(X)	(X)
AGE OF OWN CHILDREN					
Households with own children under 18 years	+/-244	2,599	+/-378	(X)	(X)
Under 6 years only	+/-13.3	24.4%	+/-6.4	(X)	(X)
Under 6 years and 6 to 17 years	+/-10.3	21.8%	+/-6.0	(X)	(X)
6 to 17 years only	+/-13.5	53.8%	+/-6.9	(X)	(X)
Total households	+/-440	4,255	+/-451	9,823	+/-681
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	+/-9.3	67.7%	+/-4.9	1.0%	+/-0.6
Households with one or more people 60 years and over	+/-8.2	20.8%	+/-4.8	42.4%	+/-3.2
Householder living alone	(X)	(X)	(X)	82.7%	+/-2.7
65 years and over	(X)	(X)	(X)	28.4%	+/-3.0
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	(X)	(X)	(X)	(X)	(X)
Opposite sex	(X)	(X)	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	+/-7.4	62.0%	+/-5.4	57.9%	+/-3.0
2-or-more-unit structures	+/-7.4	37.2%	+/-5.3	37.8%	+/-2.8
Mobile homes and all other types of units	+/-1.2	0.8%	+/-0.8	4.3%	+/-1.1
HOUSING TENURE					
Owner-occupied housing units	+/-8.8	45.6%	+/-6.0	62.5%	+/-3.5
Renter-occupied housing units	+/-8.8	54.4%	+/-6.0	37.5%	+/-3.5

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see Accuracy of the Data). The effect of nonsampling error is not represented in these tables.

Average family size is derived by dividing the number of related people in households by the number of family households.

Housing unit weight is used throughout this table (only exception is the average household and family size cells).

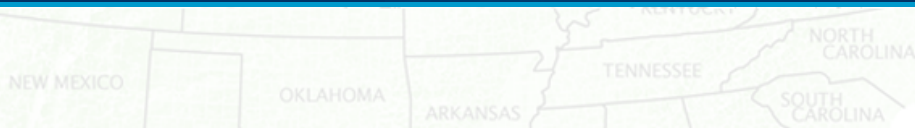
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Estimates of urban and rural population, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Source: U.S. Census Bureau, 2010-2014 American Community Survey 5-Year Estimates

Explanation of Symbols:

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B25077

MEDIAN VALUE (DOLLARS)

Universe: Owner-occupied housing units
2010-2014 American Community Survey 5-Year Estimates

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Data and Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

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	Elgin city, Illinois	
	Estimate	Margin of Error
Median value (dollars)	171,000	+/-3,777

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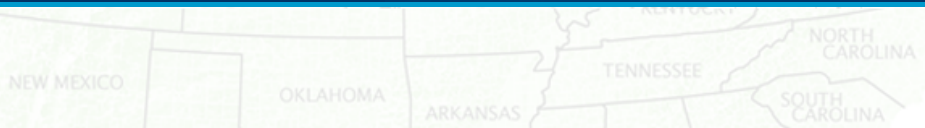
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B25062

AGGREGATE RENT ASKED (DOLLARS)

Universe: Vacant-for-rent and rented, not occupied housing units
2010-2014 American Community Survey 5-Year Estimates

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Data and Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities and towns and estimates of housing units for states and counties.

	Elgin city, Illinois	
	Estimate	Margin of Error
Aggregate rent asked	468,900	+/-145,009

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Source: U.S. Census Bureau, 2010-2014 American Community Survey 5-Year Estimates

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B25061

RENT ASKED

Universe: Vacant-for-rent and rented, not occupied housing units
2010-2014 American Community Survey 5-Year Estimates

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Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

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	Elgin city, Illinois	
	Estimate	Margin of Error
Total:	513	+/-148
Less than \$100	0	+/-26
\$100 to \$149	0	+/-26
\$150 to \$199	0	+/-26
\$200 to \$249	0	+/-26
\$250 to \$299	0	+/-26
\$300 to \$349	0	+/-26
\$350 to \$399	0	+/-26
\$400 to \$449	26	+/-39
\$450 to \$499	0	+/-26
\$500 to \$549	26	+/-32
\$550 to \$599	11	+/-19
\$600 to \$649	47	+/-55
\$650 to \$699	12	+/-19
\$700 to \$749	47	+/-47
\$750 to \$799	27	+/-31
\$800 to \$899	90	+/-83
\$900 to \$999	77	+/-67
\$1,000 to \$1,249	125	+/-88
\$1,250 to \$1,499	8	+/-13
\$1,500 to \$1,999	0	+/-26
\$2,000 or more	17	+/-29

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S1810

DISABILITY CHARACTERISTICS

2010-2014 American Community Survey 5-Year Estimates

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Subject	Elgin city, Illinois				
	Total		With a disability		Percent with a disability Estimate
	Estimate	Margin of Error	Estimate	Margin of Error	
Total civilian noninstitutionalized population	109,787	+/-874	9,629	+/-845	8.8%
Population under 5 years	10,543	+/-765	15	+/-23	0.1%
With a hearing difficulty	(X)	(X)	9	+/-14	0.1%
With a vision difficulty	(X)	(X)	15	+/-23	0.1%
Population 5 to 17 years	21,247	+/-916	927	+/-240	4.4%
With a hearing difficulty	(X)	(X)	120	+/-108	0.6%
With a vision difficulty	(X)	(X)	162	+/-120	0.8%
With a cognitive difficulty	(X)	(X)	719	+/-226	3.4%
With an ambulatory difficulty	(X)	(X)	80	+/-103	0.4%
With a self-care difficulty	(X)	(X)	236	+/-152	1.1%
Population 18 to 64 years	68,119	+/-994	5,307	+/-598	7.8%
With a hearing difficulty	(X)	(X)	1,174	+/-279	1.7%
With a vision difficulty	(X)	(X)	735	+/-196	1.1%
With a cognitive difficulty	(X)	(X)	1,890	+/-321	2.8%
With an ambulatory difficulty	(X)	(X)	2,708	+/-416	4.0%
With a self-care difficulty	(X)	(X)	951	+/-246	1.4%
With an independent living difficulty	(X)	(X)	1,740	+/-339	2.6%
Population 65 years and over	9,878	+/-612	3,380	+/-374	34.2%
With a hearing difficulty	(X)	(X)	1,116	+/-200	11.3%
With a vision difficulty	(X)	(X)	544	+/-175	5.5%
With a cognitive difficulty	(X)	(X)	852	+/-221	8.6%
With an ambulatory difficulty	(X)	(X)	2,450	+/-376	24.8%
With a self-care difficulty	(X)	(X)	801	+/-212	8.1%
With an independent living difficulty	(X)	(X)	1,649	+/-298	16.7%
SEX					
Male	55,049	+/-830	4,491	+/-587	8.2%
Female	54,738	+/-874	5,138	+/-497	9.4%
RACE AND HISPANIC OR LATINO ORIGIN					
One Race	107,128	+/-969	9,405	+/-807	8.8%

Subject	Elgin city, Illinois				
	Total		With a disability		Percent with a disability
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
White alone	71,891	+/-2,308	7,243	+/-656	10.1%
Black or African American alone	7,353	+/-965	1,006	+/-293	13.7%
American Indian and Alaska Native alone	582	+/-321	38	+/-48	6.5%
Asian alone	6,826	+/-991	356	+/-153	5.2%
Native Hawaiian and Other Pacific Islander alone	0	+/-26	0	+/-26	-
Some other race alone	20,476	+/-1,839	762	+/-298	3.7%
Two or more races	2,659	+/-508	224	+/-141	8.4%
White alone, not Hispanic or Latino	45,149	+/-1,867	5,822	+/-608	12.9%
Hispanic or Latino (of any race)	49,192	+/-1,645	2,309	+/-444	4.7%
PERCENT IMPUTED					
Disability status	6.5%	(X)	(X)	(X)	(X)
Hearing difficulty	5.1%	(X)	(X)	(X)	(X)
Vision difficulty	5.3%	(X)	(X)	(X)	(X)
Cognitive difficulty	5.4%	(X)	(X)	(X)	(X)
Ambulatory difficulty	5.6%	(X)	(X)	(X)	(X)
Self-care difficulty	5.5%	(X)	(X)	(X)	(X)
Independent living difficulty	5.1%	(X)	(X)	(X)	(X)

Subject	Elgin city, Illinois
	Percent with a disability
	Margin of Error
Total civilian noninstitutionalized population	+/-0.8
Population under 5 years	+/-0.2
With a hearing difficulty	+/-0.1
With a vision difficulty	+/-0.2
Population 5 to 17 years	+/-1.1
With a hearing difficulty	+/-0.5
With a vision difficulty	+/-0.6
With a cognitive difficulty	+/-1.1
With an ambulatory difficulty	+/-0.5
With a self-care difficulty	+/-0.7
Population 18 to 64 years	+/-0.9
With a hearing difficulty	+/-0.4
With a vision difficulty	+/-0.3
With a cognitive difficulty	+/-0.5
With an ambulatory difficulty	+/-0.6
With a self-care difficulty	+/-0.4
With an independent living difficulty	+/-0.5
Population 65 years and over	+/-3.0
With a hearing difficulty	+/-1.9
With a vision difficulty	+/-1.8
With a cognitive difficulty	+/-2.2
With an ambulatory difficulty	+/-3.2
With a self-care difficulty	+/-2.0
With an independent living difficulty	+/-2.7
SEX	
Male	+/-1.0
Female	+/-0.9
RACE AND HISPANIC OR LATINO ORIGIN	
One Race	+/-0.7
White alone	+/-0.8
Black or African American alone	+/-3.5
American Indian and Alaska Native alone	+/-6.8
Asian alone	+/-2.1
Native Hawaiian and Other Pacific Islander alone	**
Some other race alone	+/-1.4
Two or more races	+/-4.9
White alone, not Hispanic or Latino	+/-1.2
Hispanic or Latino (of any race)	+/-0.9
PERCENT IMPUTED	
Disability status	(X)
Hearing difficulty	(X)
Vision difficulty	(X)
Cognitive difficulty	(X)
Ambulatory difficulty	(X)
Self-care difficulty	(X)
Independent living difficulty	(X)

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The Census Bureau introduced a new set of disability questions in the 2008 ACS questionnaire. Accordingly, comparisons of disability data from 2008 or later with data from prior years are not recommended. For more information on these questions and their evaluation in the 2006 ACS Content Test, see the Evaluation Report Covering Disability.

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Estimates of urban and rural population, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Source: U.S. Census Bureau, 2010-2014 American Community Survey 5-Year Estimates

Explanation of Symbols:

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Race & Ethnicity

Census Tract	Block Group	Total Population	Minority %	White Alone	Black or African American Alone	American Indian or Alaska Native Alone	Asian Alone	Native Hawaiian or Pacific Islander Alone	Some other race alone	Two or more races	Hispanic or Latino	Not Hispanic or Latino
8513.01	Block Group 2	2528	79.7%	514	31	12	67	0	85	17	1802	726
8514	Block Group 1	1256	87.1%	162	110	0	24	0	0	11	949	307
8514	Block Group 4	707	72.0%	198	30	0	0	0	0	53	426	281
8514	Block Group 5	771	80.4%	151	107	0	0	0	0	0	513	258
8514	Block Group 6	1339	68.4%	423	65	0	0	0	0	10	841	498
8515	Block Group 1	1085	53.7%	502	38	0	38	0	0	11	496	589
8516	Block Group 1	2066	64.6%	731	303	8	16	0	0	90	918	1148
8516	Block Group 2	859	35.7%	552	0	0	10	0	0	0	297	562
8516	Block Group 3	1392	65.4%	481	95	0	0	0	0	79	737	655
8516	Block Group 4	987	29.0%	701	0	0	79	0	0	16	191	796
8518.01	Block Group 2	1334	19.2%	1078	18	0	0	0	0	0	238	1096
8546	Block Group 2	1381	77.7%	308	206	0	67	0	0	83	717	664
8549	Block Group 1	371	73.3%	99	202	0	0	0	0	8	62	309
8549	Block Group 2	1172	32.5%	791	91	0	144	0	0	0	146	1026
Total		17248	61.2%									

Source: 2014 5-Year ACS Survey Data, retrieved from Topologically Integrated Geographic Encoding and Referencing electronic database

POVERTY

Census Tract	Block Group	Percent	Pop for whom							
		Below Poverty Level	Poverty Status is determined	under .5	.5 to .99	1.0 to 1.24	1.25 to 1.49	1.5 to 1.84	1.84 to 1.99	Over 2.0
8513.01	Block Group 2	15.1%	2528	208	173	49	627	14	63	1394
8514	Block Group 1	5.4%	1166	26	37	86	379	115	84	439
8514	Block Group 4	22.8%	623	72	70	7	36	272	0	166
8514	Block Group 5	7.4%	771	36	21	118	193	18	0	385
8514	Block Group 6	24.2%	1321	172	148	153	18	59	71	700
8515	Block Group 1	29.8%	1047	144	168	25	60	268	40	342
8516	Block Group 1	30.7%	1980	309	298	392	78	256	0	647
8516	Block Group 2	3.4%	859	21	8	135	105	10	0	580
8516	Block Group 3	41.2%	1392	403	170	96	0	149	0	574
8516	Block Group 4	10.5%	987	104	0	0	0	74	0	809
8518.01	Block Group 2	2.2%	1334	19	11	68	0	31	11	1194
8546	Block Group 2	30.2%	1373	94	320	121	15	104	0	719
8549	Block Group 1	100.0%	8	0	8	0	0	0	0	0
8549	Block Group 2	7.0%	1172	29	53	0	52	115	17	906
		18.9%	16561							

Source: 2014 5-Year ACS Survey Data, retrieved from Topologically Integrated Geographic Encoding and Referencing electronic database



B25077

MEDIAN VALUE (DOLLARS)

Universe: Owner-occupied housing units
2010-2014 American Community Survey 5-Year Estimates

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Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

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	Census Tract 8514, Kane County, Illinois		Census Tract 8515, Kane County, Illinois		Census Tract 8516, Kane County, Illinois
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Median value (dollars)	124,000	+/-12,711	115,000	+/-32,270	134,200

	Census Tract 8516, Kane County, Illinois	Census Tract 8518.01, Kane County, Illinois		Census Tract 8549, Kane County, Illinois	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Median value (dollars)	+/-15,491	186,100	+/-8,510	164,100	+/-8,176

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Source: U.S. Census Bureau, 2010-2014 American Community Survey 5-Year Estimates

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B25077

MEDIAN VALUE (DOLLARS)

Universe: Owner-occupied housing units
2010-2014 American Community Survey 5-Year Estimates

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	Census Tract 8513.01, Kane County, Illinois		Census Tract 8546, Kane County, Illinois	
	Estimate	Margin of Error	Estimate	Margin of Error
Median value (dollars)	153,000	+/-16,282	182,800	+/-14,911

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B25075

VALUE

Universe: Owner-occupied housing units
2010-2014 American Community Survey 5-Year Estimates

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	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total:	1,155	+/-128	124	+/-37	1,080
Less than \$10,000	6	+/-9	0	+/-11	0
\$10,000 to \$14,999	0	+/-15	0	+/-11	9
\$15,000 to \$19,999	0	+/-15	0	+/-11	0
\$20,000 to \$24,999	0	+/-15	0	+/-11	0
\$25,000 to \$29,999	0	+/-15	0	+/-11	16
\$30,000 to \$34,999	0	+/-15	0	+/-11	0
\$35,000 to \$39,999	6	+/-10	0	+/-11	0
\$40,000 to \$49,999	42	+/-48	0	+/-11	0
\$50,000 to \$59,999	56	+/-49	0	+/-11	0
\$60,000 to \$69,999	98	+/-63	0	+/-11	33
\$70,000 to \$79,999	56	+/-40	0	+/-11	0
\$80,000 to \$89,999	40	+/-35	0	+/-11	53
\$90,000 to \$99,999	47	+/-37	47	+/-41	43
\$100,000 to \$124,999	236	+/-91	25	+/-28	315
\$125,000 to \$149,999	153	+/-62	13	+/-9	193
\$150,000 to \$174,999	165	+/-72	21	+/-20	228
\$175,000 to \$199,999	146	+/-81	0	+/-11	114
\$200,000 to \$249,999	72	+/-45	10	+/-14	65
\$250,000 to \$299,999	32	+/-29	8	+/-12	11
\$300,000 to \$399,999	0	+/-15	0	+/-11	0
\$400,000 to \$499,999	0	+/-15	0	+/-11	0
\$500,000 to \$749,999	0	+/-15	0	+/-11	0
\$750,000 to \$999,999	0	+/-15	0	+/-11	0
\$1,000,000 or more	0	+/-15	0	+/-11	0

	Census Tract 8516, Kane County, Illinois	Census Tract 8518.01, Kane County, Illinois		Census Tract 8549, Kane County, Illinois	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Total:	+/-162	2,283	+/-167	1,140	+/-141
Less than \$10,000	+/-15	80	+/-61	8	+/-13
\$10,000 to \$14,999	+/-15	9	+/-16	0	+/-15
\$15,000 to \$19,999	+/-15	10	+/-18	10	+/-15
\$20,000 to \$24,999	+/-15	26	+/-32	0	+/-15
\$25,000 to \$29,999	+/-26	0	+/-15	0	+/-15
\$30,000 to \$34,999	+/-15	0	+/-15	0	+/-15
\$35,000 to \$39,999	+/-15	0	+/-15	0	+/-15
\$40,000 to \$49,999	+/-15	0	+/-15	0	+/-15
\$50,000 to \$59,999	+/-15	0	+/-15	0	+/-15
\$60,000 to \$69,999	+/-39	23	+/-32	0	+/-15
\$70,000 to \$79,999	+/-15	0	+/-15	15	+/-17
\$80,000 to \$89,999	+/-66	0	+/-15	5	+/-9
\$90,000 to \$99,999	+/-33	62	+/-75	58	+/-68
\$100,000 to \$124,999	+/-119	103	+/-56	243	+/-106
\$125,000 to \$149,999	+/-144	284	+/-139	71	+/-54
\$150,000 to \$174,999	+/-111	332	+/-117	283	+/-87
\$175,000 to \$199,999	+/-65	478	+/-156	159	+/-77
\$200,000 to \$249,999	+/-40	515	+/-139	220	+/-90
\$250,000 to \$299,999	+/-18	222	+/-108	48	+/-41
\$300,000 to \$399,999	+/-15	139	+/-57	7	+/-11
\$400,000 to \$499,999	+/-15	0	+/-15	0	+/-15
\$500,000 to \$749,999	+/-15	0	+/-15	0	+/-15
\$750,000 to \$999,999	+/-15	0	+/-15	13	+/-20
\$1,000,000 or more	+/-15	0	+/-15	0	+/-15

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Source: U.S. Census Bureau, 2010-2014 American Community Survey 5-Year Estimates

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B25075

VALUE

Universe: Owner-occupied housing units
2010-2014 American Community Survey 5-Year Estimates

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	Census Tract 8513.01, Kane County, Illinois		Census Tract 8546, Kane County, Illinois	
	Estimate	Margin of Error	Estimate	Margin of Error
Total:	532	+/-96	474	+/-102
Less than \$10,000	9	+/-16	6	+/-11
\$10,000 to \$14,999	0	+/-11	10	+/-16
\$15,000 to \$19,999	0	+/-11	0	+/-11
\$20,000 to \$24,999	0	+/-11	7	+/-12
\$25,000 to \$29,999	0	+/-11	0	+/-11
\$30,000 to \$34,999	0	+/-11	0	+/-11
\$35,000 to \$39,999	0	+/-11	0	+/-11
\$40,000 to \$49,999	0	+/-11	0	+/-11
\$50,000 to \$59,999	0	+/-11	0	+/-11
\$60,000 to \$69,999	3	+/-8	0	+/-11
\$70,000 to \$79,999	31	+/-29	0	+/-11
\$80,000 to \$89,999	0	+/-11	0	+/-11
\$90,000 to \$99,999	18	+/-21	0	+/-11
\$100,000 to \$124,999	127	+/-64	50	+/-44
\$125,000 to \$149,999	65	+/-45	46	+/-47
\$150,000 to \$174,999	107	+/-51	82	+/-47
\$175,000 to \$199,999	31	+/-37	116	+/-74
\$200,000 to \$249,999	102	+/-47	107	+/-70
\$250,000 to \$299,999	39	+/-32	20	+/-22
\$300,000 to \$399,999	0	+/-11	10	+/-14
\$400,000 to \$499,999	0	+/-11	20	+/-25
\$500,000 to \$749,999	0	+/-11	0	+/-11
\$750,000 to \$999,999	0	+/-11	0	+/-11
\$1,000,000 or more	0	+/-11	0	+/-11

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B25065

AGGREGATE GROSS RENT (DOLLARS)

Universe: Renter-occupied housing units paying cash rent
2010-2014 American Community Survey 5-Year Estimates

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	Census Tract 8514, Kane County, Illinois		Census Tract 8515, Kane County, Illinois		Census Tract 8516, Kane County, Illinois
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Aggregate gross rent	571,600	+/-133,244	194,000	+/-40,452	761,700

	Census Tract 8516, Kane County, Illinois	Census Tract 8518.01, Kane County, Illinois		Census Tract 8549, Kane County, Illinois	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Aggregate gross rent	+/-174,774	301,300	+/-127,007	595,100	+/-138,840

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see Accuracy of the Data). The effect of nonsampling error is not represented in these tables.

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Estimates of urban and rural population, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Source: U.S. Census Bureau, 2010-2014 American Community Survey 5-Year Estimates

Explanation of Symbols:

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2. An '-' entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution.
3. An '-' following a median estimate means the median falls in the lowest interval of an open-ended distribution.
4. An '+' following a median estimate means the median falls in the upper interval of an open-ended distribution.
5. An '****' entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.
6. An '*****' entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.
7. An 'N' entry in the estimate and margin of error columns indicates that data for this geographic area cannot be displayed because the number of sample cases is too small.
8. An '(X)' means that the estimate is not applicable or not available.



B25060

AGGREGATE CONTRACT RENT (DOLLARS)

Universe: Renter-occupied housing units paying cash rent
2010-2014 American Community Survey 5-Year Estimates

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Data and Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities and towns and estimates of housing units for states and counties.

	Census Tract 8513.01, Kane County, Illinois		Census Tract 8546, Kane County, Illinois	
	Estimate	Margin of Error	Estimate	Margin of Error
Aggregate contract rent	496,800	+/-108,954	612,900	+/-119,945

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see Accuracy of the Data). The effect of nonsampling error is not represented in these tables.

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Estimates of urban and rural population, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Source: U.S. Census Bureau, 2010-2014 American Community Survey 5-Year Estimates

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ARIZON
NEW MEXICO

OKLAHOMA

ARKANSAS

TENNESSEE

NORTH CAROLINA

SOUTH CAROLINA

B25063

GROSS RENT

Universe: Renter-occupied housing units
2010-2014 American Community Survey 5-Year Estimates

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	Census Tract 8514, Kane County, Illinois		Census Tract 8515, Kane County, Illinois		Census Tract 8516, Kane County, Illinois
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total:	586	+/-118	254	+/-45	1,120
With cash rent:	566	+/-121	250	+/-44	1,038
Less than \$100	0	+/-15	0	+/-11	0
\$100 to \$149	0	+/-15	0	+/-11	0
\$150 to \$199	0	+/-15	0	+/-11	16
\$200 to \$249	19	+/-29	0	+/-11	68
\$250 to \$299	0	+/-15	0	+/-11	86
\$300 to \$349	0	+/-15	21	+/-31	76
\$350 to \$399	21	+/-32	0	+/-11	0
\$400 to \$449	0	+/-15	0	+/-11	45
\$450 to \$499	7	+/-12	0	+/-11	17
\$500 to \$549	7	+/-17	0	+/-11	0
\$550 to \$599	0	+/-15	0	+/-11	0
\$600 to \$649	7	+/-12	16	+/-19	75
\$650 to \$699	13	+/-14	51	+/-37	101
\$700 to \$749	51	+/-42	34	+/-33	74
\$750 to \$799	28	+/-25	32	+/-23	82
\$800 to \$899	24	+/-21	46	+/-50	44
\$900 to \$999	68	+/-51	33	+/-28	26
\$1,000 to \$1,249	151	+/-91	6	+/-9	278
\$1,250 to \$1,499	170	+/-80	11	+/-13	14
\$1,500 to \$1,999	0	+/-15	0	+/-11	18
\$2,000 or more	0	+/-15	0	+/-11	18
No cash rent	20	+/-23	4	+/-5	82

	Census Tract 8516, Kane County, Illinois	Census Tract 8518.01, Kane County, Illinois		Census Tract 8549, Kane County, Illinois	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Total:	+/-167	310	+/-127	796	+/-135
With cash rent:	+/-173	290	+/-123	771	+/-138
Less than \$100	+/-15	0	+/-15	0	+/-15
\$100 to \$149	+/-15	0	+/-15	10	+/-15
\$150 to \$199	+/-26	0	+/-15	29	+/-21
\$200 to \$249	+/-56	0	+/-15	13	+/-21
\$250 to \$299	+/-85	0	+/-15	89	+/-82
\$300 to \$349	+/-76	0	+/-15	39	+/-49
\$350 to \$399	+/-15	0	+/-15	34	+/-39
\$400 to \$449	+/-72	0	+/-15	31	+/-29
\$450 to \$499	+/-30	0	+/-15	55	+/-53
\$500 to \$549	+/-15	0	+/-15	8	+/-13
\$550 to \$599	+/-15	30	+/-49	0	+/-15
\$600 to \$649	+/-68	33	+/-51	0	+/-15
\$650 to \$699	+/-79	30	+/-35	56	+/-54
\$700 to \$749	+/-80	10	+/-16	21	+/-32
\$750 to \$799	+/-70	11	+/-19	30	+/-49
\$800 to \$899	+/-48	20	+/-30	32	+/-40
\$900 to \$999	+/-32	34	+/-42	22	+/-20
\$1,000 to \$1,249	+/-157	13	+/-21	232	+/-100
\$1,250 to \$1,499	+/-23	80	+/-63	57	+/-56
\$1,500 to \$1,999	+/-29	29	+/-33	13	+/-20
\$2,000 or more	+/-29	0	+/-15	0	+/-15
No cash rent	+/-76	20	+/-24	25	+/-24

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B25056

CONTRACT RENT

Universe: Renter-occupied housing units
2010-2014 American Community Survey 5-Year Estimates

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	Census Tract 8513.01, Kane County, Illinois		Census Tract 8546, Kane County, Illinois	
	Estimate	Margin of Error	Estimate	Margin of Error
Total:	618	+/-111	819	+/-124
With cash rent:	603	+/-116	804	+/-125
Less than \$100	0	+/-11	0	+/-11
\$100 to \$149	0	+/-11	0	+/-11
\$150 to \$199	0	+/-11	33	+/-38
\$200 to \$249	0	+/-11	0	+/-11
\$250 to \$299	0	+/-11	11	+/-19
\$300 to \$349	0	+/-11	13	+/-20
\$350 to \$399	0	+/-11	0	+/-11
\$400 to \$449	11	+/-18	20	+/-33
\$450 to \$499	20	+/-22	46	+/-56
\$500 to \$549	32	+/-35	80	+/-49
\$550 to \$599	36	+/-41	60	+/-46
\$600 to \$649	83	+/-50	56	+/-42
\$650 to \$699	65	+/-45	72	+/-46
\$700 to \$749	41	+/-34	84	+/-58
\$750 to \$799	70	+/-48	56	+/-47
\$800 to \$899	73	+/-49	79	+/-55
\$900 to \$999	83	+/-61	17	+/-26
\$1,000 to \$1,249	35	+/-30	91	+/-55
\$1,250 to \$1,499	32	+/-31	38	+/-35
\$1,500 to \$1,999	7	+/-12	38	+/-39
\$2,000 or more	15	+/-25	10	+/-16
No cash rent	15	+/-16	15	+/-24

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S1810

DISABILITY CHARACTERISTICS

2010-2014 American Community Survey 5-Year Estimates

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Subject	Census Tract 8513.01, Kane County, Illinois				
	Total		With a disability		Percent with a disability Estimate
	Estimate	Margin of Error	Estimate	Margin of Error	
Total civilian noninstitutionalized population	4,252	+/-475	348	+/-138	8.2%
Population under 5 years	404	+/-125	0	+/-11	0.0%
With a hearing difficulty	(X)	(X)	0	+/-11	0.0%
With a vision difficulty	(X)	(X)	0	+/-11	0.0%
Population 5 to 17 years	935	+/-219	35	+/-38	3.7%
With a hearing difficulty	(X)	(X)	0	+/-11	0.0%
With a vision difficulty	(X)	(X)	0	+/-11	0.0%
With a cognitive difficulty	(X)	(X)	35	+/-38	3.7%
With an ambulatory difficulty	(X)	(X)	0	+/-11	0.0%
With a self-care difficulty	(X)	(X)	0	+/-11	0.0%
Population 18 to 64 years	2,719	+/-304	222	+/-123	8.2%
With a hearing difficulty	(X)	(X)	82	+/-54	3.0%
With a vision difficulty	(X)	(X)	29	+/-36	1.1%
With a cognitive difficulty	(X)	(X)	57	+/-43	2.1%
With an ambulatory difficulty	(X)	(X)	119	+/-86	4.4%
With a self-care difficulty	(X)	(X)	10	+/-15	0.4%
With an independent living difficulty	(X)	(X)	31	+/-32	1.1%
Population 65 years and over	194	+/-73	91	+/-48	46.9%
With a hearing difficulty	(X)	(X)	19	+/-23	9.8%
With a vision difficulty	(X)	(X)	9	+/-13	4.6%
With a cognitive difficulty	(X)	(X)	19	+/-19	9.8%
With an ambulatory difficulty	(X)	(X)	53	+/-38	27.3%
With a self-care difficulty	(X)	(X)	10	+/-16	5.2%
With an independent living difficulty	(X)	(X)	39	+/-26	20.1%
SEX					
Male	2,021	+/-323	195	+/-107	9.6%
Female	2,231	+/-246	153	+/-76	6.9%
RACE AND HISPANIC OR LATINO ORIGIN					
One Race	4,147	+/-491	325	+/-136	7.8%

Subject	Census Tract 8513.01, Kane County, Illinois				
	Total		With a disability		Percent with a disability
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
White alone	2,705	+/-527	246	+/-127	9.1%
Black or African American alone	205	+/-139	35	+/-38	17.1%
American Indian and Alaska Native alone	35	+/-39	0	+/-11	0.0%
Asian alone	96	+/-69	11	+/-18	11.5%
Native Hawaiian and Other Pacific Islander alone	0	+/-11	0	+/-11	-
Some other race alone	1,106	+/-518	33	+/-35	3.0%
Two or more races	105	+/-77	23	+/-31	21.9%
White alone, not Hispanic or Latino	825	+/-218	125	+/-85	15.2%
Hispanic or Latino (of any race)	3,003	+/-483	160	+/-105	5.3%
PERCENT IMPUTED					
Disability status	5.8%	(X)	(X)	(X)	(X)
Hearing difficulty	4.2%	(X)	(X)	(X)	(X)
Vision difficulty	4.7%	(X)	(X)	(X)	(X)
Cognitive difficulty	4.3%	(X)	(X)	(X)	(X)
Ambulatory difficulty	4.3%	(X)	(X)	(X)	(X)
Self-care difficulty	4.4%	(X)	(X)	(X)	(X)
Independent living difficulty	5.6%	(X)	(X)	(X)	(X)

Subject	Census Tract 8513.01, Kane County, Illinois Percent with a disability	Census Tract 8514, Kane County, Illinois			
		Total		With a disability	
		Margin of Error	Estimate	Margin of Error	Estimate
Total civilian noninstitutionalized population	+/-3.4	6,742	+/-491	302	+/-99
Population under 5 years	+/-6.6	645	+/-143	6	+/-17
With a hearing difficulty	+/-6.6	(X)	(X)	0	+/-15
With a vision difficulty	+/-6.6	(X)	(X)	6	+/-17
Population 5 to 17 years	+/-4.1	1,433	+/-250	28	+/-31
With a hearing difficulty	+/-2.9	(X)	(X)	5	+/-10
With a vision difficulty	+/-2.9	(X)	(X)	0	+/-15
With a cognitive difficulty	+/-4.1	(X)	(X)	23	+/-29
With an ambulatory difficulty	+/-2.9	(X)	(X)	0	+/-15
With a self-care difficulty	+/-2.9	(X)	(X)	0	+/-15
Population 18 to 64 years	+/-4.4	4,312	+/-350	165	+/-66
With a hearing difficulty	+/-2.1	(X)	(X)	28	+/-23
With a vision difficulty	+/-1.3	(X)	(X)	35	+/-47
With a cognitive difficulty	+/-1.6	(X)	(X)	86	+/-54
With an ambulatory difficulty	+/-3.0	(X)	(X)	110	+/-61
With a self-care difficulty	+/-0.6	(X)	(X)	35	+/-28
With an independent living difficulty	+/-1.2	(X)	(X)	87	+/-56
Population 65 years and over	+/-19.8	352	+/-67	103	+/-55
With a hearing difficulty	+/-10.3	(X)	(X)	36	+/-28
With a vision difficulty	+/-6.9	(X)	(X)	25	+/-23
With a cognitive difficulty	+/-9.7	(X)	(X)	54	+/-44
With an ambulatory difficulty	+/-17.8	(X)	(X)	68	+/-44
With a self-care difficulty	+/-8.1	(X)	(X)	28	+/-30
With an independent living difficulty	+/-11.1	(X)	(X)	83	+/-53
SEX					
Male	+/-5.4	3,161	+/-307	165	+/-82
Female	+/-3.3	3,581	+/-308	137	+/-58
RACE AND HISPANIC OR LATINO ORIGIN					
One Race	+/-3.4	6,539	+/-501	296	+/-99
White alone	+/-4.7	3,546	+/-582	242	+/-99
Black or African American alone	+/-12.4	390	+/-176	34	+/-31
American Indian and Alaska Native alone	+/-46.1	0	+/-15	0	+/-15
Asian alone	+/-17.5	87	+/-83	9	+/-14
Native Hawaiian and Other Pacific Islander alone	**	0	+/-15	0	+/-15
Some other race alone	+/-3.3	2,516	+/-645	11	+/-17
Two or more races	+/-25.6	203	+/-136	6	+/-10
White alone, not Hispanic or Latino	+/-9.6	1,365	+/-281	190	+/-92
Hispanic or Latino (of any race)	+/-3.5	4,750	+/-466	63	+/-57
PERCENT IMPUTED					
Disability status	(X)	3.8%	(X)	(X)	(X)
Hearing difficulty	(X)	3.0%	(X)	(X)	(X)
Vision difficulty	(X)	3.1%	(X)	(X)	(X)
Cognitive difficulty	(X)	3.1%	(X)	(X)	(X)
Ambulatory difficulty	(X)	3.2%	(X)	(X)	(X)
Self-care difficulty	(X)	3.1%	(X)	(X)	(X)
Independent living difficulty	(X)	3.7%	(X)	(X)	(X)

Subject	Census Tract 8514, Kane County, Illinois		Census Tract 8515, Kane County, Illinois		
	Percent with a disability		Total		With a disability
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total civilian noninstitutionalized population	4.5%	+/-1.5	1,085	+/-129	97
Population under 5 years	0.9%	+/-2.6	180	+/-68	0
With a hearing difficulty	0.0%	+/-4.2	(X)	(X)	0
With a vision difficulty	0.9%	+/-2.6	(X)	(X)	0
Population 5 to 17 years	2.0%	+/-2.1	207	+/-96	34
With a hearing difficulty	0.3%	+/-0.7	(X)	(X)	0
With a vision difficulty	0.0%	+/-1.9	(X)	(X)	0
With a cognitive difficulty	1.6%	+/-2.0	(X)	(X)	29
With an ambulatory difficulty	0.0%	+/-1.9	(X)	(X)	0
With a self-care difficulty	0.0%	+/-1.9	(X)	(X)	0
Population 18 to 64 years	3.8%	+/-1.5	614	+/-104	36
With a hearing difficulty	0.6%	+/-0.6	(X)	(X)	5
With a vision difficulty	0.8%	+/-1.1	(X)	(X)	0
With a cognitive difficulty	2.0%	+/-1.2	(X)	(X)	10
With an ambulatory difficulty	2.6%	+/-1.4	(X)	(X)	21
With a self-care difficulty	0.8%	+/-0.7	(X)	(X)	0
With an independent living difficulty	2.0%	+/-1.3	(X)	(X)	0
Population 65 years and over	29.3%	+/-14.9	84	+/-43	27
With a hearing difficulty	10.2%	+/-8.1	(X)	(X)	9
With a vision difficulty	7.1%	+/-6.6	(X)	(X)	0
With a cognitive difficulty	15.3%	+/-11.7	(X)	(X)	0
With an ambulatory difficulty	19.3%	+/-12.2	(X)	(X)	27
With a self-care difficulty	8.0%	+/-8.2	(X)	(X)	0
With an independent living difficulty	23.6%	+/-14.2	(X)	(X)	9
SEX					
Male	5.2%	+/-2.5	578	+/-94	77
Female	3.8%	+/-1.7	507	+/-110	20
RACE AND HISPANIC OR LATINO ORIGIN					
One Race	4.5%	+/-1.5	1,074	+/-131	93
White alone	6.8%	+/-2.6	840	+/-175	93
Black or African American alone	8.7%	+/-7.8	38	+/-36	0
American Indian and Alaska Native alone	-	**	6	+/-9	0
Asian alone	10.3%	+/-14.2	38	+/-63	0
Native Hawaiian and Other Pacific Islander alone	-	**	0	+/-11	0
Some other race alone	0.4%	+/-0.7	152	+/-112	0
Two or more races	3.0%	+/-6.0	11	+/-14	4
White alone, not Hispanic or Latino	13.9%	+/-6.2	502	+/-138	54
Hispanic or Latino (of any race)	1.3%	+/-1.2	496	+/-156	39
PERCENT IMPUTED					
Disability status	(X)	(X)	1.3%	(X)	(X)
Hearing difficulty	(X)	(X)	0.4%	(X)	(X)
Vision difficulty	(X)	(X)	0.5%	(X)	(X)
Cognitive difficulty	(X)	(X)	0.0%	(X)	(X)
Ambulatory difficulty	(X)	(X)	0.6%	(X)	(X)
Self-care difficulty	(X)	(X)	0.6%	(X)	(X)
Independent living difficulty	(X)	(X)	0.0%	(X)	(X)

Subject	Census Tract 8515, Kane County, Illinois			Census Tract 8516, Kane County, Illinois	
	With a disability	Percent with a disability		Total	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Total civilian noninstitutionalized population	+/-57	8.9%	+/-5.0	6,353	+/-636
Population under 5 years	+/-11	0.0%	+/-14.1	514	+/-206
With a hearing difficulty	+/-11	0.0%	+/-14.1	(X)	(X)
With a vision difficulty	+/-11	0.0%	+/-14.1	(X)	(X)
Population 5 to 17 years	+/-37	16.4%	+/-13.6	1,648	+/-337
With a hearing difficulty	+/-11	0.0%	+/-12.4	(X)	(X)
With a vision difficulty	+/-11	0.0%	+/-12.4	(X)	(X)
With a cognitive difficulty	+/-35	14.0%	+/-13.0	(X)	(X)
With an ambulatory difficulty	+/-11	0.0%	+/-12.4	(X)	(X)
With a self-care difficulty	+/-11	0.0%	+/-12.4	(X)	(X)
Population 18 to 64 years	+/-27	5.9%	+/-4.5	3,787	+/-406
With a hearing difficulty	+/-8	0.8%	+/-1.3	(X)	(X)
With a vision difficulty	+/-11	0.0%	+/-4.4	(X)	(X)
With a cognitive difficulty	+/-13	1.6%	+/-2.0	(X)	(X)
With an ambulatory difficulty	+/-22	3.4%	+/-3.6	(X)	(X)
With a self-care difficulty	+/-11	0.0%	+/-4.4	(X)	(X)
With an independent living difficulty	+/-11	0.0%	+/-4.4	(X)	(X)
Population 65 years and over	+/-21	32.1%	+/-16.0	404	+/-141
With a hearing difficulty	+/-13	10.7%	+/-15.1	(X)	(X)
With a vision difficulty	+/-11	0.0%	+/-27.3	(X)	(X)
With a cognitive difficulty	+/-11	0.0%	+/-27.3	(X)	(X)
With an ambulatory difficulty	+/-21	32.1%	+/-16.0	(X)	(X)
With a self-care difficulty	+/-11	0.0%	+/-27.3	(X)	(X)
With an independent living difficulty	+/-13	10.7%	+/-15.1	(X)	(X)
SEX					
Male	+/-48	13.3%	+/-8.8	3,465	+/-527
Female	+/-20	3.9%	+/-3.9	2,888	+/-322
RACE AND HISPANIC OR LATINO ORIGIN					
One Race	+/-56	8.7%	+/-5.0	6,099	+/-654
White alone	+/-56	11.1%	+/-6.4	4,766	+/-636
Black or African American alone	+/-11	0.0%	+/-44.3	529	+/-230
American Indian and Alaska Native alone	+/-11	0.0%	+/-100.0	8	+/-14
Asian alone	+/-11	0.0%	+/-44.3	105	+/-109
Native Hawaiian and Other Pacific Islander alone	+/-11	-	**	0	+/-15
Some other race alone	+/-11	0.0%	+/-16.5	691	+/-477
Two or more races	+/-8	36.4%	+/-57.1	254	+/-126
White alone, not Hispanic or Latino	+/-36	10.8%	+/-8.4	2,619	+/-419
Hispanic or Latino (of any race)	+/-41	7.9%	+/-7.3	2,894	+/-760
PERCENT IMPUTED					
Disability status	(X)	(X)	(X)	8.6%	(X)
Hearing difficulty	(X)	(X)	(X)	6.8%	(X)
Vision difficulty	(X)	(X)	(X)	7.1%	(X)
Cognitive difficulty	(X)	(X)	(X)	7.2%	(X)
Ambulatory difficulty	(X)	(X)	(X)	7.4%	(X)
Self-care difficulty	(X)	(X)	(X)	7.2%	(X)
Independent living difficulty	(X)	(X)	(X)	7.6%	(X)

Subject	Census Tract 8516, Kane County, Illinois				Census Tract 8518.01, Kane County, Illinois
	With a disability		Percent with a disability		Total
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total civilian noninstitutionalized population	521	+/-219	8.2%	+/-3.5	7,700
Population under 5 years	0	+/-15	0.0%	+/-5.2	518
With a hearing difficulty	0	+/-15	0.0%	+/-5.2	(X)
With a vision difficulty	0	+/-15	0.0%	+/-5.2	(X)
Population 5 to 17 years	119	+/-102	7.2%	+/-6.2	1,396
With a hearing difficulty	0	+/-15	0.0%	+/-1.7	(X)
With a vision difficulty	0	+/-15	0.0%	+/-1.7	(X)
With a cognitive difficulty	119	+/-102	7.2%	+/-6.2	(X)
With an ambulatory difficulty	0	+/-15	0.0%	+/-1.7	(X)
With a self-care difficulty	0	+/-15	0.0%	+/-1.7	(X)
Population 18 to 64 years	314	+/-137	8.3%	+/-3.7	5,076
With a hearing difficulty	51	+/-43	1.3%	+/-1.1	(X)
With a vision difficulty	54	+/-69	1.4%	+/-1.9	(X)
With a cognitive difficulty	99	+/-79	2.6%	+/-2.1	(X)
With an ambulatory difficulty	76	+/-54	2.0%	+/-1.4	(X)
With a self-care difficulty	31	+/-35	0.8%	+/-0.9	(X)
With an independent living difficulty	112	+/-89	3.0%	+/-2.4	(X)
Population 65 years and over	88	+/-60	21.8%	+/-14.7	710
With a hearing difficulty	17	+/-27	4.2%	+/-7.0	(X)
With a vision difficulty	0	+/-15	0.0%	+/-6.6	(X)
With a cognitive difficulty	17	+/-27	4.2%	+/-7.0	(X)
With an ambulatory difficulty	88	+/-60	21.8%	+/-14.7	(X)
With a self-care difficulty	35	+/-39	8.7%	+/-9.8	(X)
With an independent living difficulty	35	+/-39	8.7%	+/-9.8	(X)
SEX					
Male	256	+/-142	7.4%	+/-4.0	3,832
Female	265	+/-128	9.2%	+/-4.5	3,868
RACE AND HISPANIC OR LATINO ORIGIN					
One Race	507	+/-216	8.3%	+/-3.6	7,664
White alone	356	+/-159	7.5%	+/-3.5	7,047
Black or African American alone	18	+/-30	3.4%	+/-5.9	144
American Indian and Alaska Native alone	0	+/-15	0.0%	+/-96.4	0
Asian alone	0	+/-15	0.0%	+/-22.8	406
Native Hawaiian and Other Pacific Islander alone	0	+/-15	-	**	0
Some other race alone	133	+/-163	19.2%	+/-20.8	67
Two or more races	14	+/-24	5.5%	+/-8.9	36
White alone, not Hispanic or Latino	338	+/-155	12.9%	+/-5.5	5,813
Hispanic or Latino (of any race)	165	+/-166	5.7%	+/-5.6	1,337
PERCENT IMPUTED					
Disability status	(X)	(X)	(X)	(X)	4.4%
Hearing difficulty	(X)	(X)	(X)	(X)	2.0%
Vision difficulty	(X)	(X)	(X)	(X)	2.2%
Cognitive difficulty	(X)	(X)	(X)	(X)	3.3%
Ambulatory difficulty	(X)	(X)	(X)	(X)	3.2%
Self-care difficulty	(X)	(X)	(X)	(X)	3.3%
Independent living difficulty	(X)	(X)	(X)	(X)	3.2%

Subject	Census Tract 8518.01, Kane County, Illinois				
	Total	With a disability		Percent with a disability	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Total civilian noninstitutionalized population	+/-421	650	+/-165	8.4%	+/-2.1
Population under 5 years	+/-151	34	+/-53	6.6%	+/-9.6
With a hearing difficulty	(X)	34	+/-53	6.6%	+/-9.6
With a vision difficulty	(X)	34	+/-53	6.6%	+/-9.6
Population 5 to 17 years	+/-229	79	+/-67	5.7%	+/-4.7
With a hearing difficulty	(X)	54	+/-59	3.9%	+/-4.4
With a vision difficulty	(X)	35	+/-53	2.5%	+/-3.8
With a cognitive difficulty	(X)	60	+/-61	4.3%	+/-4.3
With an ambulatory difficulty	(X)	35	+/-53	2.5%	+/-3.8
With a self-care difficulty	(X)	35	+/-53	2.5%	+/-3.8
Population 18 to 64 years	+/-350	283	+/-101	5.6%	+/-2.0
With a hearing difficulty	(X)	99	+/-54	2.0%	+/-1.1
With a vision difficulty	(X)	0	+/-15	0.0%	+/-0.5
With a cognitive difficulty	(X)	103	+/-78	2.0%	+/-1.5
With an ambulatory difficulty	(X)	118	+/-65	2.3%	+/-1.3
With a self-care difficulty	(X)	72	+/-55	1.4%	+/-1.1
With an independent living difficulty	(X)	71	+/-54	1.4%	+/-1.1
Population 65 years and over	+/-86	254	+/-85	35.8%	+/-11.5
With a hearing difficulty	(X)	60	+/-39	8.5%	+/-5.6
With a vision difficulty	(X)	22	+/-28	3.1%	+/-3.8
With a cognitive difficulty	(X)	75	+/-41	10.6%	+/-5.5
With an ambulatory difficulty	(X)	178	+/-76	25.1%	+/-10.5
With a self-care difficulty	(X)	51	+/-32	7.2%	+/-4.4
With an independent living difficulty	(X)	146	+/-66	20.6%	+/-9.4
SEX					
Male	+/-303	295	+/-104	7.7%	+/-2.7
Female	+/-342	355	+/-98	9.2%	+/-2.6
RACE AND HISPANIC OR LATINO ORIGIN					
One Race	+/-421	643	+/-167	8.4%	+/-2.1
White alone	+/-431	575	+/-164	8.2%	+/-2.2
Black or African American alone	+/-159	0	+/-15	0.0%	+/-17.3
American Indian and Alaska Native alone	+/-15	0	+/-15	-	**
Asian alone	+/-92	68	+/-54	16.7%	+/-10.8
Native Hawaiian and Other Pacific Islander alone	+/-15	0	+/-15	-	**
Some other race alone	+/-66	0	+/-15	0.0%	+/-32.3
Two or more races	+/-38	7	+/-13	19.4%	+/-35.6
White alone, not Hispanic or Latino	+/-373	454	+/-131	7.8%	+/-2.3
Hispanic or Latino (of any race)	+/-266	121	+/-102	9.1%	+/-6.7
PERCENT IMPUTED					
Disability status	(X)	(X)	(X)	(X)	(X)
Hearing difficulty	(X)	(X)	(X)	(X)	(X)
Vision difficulty	(X)	(X)	(X)	(X)	(X)
Cognitive difficulty	(X)	(X)	(X)	(X)	(X)
Ambulatory difficulty	(X)	(X)	(X)	(X)	(X)
Self-care difficulty	(X)	(X)	(X)	(X)	(X)
Independent living difficulty	(X)	(X)	(X)	(X)	(X)

Subject	Census Tract 8546, Kane County, Illinois				
	Total		With a disability		Percent with a disability
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total civilian noninstitutionalized population	3,690	+/-411	320	+/-85	8.7%
Population under 5 years	329	+/-144	0	+/-11	0.0%
With a hearing difficulty	(X)	(X)	0	+/-11	0.0%
With a vision difficulty	(X)	(X)	0	+/-11	0.0%
Population 5 to 17 years	822	+/-238	9	+/-14	1.1%
With a hearing difficulty	(X)	(X)	9	+/-14	1.1%
With a vision difficulty	(X)	(X)	0	+/-11	0.0%
With a cognitive difficulty	(X)	(X)	0	+/-11	0.0%
With an ambulatory difficulty	(X)	(X)	0	+/-11	0.0%
With a self-care difficulty	(X)	(X)	0	+/-11	0.0%
Population 18 to 64 years	2,354	+/-253	274	+/-85	11.6%
With a hearing difficulty	(X)	(X)	47	+/-35	2.0%
With a vision difficulty	(X)	(X)	46	+/-35	2.0%
With a cognitive difficulty	(X)	(X)	116	+/-59	4.9%
With an ambulatory difficulty	(X)	(X)	164	+/-69	7.0%
With a self-care difficulty	(X)	(X)	85	+/-49	3.6%
With an independent living difficulty	(X)	(X)	108	+/-66	4.6%
Population 65 years and over	185	+/-37	37	+/-36	20.0%
With a hearing difficulty	(X)	(X)	0	+/-11	0.0%
With a vision difficulty	(X)	(X)	17	+/-27	9.2%
With a cognitive difficulty	(X)	(X)	17	+/-27	9.2%
With an ambulatory difficulty	(X)	(X)	20	+/-23	10.8%
With a self-care difficulty	(X)	(X)	0	+/-11	0.0%
With an independent living difficulty	(X)	(X)	13	+/-17	7.0%
SEX					
Male	1,998	+/-346	139	+/-52	7.0%
Female	1,692	+/-249	181	+/-69	10.7%
RACE AND HISPANIC OR LATINO ORIGIN					
One Race	3,496	+/-439	311	+/-85	8.9%
White alone	2,186	+/-547	163	+/-62	7.5%
Black or African American alone	544	+/-276	98	+/-55	18.0%
American Indian and Alaska Native alone	0	+/-11	0	+/-11	-
Asian alone	140	+/-82	22	+/-23	15.7%
Native Hawaiian and Other Pacific Islander alone	0	+/-11	0	+/-11	-
Some other race alone	626	+/-336	28	+/-31	4.5%
Two or more races	194	+/-147	9	+/-15	4.6%
White alone, not Hispanic or Latino	791	+/-201	100	+/-52	12.6%
Hispanic or Latino (of any race)	2,095	+/-490	91	+/-57	4.3%
PERCENT IMPUTED					
Disability status	4.6%	(X)	(X)	(X)	(X)
Hearing difficulty	3.0%	(X)	(X)	(X)	(X)
Vision difficulty	3.3%	(X)	(X)	(X)	(X)
Cognitive difficulty	4.2%	(X)	(X)	(X)	(X)
Ambulatory difficulty	4.4%	(X)	(X)	(X)	(X)
Self-care difficulty	4.2%	(X)	(X)	(X)	(X)
Independent living difficulty	5.0%	(X)	(X)	(X)	(X)

Subject	Census Tract 8546, Kane County, Illinois Percent with a disability	Census Tract 8549, Kane County, Illinois			
		Total		With a disability	
		Margin of Error	Estimate	Margin of Error	Estimate
Total civilian noninstitutionalized population	+/-2.6	5,104	+/-419	640	+/-225
Population under 5 years	+/-8.0	472	+/-166	0	+/-15
With a hearing difficulty	+/-8.0	(X)	(X)	0	+/-15
With a vision difficulty	+/-8.0	(X)	(X)	0	+/-15
Population 5 to 17 years	+/-1.8	868	+/-201	75	+/-62
With a hearing difficulty	+/-1.8	(X)	(X)	11	+/-24
With a vision difficulty	+/-3.3	(X)	(X)	0	+/-15
With a cognitive difficulty	+/-3.3	(X)	(X)	51	+/-70
With an ambulatory difficulty	+/-3.3	(X)	(X)	0	+/-15
With a self-care difficulty	+/-3.3	(X)	(X)	0	+/-15
Population 18 to 64 years	+/-3.9	3,326	+/-291	414	+/-172
With a hearing difficulty	+/-1.5	(X)	(X)	113	+/-104
With a vision difficulty	+/-1.5	(X)	(X)	48	+/-56
With a cognitive difficulty	+/-2.5	(X)	(X)	210	+/-108
With an ambulatory difficulty	+/-3.0	(X)	(X)	260	+/-129
With a self-care difficulty	+/-2.1	(X)	(X)	76	+/-68
With an independent living difficulty	+/-2.9	(X)	(X)	79	+/-69
Population 65 years and over	+/-18.0	438	+/-95	151	+/-57
With a hearing difficulty	+/-13.8	(X)	(X)	48	+/-34
With a vision difficulty	+/-13.8	(X)	(X)	25	+/-18
With a cognitive difficulty	+/-13.8	(X)	(X)	17	+/-21
With an ambulatory difficulty	+/-13.8	(X)	(X)	104	+/-50
With a self-care difficulty	+/-13.8	(X)	(X)	22	+/-23
With an independent living difficulty	+/-9.9	(X)	(X)	44	+/-41
SEX					
Male	+/-2.9	2,616	+/-269	231	+/-118
Female	+/-4.2	2,488	+/-279	409	+/-148
RACE AND HISPANIC OR LATINO ORIGIN					
One Race	+/-2.7	5,004	+/-434	640	+/-225
White alone	+/-3.2	3,688	+/-489	386	+/-135
Black or African American alone	+/-13.2	748	+/-343	187	+/-163
American Indian and Alaska Native alone	**	137	+/-114	0	+/-15
Asian alone	+/-12.8	207	+/-209	41	+/-43
Native Hawaiian and Other Pacific Islander alone	**	0	+/-15	0	+/-15
Some other race alone	+/-5.3	224	+/-248	26	+/-57
Two or more races	+/-7.6	100	+/-130	0	+/-15
White alone, not Hispanic or Latino	+/-6.5	3,160	+/-383	386	+/-135
Hispanic or Latino (of any race)	+/-2.9	958	+/-417	26	+/-57
PERCENT IMPUTED					
Disability status	(X)	7.9%	(X)	(X)	(X)
Hearing difficulty	(X)	5.5%	(X)	(X)	(X)
Vision difficulty	(X)	5.9%	(X)	(X)	(X)
Cognitive difficulty	(X)	7.0%	(X)	(X)	(X)
Ambulatory difficulty	(X)	7.1%	(X)	(X)	(X)
Self-care difficulty	(X)	7.2%	(X)	(X)	(X)
Independent living difficulty	(X)	7.3%	(X)	(X)	(X)

Subject	Census Tract 8549, Kane County, Illinois	
	Percent with a disability	
	Estimate	Margin of Error
Total civilian noninstitutionalized population	12.5%	+/-4.3
Population under 5 years	0.0%	+/-5.7
With a hearing difficulty	0.0%	+/-5.7
With a vision difficulty	0.0%	+/-5.7
Population 5 to 17 years	8.6%	+/-6.9
With a hearing difficulty	1.3%	+/-2.6
With a vision difficulty	0.0%	+/-3.1
With a cognitive difficulty	5.9%	+/-8.0
With an ambulatory difficulty	0.0%	+/-3.1
With a self-care difficulty	0.0%	+/-3.1
Population 18 to 64 years	12.4%	+/-5.0
With a hearing difficulty	3.4%	+/-3.1
With a vision difficulty	1.4%	+/-1.7
With a cognitive difficulty	6.3%	+/-3.1
With an ambulatory difficulty	7.8%	+/-3.8
With a self-care difficulty	2.3%	+/-2.0
With an independent living difficulty	2.4%	+/-2.0
Population 65 years and over	34.5%	+/-14.0
With a hearing difficulty	11.0%	+/-7.2
With a vision difficulty	5.7%	+/-4.1
With a cognitive difficulty	3.9%	+/-4.7
With an ambulatory difficulty	23.7%	+/-12.4
With a self-care difficulty	5.0%	+/-4.9
With an independent living difficulty	10.0%	+/-10.3
SEX		
Male	8.8%	+/-4.6
Female	16.4%	+/-5.7
RACE AND HISPANIC OR LATINO ORIGIN		
One Race	12.8%	+/-4.4
White alone	10.5%	+/-3.9
Black or African American alone	25.0%	+/-18.0
American Indian and Alaska Native alone	0.0%	+/-18.1
Asian alone	19.8%	+/-5.4
Native Hawaiian and Other Pacific Islander alone	-	**
Some other race alone	11.6%	+/-17.9
Two or more races	0.0%	+/-23.7
White alone, not Hispanic or Latino	12.2%	+/-4.4
Hispanic or Latino (of any race)	2.7%	+/-5.6
PERCENT IMPUTED		
Disability status	(X)	(X)
Hearing difficulty	(X)	(X)
Vision difficulty	(X)	(X)
Cognitive difficulty	(X)	(X)
Ambulatory difficulty	(X)	(X)
Self-care difficulty	(X)	(X)
Independent living difficulty	(X)	(X)

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of

error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see Accuracy of the Data). The effect of nonsampling error is not represented in these tables.

The Census Bureau introduced a new set of disability questions in the 2008 ACS questionnaire. Accordingly, comparisons of disability data from 2008 or later with data from prior years are not recommended. For more information on these questions and their evaluation in the 2006 ACS Content Test, see the Evaluation Report Covering Disability.

While the 2010-2014 American Community Survey (ACS) data generally reflect the February 2013 Office of Management and Budget (OMB) definitions of metropolitan and micropolitan statistical areas; in certain instances the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB definitions due to differences in the effective dates of the geographic entities.

Estimates of urban and rural population, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Source: U.S. Census Bureau, 2010-2014 American Community Survey 5-Year Estimates

Explanation of Symbols:

1. An '***' entry in the margin of error column indicates that either no sample observations or too few sample observations were available to compute a standard error and thus the margin of error. A statistical test is not appropriate.
2. An '-' entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution.
3. An '-' following a median estimate means the median falls in the lowest interval of an open-ended distribution.
4. An '+' following a median estimate means the median falls in the upper interval of an open-ended distribution.
5. An '****' entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.
6. An '*****' entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.
7. An 'N' entry in the estimate and margin of error columns indicates that data for this geographic area cannot be displayed because the number of sample cases is too small.
8. An '(X)' means that the estimate is not applicable or not available.



S1101

HOUSEHOLDS AND FAMILIES

2010-2014 American Community Survey 5-Year Estimates

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Data and Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities and towns and estimates of housing units for states and counties.

Subject	Census Tract 8513.01, Kane County, Illinois				
	Total		Married-couple family household		Male householder, no wife present, family household
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total households	1,150	+/-77	519	+/-92	107
Average household size	3.70	+/-0.35	4.79	+/-0.70	4.15
FAMILIES					
Total families	826	+/-92	519	+/-92	107
Average family size	4.54	+/-0.46	4.69	+/-0.68	3.92
AGE OF OWN CHILDREN					
Households with own children under 18 years	476	+/-93	302	+/-89	43
Under 6 years only	11.3%	+/-11.6	17.9%	+/-17.2	0.0%
Under 6 years and 6 to 17 years	26.3%	+/-12.4	24.2%	+/-15.8	60.5%
6 to 17 years only	62.4%	+/-12.7	57.9%	+/-19.4	39.5%
Total households	1,150	+/-77	519	+/-92	107
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	47.0%	+/-7.0	63.6%	+/-9.3	56.1%
Households with one or more people 60 years and over	22.8%	+/-7.0	23.7%	+/-10.5	15.9%
Householder living alone	21.7%	+/-7.1	(X)	(X)	(X)
65 years and over	4.5%	+/-3.7	(X)	(X)	(X)
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	1.0%	+/-1.5	(X)	(X)	(X)
Opposite sex	7.0%	+/-4.1	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	53.0%	+/-7.4	69.4%	+/-11.8	65.4%
2-or-more-unit structures	45.9%	+/-7.5	30.6%	+/-11.8	34.6%
Mobile homes and all other types of units	1.0%	+/-1.5	0.0%	+/-5.2	0.0%
HOUSING TENURE					
Owner-occupied housing units	46.3%	+/-8.3	70.1%	+/-12.4	31.8%

Subject	Census Tract 8513.01, Kane County, Illinois				
	Total		Married-couple family household		Male householder, no wife present, family household
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Renter-occupied housing units	53.7%	+/-8.3	29.9%	+/-12.4	68.2%

Subject	Census Tract 8513.01, Kane County, Illinois				
	Male householder, no wife present, family household	Female householder, no husband present, family household		Nonfamily household	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Total households	+/-71	200	+/-76	324	+/-87
Average household size	+/-1.12	4.54	+/-0.56	1.28	+/-0.17
FAMILIES					
Total families	+/-71	200	+/-76	(X)	(X)
Average family size	+/-1.21	4.46	+/-0.54	(X)	(X)
AGE OF OWN CHILDREN					
Households with own children under 18 years	+/-46	131	+/-66	(X)	(X)
Under 6 years only	+/-41.6	0.0%	+/-18.8	(X)	(X)
Under 6 years and 6 to 17 years	+/-57.1	19.8%	+/-22.0	(X)	(X)
6 to 17 years only	+/-57.1	80.2%	+/-22.0	(X)	(X)
Total households	+/-71	200	+/-76	324	+/-87
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	+/-33.8	75.0%	+/-14.5	0.0%	+/-8.1
Households with one or more people 60 years and over	+/-26.3	20.0%	+/-13.9	25.3%	+/-13.2
Householder living alone	(X)	(X)	(X)	76.9%	+/-13.8
65 years and over	(X)	(X)	(X)	16.0%	+/-11.9
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	(X)	(X)	(X)	(X)	(X)
Opposite sex	(X)	(X)	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	+/-34.1	28.0%	+/-16.5	38.3%	+/-17.2
2-or-more-unit structures	+/-34.1	72.0%	+/-16.5	58.0%	+/-18.2
Mobile homes and all other types of units	+/-22.4	0.0%	+/-12.8	3.7%	+/-5.2
HOUSING TENURE					
Owner-occupied housing units	+/-35.8	19.0%	+/-15.7	29.6%	+/-17.0
Renter-occupied housing units	+/-35.8	81.0%	+/-15.7	70.4%	+/-17.0

Subject	Census Tract 8514, Kane County, Illinois				
	Total		Married-couple family household		Male householder, no wife present, family household
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total households	1,741	+/-86	821	+/-120	171
Average household size	3.87	+/-0.25	4.54	+/-0.43	4.37
FAMILIES					
Total families	1,365	+/-108	821	+/-120	171
Average family size	4.24	+/-0.24	4.46	+/-0.39	3.54
AGE OF OWN CHILDREN					
Households with own children under 18 years	746	+/-94	441	+/-93	109
Under 6 years only	23.7%	+/-9.3	17.9%	+/-11.5	51.4%
Under 6 years and 6 to 17 years	23.2%	+/-10.6	22.2%	+/-13.4	16.5%
6 to 17 years only	53.1%	+/-11.3	59.9%	+/-15.0	32.1%
Total households	1,741	+/-86	821	+/-120	171
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	50.6%	+/-5.9	59.4%	+/-8.5	76.6%
Households with one or more people 60 years and over	18.3%	+/-3.5	17.2%	+/-5.6	12.3%
Householder living alone	16.4%	+/-4.2	(X)	(X)	(X)
65 years and over	5.5%	+/-2.2	(X)	(X)	(X)
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	1.4%	+/-1.6	(X)	(X)	(X)
Opposite sex	10.3%	+/-5.0	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	71.0%	+/-5.4	72.0%	+/-8.2	52.6%
2-or-more-unit structures	29.0%	+/-5.4	28.0%	+/-8.2	47.4%
Mobile homes and all other types of units	0.0%	+/-1.6	0.0%	+/-3.3	0.0%
HOUSING TENURE					
Owner-occupied housing units	66.3%	+/-6.5	75.8%	+/-8.7	34.5%
Renter-occupied housing units	33.7%	+/-6.5	24.2%	+/-8.7	65.5%

Subject	Census Tract 8514, Kane County, Illinois				
	Male householder, no wife present, family household	Female householder, no husband present, family household		Nonfamily household	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Total households	+/-73	373	+/-92	376	+/-93
Average household size	+/-0.51	4.32	+/-0.55	1.76	+/-0.62
FAMILIES					
Total families	+/-73	373	+/-92	(X)	(X)
Average family size	+/-0.44	4.07	+/-0.51	(X)	(X)
AGE OF OWN CHILDREN					
Households with own children under 18 years	+/-61	196	+/-79	(X)	(X)
Under 6 years only	+/-33.3	21.4%	+/-14.5	(X)	(X)
Under 6 years and 6 to 17 years	+/-24.1	29.1%	+/-20.5	(X)	(X)
6 to 17 years only	+/-27.6	49.5%	+/-20.6	(X)	(X)
Total households	+/-73	373	+/-92	376	+/-93
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	+/-17.2	59.0%	+/-16.4	11.2%	+/-10.3
Households with one or more people 60 years and over	+/-12.5	7.5%	+/-6.8	34.0%	+/-13.2
Householder living alone	(X)	(X)	(X)	76.1%	+/-15.0
65 years and over	(X)	(X)	(X)	25.3%	+/-10.7
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	(X)	(X)	(X)	(X)	(X)
Opposite sex	(X)	(X)	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	+/-18.3	65.4%	+/-14.0	82.7%	+/-13.2
2-or-more-unit structures	+/-18.3	34.6%	+/-14.0	17.3%	+/-13.2
Mobile homes and all other types of units	+/-14.8	0.0%	+/-7.1	0.0%	+/-7.0
HOUSING TENURE					
Owner-occupied housing units	+/-19.9	41.0%	+/-16.2	85.4%	+/-10.4
Renter-occupied housing units	+/-19.9	59.0%	+/-16.2	14.6%	+/-10.4

Subject	Census Tract 8515, Kane County, Illinois				
	Total		Married-couple family household		Male householder, no wife present, family household
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total households	378	+/-34	141	+/-44	20
Average household size	2.87	+/-0.28	3.41	+/-0.42	3.95
FAMILIES					
Total families	219	+/-40	141	+/-44	20
Average family size	3.55	+/-0.33	3.41	+/-0.42	2.95
AGE OF OWN CHILDREN					
Households with own children under 18 years	141	+/-38	85	+/-42	20
Under 6 years only	47.5%	+/-24.9	55.3%	+/-32.7	100.0%
Under 6 years and 6 to 17 years	29.8%	+/-24.1	24.7%	+/-26.3	0.0%
6 to 17 years only	22.7%	+/-14.4	20.0%	+/-17.8	0.0%
Total households	378	+/-34	141	+/-44	20
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	43.7%	+/-10.9	60.3%	+/-21.1	100.0%
Households with one or more people 60 years and over	30.2%	+/-11.9	22.7%	+/-14.1	0.0%
Householder living alone	29.1%	+/-10.0	(X)	(X)	(X)
65 years and over	8.7%	+/-6.9	(X)	(X)	(X)
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	0.0%	+/-7.0	(X)	(X)	(X)
Opposite sex	9.0%	+/-7.7	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	32.8%	+/-9.0	53.9%	+/-21.4	0.0%
2-or-more-unit structures	67.2%	+/-9.0	46.1%	+/-21.4	100.0%
Mobile homes and all other types of units	0.0%	+/-7.0	0.0%	+/-17.6	0.0%
HOUSING TENURE					
Owner-occupied housing units	32.8%	+/-9.7	53.9%	+/-21.4	0.0%
Renter-occupied housing units	67.2%	+/-9.7	46.1%	+/-21.4	100.0%

Subject	Census Tract 8515, Kane County, Illinois				
	Male householder, no wife present, family household	Female householder, no husband present, family household		Nonfamily household	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Total households	+/-22	58	+/-44	159	+/-38
Average household size	+/-0.74	4.59	+/-0.98	1.63	+/-0.49
FAMILIES					
Total families	+/-22	58	+/-44	(X)	(X)
Average family size	+/-0.63	4.10	+/-1.29	(X)	(X)
AGE OF OWN CHILDREN					
Households with own children under 18 years	+/-22	36	+/-34	(X)	(X)
Under 6 years only	+/-61.0	0.0%	+/-45.5	(X)	(X)
Under 6 years and 6 to 17 years	+/-61.0	58.3%	+/-52.0	(X)	(X)
6 to 17 years only	+/-61.0	41.7%	+/-52.0	(X)	(X)
Total households	+/-22	58	+/-44	159	+/-38
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	+/-61.0	72.4%	+/-38.7	11.3%	+/-13.2
Households with one or more people 60 years and over	+/-61.0	10.3%	+/-17.0	47.8%	+/-24.5
Householder living alone	(X)	(X)	(X)	69.2%	+/-24.4
65 years and over	(X)	(X)	(X)	20.8%	+/-17.0
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	(X)	(X)	(X)	(X)	(X)
Opposite sex	(X)	(X)	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	+/-61.0	36.2%	+/-39.1	17.0%	+/-12.9
2-or-more-unit structures	+/-61.0	63.8%	+/-39.1	83.0%	+/-12.9
Mobile homes and all other types of units	+/-61.0	0.0%	+/-35.5	0.0%	+/-15.8
HOUSING TENURE					
Owner-occupied housing units	+/-61.0	27.6%	+/-38.7	20.1%	+/-14.5
Renter-occupied housing units	+/-61.0	72.4%	+/-38.7	79.9%	+/-14.5

Subject	Census Tract 8516, Kane County, Illinois				
	Total		Married-couple family household		Male householder, no wife present, family household
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total households	2,200	+/-141	954	+/-168	175
Average household size	2.89	+/-0.26	3.96	+/-0.31	3.59
FAMILIES					
Total families	1,429	+/-209	954	+/-168	175
Average family size	3.70	+/-0.30	3.93	+/-0.30	3.38
AGE OF OWN CHILDREN					
Households with own children under 18 years	930	+/-182	687	+/-156	52
Under 6 years only	14.0%	+/-7.8	18.9%	+/-10.2	0.0%
Under 6 years and 6 to 17 years	23.3%	+/-12.4	24.6%	+/-14.4	34.6%
6 to 17 years only	62.7%	+/-13.5	56.5%	+/-15.6	65.4%
Total households	2,200	+/-141	954	+/-168	175
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	46.1%	+/-7.5	77.6%	+/-8.6	29.7%
Households with one or more people 60 years and over	32.1%	+/-7.2	17.3%	+/-9.5	41.1%
Householder living alone	31.1%	+/-7.8	(X)	(X)	(X)
65 years and over	11.9%	+/-4.8	(X)	(X)	(X)
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	0.0%	+/-1.2	(X)	(X)	(X)
Opposite sex	7.7%	+/-5.0	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	54.4%	+/-7.8	74.6%	+/-11.3	89.1%
2-or-more-unit structures	45.6%	+/-7.8	25.4%	+/-11.3	10.9%
Mobile homes and all other types of units	0.0%	+/-1.2	0.0%	+/-2.8	0.0%
HOUSING TENURE					
Owner-occupied housing units	49.1%	+/-6.7	73.5%	+/-11.8	50.3%
Renter-occupied housing units	50.9%	+/-6.7	26.5%	+/-11.8	49.7%

Subject	Census Tract 8516, Kane County, Illinois				
	Male householder, no wife present, family household	Female householder, no husband present, family household		Nonfamily household	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Total households	+/-104	300	+/-136	771	+/-189
Average household size	+/-1.19	3.54	+/-0.99	1.15	+/-0.13
FAMILIES					
Total families	+/-104	300	+/-136	(X)	(X)
Average family size	+/-1.08	3.19	+/-0.93	(X)	(X)
AGE OF OWN CHILDREN					
Households with own children under 18 years	+/-48	191	+/-126	(X)	(X)
Under 6 years only	+/-37.8	0.0%	+/-13.4	(X)	(X)
Under 6 years and 6 to 17 years	+/-46.6	15.7%	+/-23.9	(X)	(X)
6 to 17 years only	+/-46.6	84.3%	+/-23.9	(X)	(X)
Total households	+/-104	300	+/-136	771	+/-189
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	+/-25.2	74.0%	+/-26.0	0.0%	+/-3.5
Households with one or more people 60 years and over	+/-31.3	15.3%	+/-19.0	55.0%	+/-12.1
Householder living alone	(X)	(X)	(X)	88.7%	+/-9.0
65 years and over	(X)	(X)	(X)	33.9%	+/-12.2
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	(X)	(X)	(X)	(X)	(X)
Opposite sex	(X)	(X)	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	+/-17.3	52.7%	+/-27.1	22.2%	+/-9.7
2-or-more-unit structures	+/-17.3	47.3%	+/-27.1	77.8%	+/-9.7
Mobile homes and all other types of units	+/-14.5	0.0%	+/-8.8	0.0%	+/-3.5
HOUSING TENURE					
Owner-occupied housing units	+/-31.0	39.7%	+/-26.9	22.3%	+/-9.7
Renter-occupied housing units	+/-31.0	60.3%	+/-26.9	77.7%	+/-9.7

Subject	Census Tract 8518.01, Kane County, Illinois				
	Total		Married-couple family household		Male householder, no wife present, family household
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total households	2,593	+/-142	1,583	+/-187	90
Average household size	2.97	+/-0.16	3.48	+/-0.17	3.52
FAMILIES					
Total families	1,934	+/-143	1,583	+/-187	90
Average family size	3.38	+/-0.17	3.47	+/-0.17	2.84
AGE OF OWN CHILDREN					
Households with own children under 18 years	892	+/-137	767	+/-141	50
Under 6 years only	22.8%	+/-9.2	21.9%	+/-9.1	0.0%
Under 6 years and 6 to 17 years	19.7%	+/-10.7	22.9%	+/-12.0	0.0%
6 to 17 years only	57.5%	+/-10.3	55.1%	+/-11.9	100.0%
Total households	2,593	+/-142	1,583	+/-187	90
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	36.2%	+/-4.7	49.7%	+/-6.6	55.6%
Households with one or more people 60 years and over	31.2%	+/-4.4	26.5%	+/-5.9	44.4%
Householder living alone	20.4%	+/-5.3	(X)	(X)	(X)
65 years and over	6.4%	+/-2.8	(X)	(X)	(X)
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	0.0%	+/-1.1	(X)	(X)	(X)
Opposite sex	4.9%	+/-3.2	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	90.5%	+/-4.0	97.0%	+/-3.3	27.8%
2-or-more-unit structures	8.9%	+/-3.8	3.0%	+/-3.3	72.2%
Mobile homes and all other types of units	0.6%	+/-1.0	0.0%	+/-1.7	0.0%
HOUSING TENURE					
Owner-occupied housing units	88.0%	+/-4.8	93.6%	+/-4.2	33.3%
Renter-occupied housing units	12.0%	+/-4.8	6.4%	+/-4.2	66.7%

Subject	Census Tract 8518.01, Kane County, Illinois				
	Male householder, no wife present, family household	Female householder, no husband present, family household		Nonfamily household	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Total households	+/-71	261	+/-115	659	+/-148
Average household size	+/-0.97	3.82	+/-0.86	1.32	+/-0.26
FAMILIES					
Total families	+/-71	261	+/-115	(X)	(X)
Average family size	+/-0.35	2.99	+/-0.84	(X)	(X)
AGE OF OWN CHILDREN					
Households with own children under 18 years	+/-58	75	+/-67	(X)	(X)
Under 6 years only	+/-38.6	46.7%	+/-52.1	(X)	(X)
Under 6 years and 6 to 17 years	+/-38.6	0.0%	+/-29.8	(X)	(X)
6 to 17 years only	+/-38.6	53.3%	+/-52.1	(X)	(X)
Total households	+/-71	261	+/-115	659	+/-148
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	+/-38.3	38.7%	+/-22.1	0.0%	+/-4.1
Households with one or more people 60 years and over	+/-38.3	52.1%	+/-19.3	32.2%	+/-11.5
Householder living alone	(X)	(X)	(X)	80.4%	+/-14.1
65 years and over	(X)	(X)	(X)	25.3%	+/-10.9
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	(X)	(X)	(X)	(X)	(X)
Opposite sex	(X)	(X)	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	+/-32.1	100.0%	+/-10.0	79.7%	+/-10.2
2-or-more-unit structures	+/-32.1	0.0%	+/-10.0	18.1%	+/-9.7
Mobile homes and all other types of units	+/-25.9	0.0%	+/-10.0	2.3%	+/-3.6
HOUSING TENURE					
Owner-occupied housing units	+/-35.5	95.8%	+/-8.0	79.1%	+/-12.4
Renter-occupied housing units	+/-35.5	4.2%	+/-8.0	20.9%	+/-12.4

Subject	Census Tract 8546, Kane County, Illinois				
	Total		Married-couple family household		Male householder, no wife present, family household
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total households	1,293	+/-92	463	+/-108	69
Average household size	2.84	+/-0.32	3.82	+/-0.63	4.67
FAMILIES					
Total families	775	+/-94	463	+/-108	69
Average family size	3.64	+/-0.40	3.77	+/-0.62	3.17
AGE OF OWN CHILDREN					
Households with own children under 18 years	529	+/-91	320	+/-96	49
Under 6 years only	23.4%	+/-14.5	12.8%	+/-14.9	85.7%
Under 6 years and 6 to 17 years	13.6%	+/-9.6	13.1%	+/-13.5	0.0%
6 to 17 years only	62.9%	+/-15.8	74.1%	+/-18.8	14.3%
Total households	1,293	+/-92	463	+/-108	69
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	45.0%	+/-6.3	72.4%	+/-9.8	85.5%
Households with one or more people 60 years and over	23.1%	+/-5.4	16.0%	+/-7.5	13.0%
Householder living alone	35.2%	+/-6.8	(X)	(X)	(X)
65 years and over	7.3%	+/-4.2	(X)	(X)	(X)
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	0.0%	+/-2.1	(X)	(X)	(X)
Opposite sex	9.9%	+/-5.4	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	44.5%	+/-7.6	74.9%	+/-12.3	58.0%
2-or-more-unit structures	55.5%	+/-7.6	25.1%	+/-12.3	42.0%
Mobile homes and all other types of units	0.0%	+/-2.1	0.0%	+/-5.8	0.0%
HOUSING TENURE					
Owner-occupied housing units	36.7%	+/-7.8	59.2%	+/-13.5	27.5%
Renter-occupied housing units	63.3%	+/-7.8	40.8%	+/-13.5	72.5%

Subject	Census Tract 8546, Kane County, Illinois				
	Male householder, no wife present, family household	Female householder, no husband present, family household		Nonfamily household	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Total households	+/-42	243	+/-94	518	+/-112
Average household size	+/-1.44	4.09	+/-0.42	1.14	+/-0.09
FAMILIES					
Total families	+/-42	243	+/-94	(X)	(X)
Average family size	+/-0.52	3.54	+/-0.45	(X)	(X)
AGE OF OWN CHILDREN					
Households with own children under 18 years	+/-43	160	+/-80	(X)	(X)
Under 6 years only	+/-27.4	25.6%	+/-28.5	(X)	(X)
Under 6 years and 6 to 17 years	+/-39.0	18.8%	+/-18.2	(X)	(X)
6 to 17 years only	+/-27.4	55.6%	+/-28.0	(X)	(X)
Total households	+/-42	243	+/-94	518	+/-112
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	+/-27.3	77.4%	+/-16.3	0.0%	+/-5.2
Households with one or more people 60 years and over	+/-19.5	18.1%	+/-12.5	33.2%	+/-11.6
Householder living alone	(X)	(X)	(X)	87.8%	+/-7.6
65 years and over	(X)	(X)	(X)	18.1%	+/-10.0
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	(X)	(X)	(X)	(X)	(X)
Opposite sex	(X)	(X)	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	+/-32.5	30.5%	+/-16.9	22.2%	+/-9.4
2-or-more-unit structures	+/-32.5	69.5%	+/-16.9	77.8%	+/-9.4
Mobile homes and all other types of units	+/-31.7	0.0%	+/-10.7	0.0%	+/-5.2
HOUSING TENURE					
Owner-occupied housing units	+/-33.5	29.6%	+/-22.6	21.0%	+/-9.2
Renter-occupied housing units	+/-33.5	70.4%	+/-22.6	79.0%	+/-9.2

Subject	Census Tract 8549, Kane County, Illinois				
	Total		Married-couple family household		Male householder, no wife present, family household
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total households	1,936	+/-96	860	+/-132	72
Average household size	2.63	+/-0.17	3.40	+/-0.31	3.58
FAMILIES					
Total families	1,245	+/-139	860	+/-132	72
Average family size	3.23	+/-0.24	3.40	+/-0.31	2.17
AGE OF OWN CHILDREN					
Households with own children under 18 years	678	+/-122	356	+/-95	57
Under 6 years only	26.8%	+/-12.1	13.8%	+/-11.8	50.9%
Under 6 years and 6 to 17 years	15.8%	+/-8.7	27.2%	+/-14.9	0.0%
6 to 17 years only	57.4%	+/-11.9	59.0%	+/-12.5	49.1%
Total households	1,936	+/-96	860	+/-132	72
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	37.9%	+/-6.1	44.1%	+/-8.7	86.1%
Households with one or more people 60 years and over	25.6%	+/-4.8	34.8%	+/-9.0	0.0%
Householder living alone	28.0%	+/-5.7	(X)	(X)	(X)
65 years and over	5.1%	+/-2.3	(X)	(X)	(X)
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	0.0%	+/-1.4	(X)	(X)	(X)
Opposite sex	8.0%	+/-3.3	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	66.2%	+/-6.0	83.7%	+/-10.0	90.3%
2-or-more-unit structures	31.1%	+/-5.9	15.3%	+/-9.9	9.7%
Mobile homes and all other types of units	2.7%	+/-2.4	0.9%	+/-1.6	0.0%
HOUSING TENURE					
Owner-occupied housing units	58.9%	+/-6.7	81.2%	+/-10.7	51.4%
Renter-occupied housing units	41.1%	+/-6.7	18.8%	+/-10.7	48.6%

Subject	Census Tract 8549, Kane County, Illinois				
	Male householder, no wife present, family household	Female householder, no husband present, family household		Nonfamily household	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Total households	+/-53	313	+/-112	691	+/-114
Average household size	+/-0.65	3.23	+/-0.44	1.31	+/-0.16
FAMILIES					
Total families	+/-53	313	+/-112	(X)	(X)
Average family size	+/-0.25	3.02	+/-0.43	(X)	(X)
AGE OF OWN CHILDREN					
Households with own children under 18 years	+/-55	265	+/-108	(X)	(X)
Under 6 years only	+/-50.9	39.2%	+/-22.3	(X)	(X)
Under 6 years and 6 to 17 years	+/-35.9	3.8%	+/-6.0	(X)	(X)
6 to 17 years only	+/-50.9	57.0%	+/-22.2	(X)	(X)
Total households	+/-53	313	+/-112	691	+/-114
SELECTED HOUSEHOLDS BY TYPE					
Households with one or more people under 18 years	+/-25.9	93.6%	+/-7.0	0.0%	+/-3.9
Households with one or more people 60 years and over	+/-30.7	15.3%	+/-10.6	21.6%	+/-8.6
Householder living alone	(X)	(X)	(X)	78.4%	+/-9.7
65 years and over	(X)	(X)	(X)	14.2%	+/-6.8
UNMARRIED-PARTNER HOUSEHOLDS					
Same sex	(X)	(X)	(X)	(X)	(X)
Opposite sex	(X)	(X)	(X)	(X)	(X)
UNITS IN STRUCTURE					
1-unit structures	+/-19.1	42.2%	+/-19.7	52.7%	+/-12.1
2-or-more-unit structures	+/-19.1	57.8%	+/-19.7	41.0%	+/-11.7
Mobile homes and all other types of units	+/-30.7	0.0%	+/-8.4	6.4%	+/-6.6
HOUSING TENURE					
Owner-occupied housing units	+/-37.7	29.4%	+/-16.8	45.3%	+/-12.5
Renter-occupied housing units	+/-37.7	70.6%	+/-16.8	54.7%	+/-12.5

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see Accuracy of the Data). The effect of nonsampling error is not represented in these tables.

Average family size is derived by dividing the number of related people in households by the number of family households.

Housing unit weight is used throughout this table (only exception is the average household and family size cells).

While the 2010-2014 American Community Survey (ACS) data generally reflect the February 2013 Office of Management and Budget (OMB) definitions of metropolitan and micropolitan statistical areas; in certain instances the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB definitions due to differences in the effective dates of the geographic entities.

Estimates of urban and rural population, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Source: U.S. Census Bureau, 2010-2014 American Community Survey 5-Year Estimates

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S2301

EMPLOYMENT STATUS

2010-2014 American Community Survey 5-Year Estimates

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Data and Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities and towns and estimates of housing units for states and counties.

Subject	Census Tract 8513.01, Kane County, Illinois				
	Total		In labor force		Employed
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Population 16 years and over	3,057	+/-305	77.3%	+/-4.8	65.9%
AGE					
16 to 19 years	198	+/-99	29.8%	+/-24.6	15.2%
20 to 24 years	486	+/-132	86.8%	+/-11.1	62.1%
25 to 44 years	1,245	+/-256	92.7%	+/-5.0	84.2%
45 to 54 years	564	+/-145	85.3%	+/-10.1	75.2%
55 to 64 years	370	+/-137	66.8%	+/-14.2	57.0%
65 to 74 years	138	+/-69	0.0%	+/-18.0	0.0%
75 years and over	56	+/-43	0.0%	+/-36.3	0.0%
RACE AND HISPANIC OR LATINO ORIGIN					
One race	3,012	+/-313	77.3%	+/-4.8	66.3%
White	1,996	+/-382	75.4%	+/-5.6	67.7%
Black or African American	158	+/-104	89.2%	+/-13.4	64.6%
American Indian and Alaska Native	35	+/-39	100.0%	+/-46.1	34.3%
Asian	96	+/-69	77.1%	+/-35.1	77.1%
Native Hawaiian and Other Pacific Islander	0	+/-11	-	**	-
Some other race	727	+/-321	79.1%	+/-10.5	63.0%
Two or more races	45	+/-40	75.6%	+/-32.4	37.8%
Hispanic or Latino origin (of any race)	2,091	+/-316	77.5%	+/-5.6	65.8%
White alone, not Hispanic or Latino	625	+/-162	70.6%	+/-10.8	62.9%
Population 20 to 64 years	2,665	+/-296	86.5%	+/-3.9	74.5%
SEX					
Male	1,340	+/-201	96.0%	+/-3.2	86.3%
Female	1,325	+/-139	76.8%	+/-6.9	62.5%
With own children under 6 years	324	+/-108	76.2%	+/-18.2	51.2%
POVERTY STATUS IN THE PAST 12 MONTHS					
Below poverty level	328	+/-110	87.2%	+/-11.0	56.4%
DISABILITY STATUS					
With any disability	208	+/-122	75.5%	+/-15.5	58.2%

Subject	Census Tract 8513.01, Kane County, Illinois				
	Total		In labor force		Employed
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
EDUCATIONAL ATTAINMENT					
Population 25 to 64 years	2,179	+/-261	86.4%	+/-4.5	77.2%
Less than high school graduate	678	+/-238	85.1%	+/-6.3	74.5%
High school graduate (includes equivalency)	619	+/-220	88.0%	+/-9.1	82.1%
Some college or associate's degree	523	+/-154	76.7%	+/-11.8	66.3%
Bachelor's degree or higher	359	+/-114	100.0%	+/-7.4	90.0%
PERCENT IMPUTED					
Employment status for population 16 years and over	6.1%	(X)	(X)	(X)	(X)

Subject	Census Tract 8513.01, Kane County, Illinois			Census Tract 8546, Kane County, Illinois	
	Employed	Unemployment rate		Total	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Population 16 years and over	+/-5.4	14.7%	+/-4.8	2,604	+/-249
AGE					
16 to 19 years	+/-13.4	49.2%	+/-26.7	252	+/-97
20 to 24 years	+/-12.5	28.4%	+/-10.0	336	+/-135
25 to 44 years	+/-5.8	9.2%	+/-5.9	1,063	+/-164
45 to 54 years	+/-12.7	11.9%	+/-11.2	493	+/-143
55 to 64 years	+/-13.2	14.6%	+/-14.0	275	+/-84
65 to 74 years	+/-18.0	-	**	110	+/-48
75 years and over	+/-36.3	-	**	75	+/-50
RACE AND HISPANIC OR LATINO ORIGIN					
One race	+/-5.5	14.2%	+/-5.0	2,509	+/-262
White	+/-6.8	10.1%	+/-5.9	1,577	+/-335
Black or African American	+/-21.8	27.7%	+/-18.2	381	+/-165
American Indian and Alaska Native	+/-56.9	65.7%	+/-56.9	1	+/-2
Asian	+/-35.1	0.0%	+/-30.1	99	+/-51
Native Hawaiian and Other Pacific Islander	**	-	**	0	+/-11
Some other race	+/-14.1	20.3%	+/-13.3	451	+/-225
Two or more races	+/-40.4	50.0%	+/-50.0	95	+/-67
Hispanic or Latino origin (of any race)	+/-6.9	15.1%	+/-6.2	1,362	+/-298
White alone, not Hispanic or Latino	+/-11.6	10.9%	+/-8.7	707	+/-155
Population 20 to 64 years	+/-4.7	13.8%	+/-4.5	2,167	+/-233
SEX					
Male	+/-6.2	10.1%	+/-5.2	1,196	+/-172
Female	+/-8.7	18.6%	+/-9.1	971	+/-131
With own children under 6 years	+/-15.3	32.8%	+/-20.6	187	+/-97
POVERTY STATUS IN THE PAST 12 MONTHS					
Below poverty level	+/-20.7	35.3%	+/-20.8	576	+/-259
DISABILITY STATUS					
With any disability	+/-25.9	22.9%	+/-25.1	274	+/-85
EDUCATIONAL ATTAINMENT					
Population 25 to 64 years	+/-4.9	10.6%	+/-4.9	1,831	+/-193
Less than high school graduate	+/-9.9	12.5%	+/-9.4	578	+/-181
High school graduate (includes equivalency)	+/-11.7	6.8%	+/-8.0	430	+/-133
Some college or associate's degree	+/-12.4	13.5%	+/-13.0	436	+/-141
Bachelor's degree or higher	+/-10.2	10.0%	+/-10.2	387	+/-154
PERCENT IMPUTED					
Employment status for population 16 years and over	(X)	(X)	(X)	4.8%	(X)

Subject	Census Tract 8546, Kane County, Illinois				
	In labor force		Employed		Unemployment rate
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Population 16 years and over	70.5%	+/-6.7	62.5%	+/-8.9	11.4%
AGE					
16 to 19 years	15.5%	+/-18.9	15.1%	+/-19.4	2.6%
20 to 24 years	69.9%	+/-15.7	69.9%	+/-15.7	0.0%
25 to 44 years	90.2%	+/-7.3	77.9%	+/-12.4	13.7%
45 to 54 years	75.7%	+/-12.7	64.3%	+/-14.7	15.0%
55 to 64 years	57.8%	+/-16.8	55.6%	+/-17.0	3.8%
65 to 74 years	33.6%	+/-27.5	20.0%	+/-19.6	40.5%
75 years and over	45.3%	+/-41.3	45.3%	+/-41.3	0.0%
RACE AND HISPANIC OR LATINO ORIGIN					
One race	71.0%	+/-7.0	62.9%	+/-9.4	11.5%
White	68.4%	+/-9.6	59.9%	+/-12.9	12.4%
Black or African American	73.2%	+/-15.1	65.4%	+/-19.0	10.8%
American Indian and Alaska Native	0.0%	+/-100.0	0.0%	+/-100.0	-
Asian	64.6%	+/-25.8	47.5%	+/-23.7	26.6%
Native Hawaiian and Other Pacific Islander	-	**	-	**	-
Some other race	79.8%	+/-11.7	74.5%	+/-13.9	6.7%
Two or more races	56.8%	+/-25.1	52.6%	+/-25.1	7.4%
Hispanic or Latino origin (of any race)	69.8%	+/-9.7	63.9%	+/-13.8	8.5%
White alone, not Hispanic or Latino	73.4%	+/-7.9	62.5%	+/-11.4	14.8%
Population 20 to 64 years	79.6%	+/-6.9	70.7%	+/-9.7	11.2%
SEX					
Male	86.3%	+/-8.4	71.7%	+/-12.9	17.0%
Female	71.5%	+/-10.0	69.6%	+/-9.7	2.6%
With own children under 6 years	63.6%	+/-26.6	63.6%	+/-26.6	0.0%
POVERTY STATUS IN THE PAST 12 MONTHS					
Below poverty level	59.2%	+/-14.4	42.9%	+/-18.5	27.6%
DISABILITY STATUS					
With any disability	58.0%	+/-17.8	44.9%	+/-17.1	22.6%
EDUCATIONAL ATTAINMENT					
Population 25 to 64 years	81.4%	+/-7.2	70.9%	+/-10.4	12.9%
Less than high school graduate	60.2%	+/-18.6	55.9%	+/-19.0	7.2%
High school graduate (includes equivalency)	91.4%	+/-7.4	72.1%	+/-16.3	21.1%
Some college or associate's degree	86.5%	+/-10.9	68.8%	+/-16.3	20.4%
Bachelor's degree or higher	96.4%	+/-4.3	94.3%	+/-5.4	2.1%
PERCENT IMPUTED					
Employment status for population 16 years and over	(X)	(X)	(X)	(X)	(X)

Subject	Census Tract
	8546, Kane
	County, Illinois
	Unemployment rate
	Margin of Error
Population 16 years and over	+/-6.0
AGE	
16 to 19 years	+/-12.8
20 to 24 years	+/-11.0
25 to 44 years	+/-8.5
45 to 54 years	+/-11.0
55 to 64 years	+/-6.0
65 to 74 years	+/-42.5
75 years and over	+/-46.8
RACE AND HISPANIC OR LATINO ORIGIN	
One race	+/-6.2
White	+/-8.1
Black or African American	+/-15.1
American Indian and Alaska Native	**
Asian	+/-34.0
Native Hawaiian and Other Pacific Islander	**
Some other race	+/-8.1
Two or more races	+/-15.6
Hispanic or Latino origin (of any race)	+/-7.9
White alone, not Hispanic or Latino	+/-10.8
Population 20 to 64 years	+/-5.9
SEX	
Male	+/-9.4
Female	+/-2.7
With own children under 6 years	+/-20.5
POVERTY STATUS IN THE PAST 12 MONTHS	
Below poverty level	+/-17.0
DISABILITY STATUS	
With any disability	+/-21.4
EDUCATIONAL ATTAINMENT	
Population 25 to 64 years	+/-6.8
Less than high school graduate	+/-7.9
High school graduate (includes equivalency)	+/-14.8
Some college or associate's degree	+/-14.8
Bachelor's degree or higher	+/-3.7
PERCENT IMPUTED	
Employment status for population 16 years and over	(X)

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Source: U.S. Census Bureau, 2010-2014 American Community Survey 5-Year Estimates

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8. An '(X)' means that the estimate is not applicable or not available.

error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see Accuracy of the Data). The effect of nonsampling error is not represented in these tables.

The Census Bureau introduced a new set of disability questions in the 2008 ACS questionnaire. Accordingly, comparisons of disability data from 2008 or later with data from prior years are not recommended. For more information on these questions and their evaluation in the 2006 ACS Content Test, see the Evaluation Report Covering Disability.

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Appendix H Section 106 Coordination

OCT 22 2015

PRESERVATION SERVICES

IHPA REVIEW
H/A _____
AC _____
AR _____
File _____



U.S. Department
of Transportation
Federal Transit
Administration

REGION V
Illinois, Indiana,
Michigan, Minnesota,
Ohio, Wisconsin

200 West Adams Street
Suite 320
Chicago, IL 60606-5253
312-353-2789
312-886-0351 (fax)

October 14, 2015

Rachel Leibowitz, Ph.D.
Deputy State Historic Preservation Officer
Illinois Historic Preservation Agency
1 Old State Capitol Plaza
Springfield, IL 62701

RE: FTA Supplemental Section 106 Historic Review Initiation/Determination: Metra
Milwaukee District West Line Fox River Bridge Project, Kane County, City of Elgin,
Illinois - IHPA Log #010082310

Dear Ms. Leibowitz:

As part of its responsibilities under 36 CFR § 800 – Protection of Historic Properties and the National Historic Preservation Act (NHPA), the Federal Transit Administration (FTA) is reinitiating the Section 106 Consultation Process for the proposed Metra Milwaukee District West Line (MD-W) Fox River Bridge Project (proposed Project) in Kane County, City of Elgin, Illinois. FTA is also providing the associated Area of Potential Effects (APE), eligibility and effects determinations. FTA is taking this action due in part to the unusual length of time that has passed since consultation was initiated for the proposed Project with the Illinois Historic Preservation Agency (IHPA) in 2010.

The purpose of the proposed UP-W Fox River Bridge Project is to replace the existing single-track railroad bridge, which was originally constructed in 1881 and is nearing the end of its useful life. The existing bridge is about 500 feet long and consists of six steel spans resting on the original masonry abutments and piers. Three of the original spans were replaced in 1905 and the other three were replaced in 1926. The new bridge will be double-tracked, within and adjacent to the footprint of the existing structure, and will relieve an existing bottleneck that delays railroad traffic. Existing bridge piers will be removed and replaced while the current abutments will be partially removed and altered to accommodate the new structure. Double-track will be realigned / constructed on land about 500 feet northward and 650 feet southward from the bridge. FTA has determined that the proposed Project will be a Federal undertaking as defined in §800.16(y) and that it is a type of activity that has the potential to cause effects on historic properties.

The Section 106 consultation process consists of four steps, all of which are completed in consultation with the State Historic Preservation Officer (SHPO) and/or Tribal Historic Preservation Officer (THPO), and other consulting parties.

1. FTA initiates the Section 106 process, pursuant to §800.3 – Initiation of the Section 106 process, with the SHPO (or THPO if the property is on tribal lands) and other consulting parties if any.

RE: FTA Supplemental Section 106 Historic Review Initiation/Determination: Metra Milwaukee District West Line Fox River Bridge Project, Kane County, City of Elgin, Illinois - IHPA Log #010082310

2. FTA determines the project's Area of Potential Effects (APE) and the properties within the APE that are listed, or eligible for listing, in the National Register of Historic Places (NRHP). FTA evaluates properties eligible for listing using the processes established in 36 CFR § 60 and National Register Bulletin 15. FTA's determination of the APE requires consultation with and concurrence by the SHPO. If FTA determines there are no properties within the APE that are listed, or eligible for listing, in the NRHP, or if FTA determines there are historic properties present but the project will have no effect upon them, then FTA will determine "no historic properties affected" in consultation with the SHPO and / or THPO.
3. FTA determines adverse effects with respect to historic properties within the APE. FTA's determination considers whether the project will diminish those qualities that make any of the properties eligible for listing in the NRHP. FTA makes a determination of "adverse effect" when the project will diminish these qualities in one or more properties; if not, FTA makes a determination of "no adverse effect." FTA's determination of "no adverse effect," along with concurrence by the SHPO, completes the Section 106 consultation process.
4. If FTA determines an "adverse effect," it consults with the Advisory Council on Historic Preservation (ACHP), SHPO, affected tribes, and other interested parties, as appropriate, to resolve the adverse effects on historic properties. Resolution of adverse effects may involve redesigning a project to avoid, minimize, or mitigate impacts to historic properties. Actions that the consulting parties agree upon to mitigate adverse effects are documented in a Memorandum of Agreement (MOA). Once the agreement is signed by all appropriate parties, including the SHPO and other invited signatories, and the agreement is filed with the ACHP, the Section 106 process is completed, and the FTA's responsibilities are fulfilled when the MOA's stipulations are implemented.

Lin Engineering notified IHPA of the proposed MD-W Line Fox River Bridge Project via correspondence dated August 17, 2010. In correspondence dated September 17, 2010, Huff & Huff provided IHPA with topographical and zoning maps delineating the proposed Project location in addition to various site photographs. The proposed Project location boundary is about 1,700 feet long and 190 feet wide. In a letter dated September 24, 2010, IHPA stated "Based upon the information provided, no historic properties are affected. We, therefore, have no objection to the undertaking proceeding as planned.....This clearance remains in effect for two years from date of issuance."

In correspondence dated August 5, 2011, Huff & Huff advised IHPA that Metra had changed the design of the bridge to accommodate two tracks as opposed to one. IHPA provided a concurrence letter dated August 25, 2011 that was essentially the same as the aforementioned IHPA letter.

On August 17, 2012, FTA invited the following tribes to participate in consultation and help identify places that may have traditional religious and cultural importance to them at or near the proposed Project site: Citizen Potawatomi Nation; Forest County Potawatomi Community; Hannahville Indian Community; and Prairies Band of Potawatomi Nation. FTA received one response from the Forest County Potawatomi dated September 27, 2012. This tribe requested that they be provided with results of any archival review, cultural resource investigation studies, and archaeological reports. They would like to be consulted should there be an impact or effect to cultural and historic properties as a result of the proposed Project.

RE: FTA Supplemental Section 106 Historic Review Initiation/Determination: Metra
Milwaukee District West Line Fox River Bridge Project, Kane County, City of Elgin,
Illinois - IHPA Log #010082310

Metra responded to the Forest County Potawatomi in correspondence dated November 9, 2012 and provided the aforementioned IHPA August 25, 2011 letter. Metra confirmed that the SHPO and Forest County Potawatomi would be notified should any discovery result in the requirement for any archival reviews, cultural resource investigation studies or archaeological reports.

In the enclosed correspondence dated September 3, 2015, Metra provided FTA with an updated project description, APE, copies of the aforementioned IHPA letters, and an Inadvertent Discovery Plan (IDP). Apart from the IDP, there are no substantive changes to the proposed Project. The IDP has been prepared for the proposed Project regarding potential archaeological findings and is provided to IHPA for review and comment. If archaeological deposits are encountered from the post-contact period during monitoring, they will be evaluated by a qualified professional archaeologist regarding their eligibility for listing in the NRHP in consultation with the IHPA and Forest County Potawatomi. All archaeological recording will be done in accordance with the Secretary of the Interior's "Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716) and reports of the archaeological documentation will be submitted to the IHPA and Forest County Potawatomi for review and comment.

In compliance with Section 106 of the NHPA, and in accordance with the procedures related to the identification of historic properties described in the implementing regulations at 36 CFR § 800, based on the aforementioned documentation, FTA has determined the following for the proposed MD-W Fox River Bridge Project: the APE is the boundary as delineated on the enclosed aerial map; there are no properties on or eligible for the NRHP within the APE; and the Project would result in **no historic properties affected**. Pursuant to 36 CFR § 800, FTA is seeking IHPA concurrence with the aforementioned APE and eligibility/effects determinations within 30 days of receipt of this letter.

If FTA can provide any assistance or additional information which would aid in your prompt reply, please feel free to contact Reggie Arkell at 312-886-3704 or reginald.arkell@dot.gov. Thank you for your assistance.

Sincerely,



Marisol R. Simón
Regional Administrator

CC: Tom Weaver, Metra

Enclosures: Metra correspondence dated September 3, 2015 and attachments

CONCUR

By: R. Leitowitz
Deputy State Historic Preservation Officer

Date: 10/20/15 SJH

Appendix I Cultural Resources Inadvertent Discovery Plan

Metra
Fox River Bridge Replacement and Track Addition Project
Cultural Resources Inadvertent Discovery Plan

Inadvertent Discoveries: Archaeological Deposits

During construction of the Fox River Bridge Replacement and Track Addition Project (Project), if unanticipated archaeological artifacts, structural remains or other features are encountered, the Metra construction supervisor will stop ground-disturbing work in the area of the find, will temporarily fence the area for protection, and will notify the Federal Transit Administration (FTA) and the Illinois Historic Preservation Agency (IHPA) of the discovery. Upon consultation with the FTA and the IHPA, Metra will retain a professional archaeologist who meets federal qualifications (36 CFR Part 61; 48 Fed. Reg. 44716 (1983)) to examine the discovery. The archaeological consultant will make recommendations to the FTA and the IHPA regarding further action.

If the consulting archaeologist recommends resuming project construction work and the FTA and the IHPA agree, then construction may proceed immediately. If the archaeological consultant recommends that further investigation is necessary, then the FTA and the IHPA will collaborate with the archaeological consultant to determine steps to be taken to evaluate the discovery and determine whether the archaeological deposits encountered are eligible for the National Register of Historic Places (NRHP). If the archaeological consultant recommends that the archaeological remains are not eligible for the NRHP, and the FTA and the IHPA agree, construction work for the project may proceed immediately. If the archaeological consultant recommends that the archaeological remains are eligible for the NRHP, and the FTA and the IHPA agree, the FTA and the archaeological consultant, in consultation with the IHPA, will promptly develop a data recovery plan to mitigate the effect of the project on the archaeological deposits.

The FTA will ensure that the data recovery plan is executed. When the archaeological consultant believes that the fieldwork effort has successfully carried out the mitigation plan, they will consult with the FTA and the IHPA. If the FTA and the IHPA agree that the mitigation plan has been successfully completed, construction will be allowed to resume immediately.

Once archaeological fieldwork is complete, the archaeological consultant will prepare a report that describes the data recovery methodology, the results of the fieldwork, and the analysis of the data recovered. A draft report will be prepared on a schedule agreed upon by the FTA and the IHPA. The FTA and the IHPA will review the draft and provide comments. Once the archaeological consultant has received and addressed the comments, a final report will be completed on a schedule agreed upon by the FTA, the IHPA and the archaeological consultant. At a minimum, the archaeological consultant will provide a copy of the final report to the FTA and to the IHPA.

In consultation with the IHPA, the FTA will arrange for curation of all archaeological materials recovered, and all records created during the fieldwork.

Inadvertent Discoveries: Human Remains

In the State of Illinois, the treatment of inadvertent discoveries of human remains is guided by the Human Skeletal Remains Act (20 ILCS 3440).

During Project construction activities, if human remains are encountered, the Metra construction supervisor shall immediately stop work, cordon off, and protect the area. Metra shall then immediately notify the Kane County coroner, the FTA, and the IHPA. The coroner will determine whether the remains are a crime scene. If so, the coroner will maintain jurisdiction and determine the appropriate steps. If not, jurisdiction will be transferred to the IHPA and the IHPA will, in consultation with the FTA, determine the appropriate steps to be taken.

If it is determined that human remains are to be disinterred, disinterment will occur either under the authority of the coroner, or through consultation with the IHPA. Any disinterment will be conducted by an archaeologist experienced in disinterring human remains. Disinterred human remains will also be examined by a physical anthropologist. The physical anthropologist will create an inventory of the skeletal elements, and to the extent possible, determine the number of individuals represented, the age, sex and ethnicity of the individuals, as well as estimate the stature of individuals, record pathologies and any other pertinent information. No invasive or destructive analysis will be undertaken.

If the physical anthropologist, in consultation with the IHPA, can reasonably determine that it is likely that the remains are Native American, the FTA shall, in consultation with the IHPA, notify the appropriate tribal groups. Disposition of disinterred human remains determined to be Native American, and any associated funerary objects, shall be determined pursuant to applicable state and federal law through consultation among the tribes, the FTA, and the IHPA. Disposition of human remains that are determined to not be Native American shall be determined by the FTA in consultation with the IHPA.

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May 6, 2015

Appendix J References

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